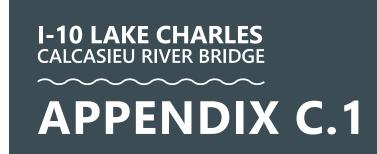


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Public Involvement and Agency Scoping Meeting Summary Report

October 2013

Public Involvement Summary: Notice of Intent, Solicitation of Views, and Public Scoping Meeting

Environmental Impact Statement

I-10 Calcasieu River Bridge (I-10/I-210 West End to I-10/I-210 East End) Calcasieu Parish, Louisiana

Louisiana Department of Transportation and Development



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Federal Highway Administration



February 2014

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Public Involvement Summary Solicitation of Views, Notice of Intent and Public Scoping Meeting

I-10 Calcasieu River Bridge (I-10/I-210 West End to I-10/I-210 East End)

1.0 PROJECT BACKGROUND

The limits of the proposed project extend along Interstate Highway 10 (I-10) between the Interstate Highway 210 (I-210) interchanges, a distance of approximately 9 miles. The proposed project includes the Calcasieu River Bridge. The primary purpose of the proposed project is to improve traffic congestion, but will also address safety and roadway/bridge design issues. The below project location map shows the limits of the proposed project.



Figure 1. Project Location Map.

The existing I-10 corridor outside of the project limits is a six-lane facility (three lanes in each direction). Within the proposed project limits, including the Calcasieu River Bridge, I-10 is primarily a four-lane facility (two lanes in each direction). The proposed project is intended to provide system continuity on I-10 through the Lake Charles metropolitan area by upgrading the existing system and increasing capacity through the region.

The steep approaches to the Calcasieu River Bridge are becoming four-lane bottlenecks on the connecting six-lane highway. The structure has an approximate average daily traffic (ADT) of 53,000 vehicles per day which is carried on 4 – 12 foot wide lanes without bridge shoulders and the bridge

grades are as steep as 5 percent. The Calcasieu River Bridge structure, originally completed in 1952, has recently undergone a rehabilitation project, but will be in need of additional improvements in the future.

In addition to roadway and bridge alternatives, improvements to be investigated within the proposed project limits include: a redesign of Sampson Street from Sulphur Avenue to provide grade separations with existing railroads; a redesign of the access to and from I-10 on the west side of the bridge between Sampson Street and PPG Drive; a redesign of the access to and from I-10 near the east end of the bridge; a redesign of access to and from Ryan Street and consideration of the frontage roads from PPG Drive to US 90 East.

An engineering and environmental feasibility study was initiated in 2000, completed in 2002 and identified several feasible alternatives. In addition to the feasibility study, in accordance with the National Environmental Policy Act (NEPA), several environmental and other studies were initiated for various components of the proposed project. Due to issues identified during the NEPA process, environmental documents were not finalized. The two major issues identified during the process were bridge height and the discovery of unknown hazardous contamination within the proposed right-of-way (ROW) in the area of the Sampson Street interchange. Because the project is receiving high public interest and there is a potential for significant impacts, the Louisiana Department of Transportation and Development (DOTD) and the Federal Highway Administration (FHWA) are preparing an Environmental Impact Statement (EIS).

2.0 SOLICITATION OF VIEWS

During the initial planning stage of the project, views from Federal, state and local agencies, organizations and individuals were solicited. Early coordination was initiated with a Solicitation of Views (SOV) packet, which was mailed September 9, 2013 to applicable Federal, state and local agencies, organizations, Native American Tribal contacts and elected officials. The packet included a letter, preliminary project description and project location map. The SOV letter requested identification of possible adverse economic, social, or environmental effects or concerns. **Table 2-1** summarizes the responses to the SOV packet by the agencies. Copies of the SOV responses are included in **Appendix A**. SOV responses will also be included and addressed in the EIS.

TABLE 2-1. SUMMARY OF SOLICITATION OF VIEWS RESPONSES

ID# *	Date	Responder & Organization	Response Summary
1	9/11/13	Sarah Haymaker State Conservationist, NRCS, USDA	Review of project map and narrative indicates the proposed construction areas are within existing ROWs and, therefore, are exempt from the rules and regulations of the FPPA-Subtitle I of Title XV, Section 1539-1549; nor are impacts to NRCS work in the vicinity anticipated. Referred to the Web Soil Survey for specific information on soils.
2	9/11/13	Alice Yett FAA	No comment on the SOV. Per the FAA, it will complete a study 45 days before project construction that will review impacts to either runways or navigational equipment. Based on the location of the Calcasieu River Bridge, there is potential to impact navigational equipment (i.e., the signal emitted from the equipment). The study will be good for 18 months, with a possibility for an 18-month extension. FAA suggests DOTD re-initiate contact with the FAA once project design begins.
3	9/12/13	Michael Bechdol Coordinator, Sole Source Aquifer Program, Ground Water/UIC Section, USEPA, Region IV	Do not anticipate an adverse effect on the quality of the ground water underlying the project site; based solely upon the potential impact to the quality of ground water as it relates to the USEPA's authority pursuant to Section 1424(e) of the Safe Drinking Water Act.
4	10/2/13	Mayra G. Diaz Floodplain Management and Insurance Branch, FEMA Region VI, Mitigation Division	Request that the parish floodplain administrator be contacted for the review and possible permit requirements and that the project be in compliance with EOs 11988 and 11990.
5	10/2/13	Pam Breaux SHPO, LA Office of the Lieutenant Governor, Department of Culture, Recreation & Tourism, Office of Cultural Development	The following will need to be provided in order to complete the Section 106 review: description of the APE; description of all historic properties within and adjacent to the APE; detailed project scope of work including design plans; map and site plan; and photographs of the APE, project location and historic structures.
6	10/3/13	Susan Veillon CFM, Floodplain Management Program Coordinator, DOTD	Project runs in and out of the flood zone and crosses the Kayouchee Coulee, a designated floodway. Give consideration for the occurrence of a base flood inundation, clearing debris and keeping the area cleared. Request the floodplain administrators for Calcasieu Parish and Cities of Lake Charles and Westlake be contacted to ensure compliance with the National Flood Insurance Program.

ID# *	Date	Responder & Organization	Response Summary
7	10/3/13	James H. Welsh Commissioner of Conservation, LA Department of Natural Resources, Office of Conservation	Review of records indicated: Presence of oil and/or gas wells located near the project area, as well as registered water wells in the vicinity of the project area. Unregistered water wells may also be located in the area.
8	10/4/13	Roger Thomas CPSO Crash Reconstructionist, Calcasieu Parish Sheriff's Office, Retired LA State Police Sergeant, Troop D, Lake Charles	Expressed concerns about utilizing I-210 as a detour route, with large trucks traveling in the right-outside lane of I-210. Noted that motorists in the past have suggested large trucks be directed to the left inside-lane so that traffic can easily transition onto I-210 from the entrance ramps.
9	10/7/13	Bill Shearman Chairman, Downtown Development Authority City of Lake Charles	Included Resolution 2013–10 adopted on Oct. 7, 2013, which 1) encourages holding a public meeting as soon as practicable, 2) keeping the existing bridge open for traffic during all construction phases, 3) recommends any land removed from DOTD use along the frontage road on the south side of the I-10 bridge be reconfigured for more convenient local access and/or allocated to the city for reconfiguring access to adjacent city-owned lakefront properties to enhance economic development; and 4) recommends bridge improvements and designs include the Transportation Enhancement considerations to enhance the gateway into the Downtown Lakefront Development District.
10	10/7/13	Lynn F. Thibodeaux Clerk of the Council, City of Lake Charles, Office of the City Council	Included Resolution 222-13 in response to the SOV, adopted on 10/2/13, which 1) encourages holding a public meeting as soon as practicable, 2) keeping the existing bridge open for traffic during all construction phases, 3) recommends any land removed from DOTD use along the frontage road on the south side of the I-10 bridge be reconfigured for more convenient local access and/or allocated to the city for reconfiguring access to adjacent city-owned lakefront properties to enhance economic development; 4) requests incorporating iconic features of the current bridge be preserved and used wherever possible or duplicated within the new bridge design; 5) recommends the inclusion of transportation enhancement considerations (e.g., gateway signage, landscaping) for the City of Lake Charles and City of Westlake gateways, and 6) requests that safety concerns be addressed and additional study on the curve and the approach to the Opelousas Street exit be performed to eliminate traffic safety hazards.

ID# *	Date	Responder & Organization	Response Summary
11	10/8/13	Grant Bush Executive Director, IMCAL MPO Transportation Director	Issued support for the project. Also noted two prior resolutions passed on previously studied I-10 Bridge options: 1) Resolution #2037 passed by the Transportation Committee on Dec. 11, 2008, adopting option 4; and 2) Resolution 2040m passed by IMCAL on April 27, 2009 adopting option 4. Option 4 included replacing the existing bridge with a parallel bridge while maintaining traffic on the existing bridge.
12	10/8/13	Cleve Hardman Director of Outdoor Recreation, LA Office of the Lieutenant Governor, Department of Culture, Recreation & Tourism, Office of State Parks	LWCF Project #22-00201 Lakefront Recreation Area is the only facility in proximity to the project boundaries; it does not appear this location will be disrupted by the proposed project. Request consideration of restrictions of the LWCF in regards to project development.
13	10/10/13	Dana Masters THPO/Cultural Director, Council Member, Jena Band of Choctaw Indians	Deferred SOV to the Coushatta Tribe of Louisiana.
14	10/11/13	Pam Lightfoot CFM, Floodplain Management Program Coordinator, DOTD	Project runs in and out of the flood zone and crosses the Kayouchee Coulee, a designated floodway. Give consideration for the occurrence of a base flood inundation, clearing debris and keeping the area cleared. Request the floodplain administrators for Calcasieu Parish and Cities of Lake Charles and Westlake be contacted to ensure compliance with the NFIP.
15	10/17/13	Raul Gutierrez, Ph.D. Wetlands Section, Water Quality Protection Division, USEPA	Jurisdictional waters of the U.S. occur along the proposed project route. USEPA recommends coordination with the USACE, New Orleans District to verify if permits are needed. USEPA will review to ensure impacts to waters of the U.S. are minimized and unavoidable impacts compensated.

ID# *	Date	Responder & Organization	Response Summary
16	11/5/13	Cheryl Sonnier Nolan Assistant Secretary Office of Environmental Compliance LDEQ, Office of Environmental Compliance	 General comments relate to the obtainment of all necessary approvals and permits. This includes the following: submit a LPDES application if the project results in a discharge to waters of the state; the potential need for modification of the LPDES permit before accepting additional wastewater if the project results in a discharge to an existing wastewater treatment system; contacting the LDEQ Water Permits Division for storm water general permits if the construction area is equal to or greater than one acre; contacting the USACE regarding permitting issues if work will occur in areas subject to USACE jurisdiction, which may involve a water quality certification from LDEQ; observe precaution to protect groundwater and workers from hazardous constituents, if applicable; and if hazardous wastes, soils, or groundwater are encountered, notify the LDEQ SPOC. Specific comments include the following: Without final piling locations and proposed depths, it is not possible to provide specificity in recommending depths which would be protective of the subsurface environment. LDEQ has no objection to piling depths of 75 feet below current existing grade or less north of the current I-10 footprint – per the correspondence from LDEQ to DOTD on November 19, 2009. No piling should exceed a depth of 40 feet below current existing grade south of the current I-10 footprint with the exception of the following: using a line drawn from CPT18, CPT7, and a point 50 feet due east of I8 as a reference, there would be no depth restrictions to the east of this line (see EDMS Document ID# 6754900 for reference points).
17	11/14/13	Rhonda Smith Chief, Office of Planning and Coordination, USEPA	 When draft EIS is complete, send one hard copy and four digital copies to the Region VI office for comment. When ready to file the draft EIS with USEPA, do so electronically by using their e-NEPA Electronic Filing website (http://www.epa.gov/compliance/nepa/submiteis/index.html). Letter identifies generalized issues for attention in the preparation of the EIS, including the following: Clearly identify the underlying purpose and need. Develop a defined screening process for the evaluation of alternatives. Provide a description of the affected environment (baseline conditions). Assess environmental consequences by determining the intensity of impacts and if these impacts are significant and adverse, then provide measures to avoid, minimize or mitigate. These resources/issues include water resources, Section 4(f) and 6(f), project specific locations, biological resources, habitat and wildlife, invasive species, air quality, climate change, greenhouse gases, hazardous materials, tribal consultation, cultural resources, environmental justice and impacted communities, children's health and safety, indirect and cumulative impacts, mitigation and monitoring, and coordination with land use planning activities. Refer to the comment itself in Appendix A for a description of the regulations and guidelines dictating the assessment of environmental consequences.

Pam Mattingly CFM, Floodplain Administrator, Calcasieu Parish Police Jury, Division of Planning and Development Pam Mattingly CFM, Floodplain management purposes, the proposed project portion located in the unincorporated areas of Calcasieu Parish is outside the 100-year floodplain and has no elevation or permit requirements.	ID# *	Date	Responder & Organization	Response Summary
	18	11/15/13	CFM, Floodplain Administrator, Calcasieu Parish Police Jury, Division of Planning and	areas of Calcasieu Parish is outside the 100-year floodplain and has no elevation or permit

NOTE: * Copies of the SOV letters in their entirety are found in Appendix A and are referenced by ID #.

Acronym List:

APE = Area of Potential Effects

CFM = Certified Floodplain Manager

CPSO = Calcasieu Parish Sherriff's Office

DOTD = Louisiana Department of Transportation and Development

EO = Executive Order

FAA = Federal Aviation Administration

FPPA = Farmland Protection Policy Act

FEMA = Federal Emergency Management Administration

IMCAL = Imperial Calcasieu Regional Planning and Development Commission

LPDES = Louisiana Pollutant Discharge Elimination System

LDEQ = Louisiana Department of Environmental Quality

LWCF = Land and Water Conservation Fund

MPO = Metropolitan Planning Organization

NFIP = National Flood Insurance Program

NRCS = National Resources Conservation Service

ROW = Right-of-Way

SHPO = State Historic Preservation Officer

SPOC = LDEQ's Single-Point-of-Contact

THPO = Tribal Historic Preservation Officer

USACE = United States Army Corps of Engineers

USDA = United States Department of Agriculture

USEPA = United States Environmental Protection Agency

3.0 NOTICE OF INTENT

A Notice of Intent (dated July 25, 2013) stating that the FHWA and DOTD had initiated the preparation of and EIS for the I-10 Calcasieu River Bridge Project (I-10/I-210 West End to I-10/I-210 East End) was published in the Federal Register on August 1, 2013. A comment letter, submitted by Ms. Leslie Barras, was received by FHWA in response to the NOI. See **Appendix B-1** for a copy of the NOI and **Appendix C-1**, **ID #1** for a copy of Ms. Barras' letter. What follows is a summary of her comments within that letter.

Comment ID #1:

- Commenter requested to be involved in the NEPA scoping and review process for the proposed project.
- Commenter requested status as a consulting part in the Section 106 process of the National Historic Preservation Act (NHPA) given her concerns and interest in preserving the existing bridge. An explanation related to the Section 106 process is provided in **Section 4.0** below.
- Commenter referenced the Waitemata Harbour Bridge (Auckland Harbour Bridge) in Auckland, New Zealand as an "innovative capacity expansion of another truss bridge of the same era as the I-10 bridge." Commenter stated that it is known as a "clip on" bridge because of the addition of lanes in the late 1960s on either side of the original bridge. Commenter included pictures of the bridge (see below) and the following link, providing an engineering overview of the "clip on" project and lessons learned:

http://www.ipenz.org.nz/heritage/itemdetail.cfm?itemid=117



Photographs: Waitemata Harbour Bridge (Auckland Harbour Bridge).

Response to Comment ID #1: Commenter is included on the project mailing list and has and will continue to receive quarterly newsletters, meeting invitations and all other project updates/correspondence. Commenter has been accepted by FHWA as a Section 106 consulting party and will be invited to all meetings/discussions related to this issue. In relation to the "clip on" bridge widening, consideration will be given to various bridge design options, including widening of the existing bridge, construction of a new bridge, rehabilitation of the existing bridge and a no-build alternative. Alternatives will be evaluated based on design, operational and safety considerations; and Section 106 coordination will occur concurrently and input from these coordination efforts will also be a consideration in the selection of a preferred alternative(s) to be evaluated within the EIS.

Subsequent to the above comments but prior to the Public Scoping Meeting, Ms. Barras submitted an additional comment letter dated October 9, 2013, providing comments on the *Comprehensive Preliminary Alternatives Report*¹ posted to the project Website. This report was completed in May 2002 as an

¹ I-10 Calcasieu River Bridge and Approaches Comprehensive Preliminary Alternatives Report, May 2002, S.P. No. 700-10-0115, F.A.P. No. BR-10-1(212)29

engineering and environmental feasibility study (i.e., Stage 0 report in the DOTD's project delivery process) for a six mile corridor along I-10, including the Calcasieu River Bridge. The report reviewed by the commenter summarized the six technical memorandums previously submitted to the state and federal agencies during the preliminary phases of the project at the time. After the 2002 *Comprehensive Preliminary Alternatives Report* was completed, the *I-10 Calcasieu River Bridge and Approaches EA* was initiated, but then put on hold due to a re-evaluation of the navigational clearance needed at the bridge crossing. Below is a summary of Ms. Barras' comments on the 2002 *Comprehensive Preliminary Alternatives Report*, submitted on October 9, 2013; and a copy of Ms. Barras' letter outlining these comments is presented in **Appendix C-1, Comment ID #2**.

Comment ID #2:

- On the purpose and need, the commenter disagrees that the purpose of the project should be to replace the existing bridge. Commenter states the array of alternatives proposed should include preservation of the existing bridge. Commenter states that the purpose and need should present updated traffic and safety data.
- Commenter notes that the Calcasieu River Bridge was declared eligible for the National Register of Historic Places (NRHP), was placed on the list of Nationally and Exceptionally Significant Features of the Interstate Highway System and that compliance with both Section 106 of the NRHP and Section 4(f) is required. Commenter states that if a prudent and feasible alternative exists that involves using the historic bridge, the FHWA must select that alternative.
- Commenter states that additional alternatives need to be identified and evaluated, citing the Waitemata Harbour Bridge "clip on" widening as an example option that should be studied (see above photographs in this section). Commenter states that the only alternative from the 2002 report that preserves the bridge while providing increased capacity, improved access and maintaining traffic during construction is Alignment 3, which should be advanced in the NEPA process. Commenter states that a variation on this option should be included, which consists of using the existing historic bridge for through-traffic and constructing lower-elevation frontage roads on either side of the existing bridge for local traffic.

Response to Comment ID #2: The purpose and need referenced by the commenter was as presented in the 2002 Comprehensive Preliminary Alternatives Report, and is therefore, not applicable to the current project under study for the EIS. As part of the EIS process, the purpose and need has been revised based on the current project and includes updated data. This Draft Purpose and Need for the EIS subject project is posted to the project Website, was summarized in the Fall 2013 project newsletter and has been presented and solicited for input at both the Agency and Public Scoping Meeting on October 24, 2013. As part of the EIS process, consideration will be given to various bridge design options, including widening of the existing bridge, construction of a new bridge, rehabilitation of the existing bridge and a nobuild alternative. Alternatives will be evaluated based on design, operational and safety considerations; and Section 106 coordination will occur concurrently and input from these coordination efforts will also be a consideration in the selection of a preferred alternative(s) to be evaluated within the EIS. In relation to construction of lower elevation frontage roads compared to the existing bridge height, FHWA and DOTD, in accordance with U.S. Coast Guard (USCG) request, will complete an updated navigation study and mitigation plan, which will evaluate existing and future navigational clearance needs for property owners north of the bridge, as well as potential economic impacts (see Section 5.1.2, Comment ID# 6).

4.0 SECTION 106 OF THE NHPA

The NRHP eligible Calcasieu River Bridge, as well as other historic and/or potentially historic properties, may be affected by the proposed project. Section 106 of the NHPA requires the FHWA and the DOTD, in consultation with the Louisiana State Historic Preservation Officer (SHPO), to identify potential parties for consultation in order to assure that historic properties are taken into consideration at all levels of project planning and development. Accordingly, formal Section 106 consulting party invites were mailed to local historic organizations and tribes. See **Appendix B-2** for a copy of the Section 106 consulting party invite. Additionally, the newspaper meeting advertisement for the Public Scoping Meeting (see **Appendix B-5**) included a solicitation for individuals/organizations requesting to become Section 106 consulting parties.

This request was also announced at both the Agency and Public Scoping Meetings, further described in **Section 5.0**. Persons/organizations requesting to be a Section 106 consulting party were asked to provide reasons for their request; FHWA will make the final determination of who will be accepted as a Section 106 consulting party.

5.0 AGENCY AND PUBLIC SCOPING MEETINGS

The first agency and public meetings of the EIS process were held on Thursday, October 24, 2013 at the Lake Charles Civic Center, located at 900 Lake Shore Drive, Lake Charles, LA, 70601. The agency scoping meeting was held from 2:00 p.m. to 3:00 p.m. and included a formal presentation and question/answer session. An open-forum public scoping meeting was held on the same day from 5:00 p.m. to 8:00 p.m., which included eight station locations where the public could view project exhibits and converse with knowledgeable project team members. The agency and public scoping meetings are further described in the sections below.

5.1 Agency Scoping Meeting

Agency Work Group (AWG) meetings are the primary mechanism for ensuring agency participation in the project development process. The AWG is comprised of lead, cooperating and participating agency representatives. The agency scoping meeting summarized in this report is the first AWG meeting held for the proposed project. The purpose of the agency scoping meeting was to facilitate a discussion and solicit comments on issues material to the *Draft Project Coordination Plan*, *Draft Purpose and Need* and proposed study area, as well as solicit input on specific issues/resources to be addressed in the EIS. Agency invite letters were mailed to applicable federal, state, regional, and local agencies, as well as local elected officials. See **Appendix B-3** for a copy of the Agency Scoping Meeting invite. Thirty-six (36) agency representatives/elected officials and five consultant representatives attended the Agency Scoping Meeting. Sign-in sheets from the Agency Scoping Meeting are presented in **Appendix B-7**. As previously mentioned, this meeting included a formal presentation, after which a question and answer session was held. See **Appendix B-8** for a copy of the agency presentation. Agency representatives/elected officials were also able to view the eight public meeting stations with exhibits to be presented to the general public at the public scoping meeting later that day. See **Section 5.2** and **Appendix B-10** for a description and copies of each exhibit, respectively.

5.1.1 Verbal Comments

Verbal comments were received during a question and answer session immediately following the agency presentation. **Table 5-1** below presents a summary of these comments and a corresponding response based on the current status of the proposed project.

TABLE 5-1. VERBAL COMMENT SUMMARY – AGENCY SCOPING MEETING

ID#	Name & Organization	Comment(s) Summary	Response
1	John Cardone City Administrator, City of Lake Charles	Commenter would like to see alternatives developed that include keeping I-10 open as long as possible during construction and does not like the idea of having only I-210 open during construction.	Comment noted. Various alternatives will be studied during the alternatives development phase of the proposed project. Alternatives will be evaluated based on design, operational and safety considerations, which will likely include the feasibility and functionality of keeping I-10 open during construction as a screening criterion. Results of the evaluation will be documented, including reasons for which any alternatives may have been dropped from further consideration.
2	Grant L. Bush Executive Director, IMCAL	As noted in previous comment letters from IMCAL on the proposed project, the commenter stated IMCAL's desire to see I-10 remain open during construction.	Comment noted. See response to comment ID #1 in this table.
3	Jason Derise Captain, Lake Charles Police Department	Commenter recommended the consideration of implementing a draw bridge across the river because the marine traffic is not crossing every day, which in turn would allow the grades to be as flat as possible, thereby improving safety.	Comment noted. See response to comment ID #1 in this table.
4	Honorable Michael E. Danahay State Representative, District 33, Louisiana House of Representatives	Commenter requested the Project Team expedite the EIS process, noting that the three-year estimate for completion of the EIS seems long. Commenter inquired if there was any funding allocated for the project, when funding would be requested and if it could be sought before the EIS is completed.	The Project Team noted that the EIS process would be completed in the most timely and efficient manner practicable. The Project Team also noted, however, that all of the EIS steps are important for NEPA compliance, and therefore, necessary to the process itself. See Comment ID #7 in this table regarding available funding for the proposed project.
5	James R. Wetherington Bridge Specialist, USCG District 8	Commenter noted that the bridge is the main concern of the USCG. Commenter acknowledged the 2001 marine use study previously completed for this project, but given the amount of time having elapsed, requested an updated marine use study be prepared. Additionally, the USCG would like the updated marine use study to consider a mitigation plan.	Project Team noted in response to the comment that a 2006 Lake Charles Port Planning Study also investigated navigational clearance. Commenter stated that an updated navigation study and mitigation plan would still be required by the USCG. Concluded that the Project Team would have follow-up discussions with the USCG regarding the need for an updated navigation study and mitigation plan; and that the USCG would submit a formal written request for these updated studies.

6	Lori Marinovich DDA and Planning, City of Lake Charles	Commenter expressed appreciation for the opportunity to participate in the Section 106 process.	Comment noted. Commenter was encouraged during the meeting to sign-up to request to be a Section 106 consulting party and to include a reason for their request. Commenter signed-up at the Agency Scoping Meeting and FHWA has accepted her request.		
7	Bob Mahoney Environmental Coordinator, FHWA	Commenter expressed appreciation for the agency input. Commenter encouraged all agencies to participate and present any known potential issues or concerns they may have, stating that the sooner an issue is known, the more efficiently it can be addressed and the EIS process can move forward. Commenter also asked that agencies who request to become a Section 106 consulting party give a reason with their request. In response to comment ID #4 in this table, the commenter noted that funding is not available at this time, but local and regional officials do not need to wait until after the EIS to begin the process of acquiring funding.	Comment noted.		
DDA = D EIS = En FHWA = IMCAL = NEPA =	Acronym List: DDA = Downtown Development Authority EIS = Environmental Impact Statement FHWA = Federal Highway Administration IMCAL = Imperial Calcasieu Regional Planning and Development Commission NEPA = National Environmental Policy Act USCG = United States Coast Guard				

5.1.2 Written Comments

A total of six written comments were received in response to the Agency Scoping Meeting. Copies of these comments are presented in **Appendix C-2. Table 5-2** below presents a summary of these comments and a corresponding response based on the current status of the proposed project.

TABLE 5-2. WRITTEN COMMENT SUMMARY – AGENCY SCOPING MEETING

ID# *	Name & Organization	Comment(s) Summary	Response
1	John S. Bruce Public Works Director, City of Sulphur	 Regarding the <i>Draft Purpose and Need</i>, the commenter noted safety issues with overpasses and shoulder widths, geometric and alignment issues leading accidents (overturning) at IH-10 and US 171, and problems resulting from the number of travel lanes. Commenter suggested coordination related to I-210 maintenance/diversion Commenter requested to be sent future updates on the proposed project. 	 Comment noted. All of the commenter's suggested reasons for the project are included within the <i>Draft Purpose and Need</i> for proposed project. The purposes of the proposed project are to increase capacity and reduce congestion, improve roadway deficiencies and traffic operations and to enhance safety. The ability of a proposed alternative to meet the needs of the project and fulfill these stated purposes will serve as evaluation criteria for the advancement of an alternative throughout the EIS process. Coordination efforts related to the alternatives development and screening process, which will include discussions related to alternatives that allow for I-10 and the bridge to remain open during construction compared to alternatives that would require I-210 to be used as a diversion route, will occur during the next AWG meeting (date TBD). Commenter is included on the project mailing list and will continue to receive quarterly project newsletters, meeting invites and all other project correspondence and updates.
2	Lori Marinovich DDA and Planning, City of Lake Charles	Commenter stated that the City of Lake Charles includes historic districts adjacent to the project. Commenter requested to become a Section 106 consulting party with the reasoning that she coordinates the historic districts for the City and is a member of the preservation society.	FHWA has reviewed and accepted the commenter's request to be a Section 106 consulting party. As part of the Section 106 process, steps will be taken to identify historic properties/districts and those that are historic will be evaluated to see if the proposed project will have adverse effects.
3	Bill Shearman Chairman, DDA City of Lake Charles	The City of Lake Charles DDA submitted for the official record of the Agency Scoping Meeting a copy of their resolution responding to the I-10 Calcasieu River Bridge SOV packet mailed September 9, 2013. The content of this resolution is outlined in Table 2-1, Comment ID #9 .	SOV responses will be included and addressed in the EIS.

ID# *	Name & Organization	Comment(s) Summary	Response
4	Lynn F. Thibodeaux Clerk of the Council, City of Lake Charles, Office of the City Council	The Lake Charles City Council submitted for the official record of the Agency Scoping Meeting a copy of their resolution responding to the I-10 Calcasieu River Bridge SOV packet mailed September 9, 2013. The content of this resolution is outlined in Table 2-1 , Comment ID #10 .	SOV responses will be included and addressed in the EIS.
5	Joe Toups Director, Lake Charles Civic Center	 Regarding the <i>Draft Project Coordination Plan</i>, commenter made reference to the quarterly project newsletters, but did not make any specific suggestions about the newsletter. Commenter stated that the bridge should be as tall as the Trinity River Bridge near Anuach, Texas so as to not limit expansion and development north of the Calcasieu River Bridge. Commenter noted that plant expansions will result in large loads of equipment and parts shipped to areas north of the bridge. 	 Comment noted. The project newsletters will occur on a quarterly basis and be emailed to all interested persons having notified the Project Team of their desire to receive the newsletters, identified stakeholders, elected officials, and agency representatives. Bridge height/navigational clearance will be further analyzed as part of the proposed project. See comment ID #6 in this table.
6	James R. Wetherington, Bridge Specialist, USCG District 8 on behalf of David M. Frank, Commander, USCG District 8	Commenter submitted a formal letter request to Mr. Carl M. Highsmith, Programs Operations Manager, FHWA, for an updated navigation study, noting the USCG's acceptance to serve as both a Participating and Cooperating Agency. The letter asks that the navigation study update the 2001 Marine Use Study and 2006 Lake Charles Port Planning Study and that it be completed in time to be included in the Draft EIS. Per the USCG, the updated navigation study should document all navigation north of the bridge site, which should include contacting all property owners north of the bridge site and documenting their respective current and future waterway usage (including but not limited to the large vessels known to occupy berthing north of the bridge). Finally, the USCG requests that any issues requiring mitigation be mentioned specifically and agreed to, at least in principal, and specifically noted in the FEIS prior to the signature of the ROD.	An updated navigation study and mitigation plan fulfilling the requirements outlined in the USCG's formal request is being undertaken by the FHWA and the DOTD.

ID# *	Name &	Commont(a) Summon	Doonanaa
וט#	Organization	Comment(s) Summary	Response

NOTE: * Copies of the Agency Scoping Meeting written comments in their entirety are found in Appendix C-2 and are referenced by ID #.

Acronym List:
DDA = Downtown Development Authority

DOTD = Louisiana Department of Transportation and Development EIS = Environmental Impact Statement

FEIS = Final Environmental Impact Statement
FHWA = Federal Highway Administration
IMCAL = Imperial Calcasieu Regional Planning and Development Commission
NEPA = National Environmental Policy Act

ROD = Record of Decision

USCG = United States Coast Guard

5.2 Public Scoping Meeting

Public involvement is intended to create opportunities for the pubic to have input in identifying transportation problems and solutions and to participate in the project planning process. The purpose of the Public Scoping Meeting was to present and solicit comments on the Draft Project Coordination Plan, Draft Purpose and Need and project study area, while also aiding in the public's understanding of the NEPA/EIS process. Public meeting notices (postcards) were mailed to adjacent property owners throughout the project corridor, and postcards were also distributed to community centers, libraries, churches, and other public facilities within the City of Lake Charles (see Appendix B-4). Advertisements were placed in the local newspapers, running both two and one week prior to the Public Scoping Meeting and project newsletters advertising the meeting were mailed to local agencies, officials, businesses and other identified stakeholders (see Appendices B-5 and B-6, respectively). Invites and materials such as the Draft Project Coordination Plan and Draft Purpose and Need were posted to the project website prior to the meeting. Eighteen (18) members of the general public; 20 agency representatives, elected officials, or media representatives; and eight consultants attended the Public Scoping Meeting. The Public Scoping Meeting sign-in sheets are presented in Appendix B-7. Photographs taken at the Public Scoping Meeting are presented in **Appendix D**.

As previously described, the Public Scoping Meeting included eight different meeting stations, each manned by a project team member to answer questions and facilitate discussion. The eight meeting stations are described below, and the exhibits displayed at each station are presented in **Appendix B-10**.

Station 1 – Welcome and Section 106 Sign-In: Attendees were asked to sign-in and provide an email address if they wished to receive meeting notifications and newsletters on the proposed project. Attendees were given three handouts: 1) A project fact sheet presenting a general overview of the project, the EIS process, estimated EIS timeline, next steps, and project location map; 2) A project history summary outlining previous studies completed within the project corridor and on the Calcasieu River Bridge, beginning in 1950 and extending to present-day; and 3) a station checklist that described the exhibits and purpose of each meeting station. Additionally, attendees were encouraged to sign-up here if they were interested in requesting to be a NHPA Section 106 consulting party and were also asked to provide reasoning for that request. A copy of the Fact Sheet, Project History and Station Checklist are provided in **Appendix B-9.**

Station 2 – Project Location Map: Attendees viewed the overall project location map, including the proposed project limits (I-10/1-210 West End to I-10/I-210 East End).

Station 3 – The EIS Process: Attendees were presented with a step-by-step representation of the EIS process, including an explanation of where we currently are in this process, what an EIS is, and why is it needed.

Station 4 – Draft Project Timeline: Attendees viewed a graphic representation of the project timeline, including when the next public involvement and comment opportunities would occur and where we currently are within this timeline of events.

Station 5 – Draft Purpose and Need: Attendees were invited to discuss the project's *Draft Purpose and Need* with project staff.

Station 6 – Preliminary Typical Sections: Attendees were presented with an existing typical section of I-10, depicting two-through lanes in each direction as well as a proposed typical section of I-10, depicting three-through lanes in each direction.

Station 7 – Constraints Mapping: Attendees were invited to identify any environmental, topographical or other consideration that may affect the location, development or other aspect of the project within the study area. Attendees were asked to draw, outline or note any potential constraining factors directly on

the maps provided at this station. Previously identified constraining factors were presented on the maps for public review.

Station 8 – Let Us Hear From You: Attendees were invited to ask questions about the project and complete written comment forms or give formal verbal comments. Attendees had the option of completing the comment form at the meeting, after the meeting or mailing it back, or submitting a comment through the project website.

5.2.1 Verbal Comments

No formal verbal comments were submitted at the Public Scoping Meeting. However, Project Team members documented comments and questions received at the various meeting stations. **Table 5-3** below presents a summary of those comments and questions, with corresponding responses.

TABLE 5-3. VERBAL COMMENT SUMMARY - PUBLIC SCOPING MEETING

ID#	Comments	Deemana
ID#	Comments	Response
1	The new bridge should be a signature bridge similar to the existing Calcasieu River Bridge.	Comment noted. Various alternatives will be studied during the alternatives development phase of the proposed project, including rehabilitation and the no-build alternative. Alternatives will be evaluated based on design, operational and safety considerations. Results of the evaluation will be documented, including reasons for which any alternatives may have been dropped from further consideration. The aesthetic design and construction of the proposed bridge will be based on numerous factors, including but not limited to the type/style of bridge constructed, bridge design criteria, available funding, and public and governmental support. Section 106 coordination efforts, as well as future Agency Work Group and public meetings will present opportunities for discussion related to the bridge design/aesthetic. Bridge aesthetics will be formalized during final project design.
2	The new bridge should be architecturally appealing.	See response to Comment ID #1 in this table.
3	The existing bridge, including the bridge height, is signature to Lake Charles and should stay.	See response to Comment ID #1 in this table.
4	Beautification efforts should occur along the I-10 corridor.	Comment noted.
5	Request for a segmented bridge.	See response to Comment ID #1 in this table.
6	Request that the bridge be high enough to not have an impact on industry.	FHWA and DOTD, in accordance with USCG request, will complete an updated navigation study and mitigation plan, which will evaluate existing and future navigational clearance needs for property owners north of the bridge, as well as potential economic impacts (see Section 5.1.2, Comment ID# 6).
7	The lowest bridge height will adversely affect port development to the north of the existing Calcasieu River Bridge	See response to Comment ID #6 in this table.

8	Ramps should be designed to have the best economic impact.	Comment noted. The purposes of the proposed project are to increase capacity and reduce congestion, improve roadway deficiencies and traffic operations and to enhance safety. Additionally, the proposed project would provide better access by redesigning interchanges and improving frontage roads, thus supporting economic development in the Lake Charles metropolitan area. The ability of a proposed alternative to meet the needs of the project and fulfill these stated purposes will serve as evaluation criteria for the advancement of an alternative throughout the EIS process.		
Questions				
9	What is the proposed bridge height?	Bridge height has not yet been determined. See response to Comment ID #6 in this table.		
10	Has funding been allocated for the proposed project?	The proposed project is included in the current (2008) Statewide Transportation Plan (STP) as a Priority A Mega highway project. Priority A Mega highway projects are selected through a process that considers future travel demand, as estimated by the Statewide Travel Demand Model, economic impacts, safety, etc. This process allows the most needed projects to be implemented first. DOTD allocates funding to projects in phases (Feasibility, Environmental, ROW, Utility, Design, and Construction), and funding is set up for an active phase. For this project, the current active and funded phase is "Environmental". Feasibility has been completed and the next phase following environmental to be activated/funded will be "Design".		

5.2.2 Written Comments

A total of nine written comments were received in response to the Public Scoping Meeting. Copies of these written comments are presented in **Appendix C-3**. The comments are identified in **Table 5-4** below. Because several of the comments address multiple issues, the comments are summarized below **Table 5-4**, and responses are provided accordingly.

TABLE 5-4. WRITTEN COMMENTS RECEIVED - PUBLIC SCOPING MEETING

ID# *	Name	Organization (if applicable)	Title (if applicable)		
1	Anonymous				
2	Charlie Atherton	Lake Charles Harbor and Terminal District (LCHTD)			
3	Steve Belin	Phillips 66	Manager, Eastern Region Remediation Management, Phillips 66		
	Steve Geiger	Phillips 66	Manager, Lake Charles Manufacturing Complex, Phillips 66		
4	Adley Cormier	Lake Charles Historic Preservation Commission	Member		
5	Ben Garber				
6	Igbal Mohammad				
7	Cornelius Moon				

	Don Tipton		Executive Board of Directors		
8	Sandra Tipton	Friend Ships Unlimited			
	Teri Shields				
9	Michael Tritico,	RESTORE	Biologist and President of RESTORE		
10	Michael Tritico	RESTORE	Biologist and President of RESTORE		
11	Perry D. Vincent LA Radio Communications, Inc				
Notes:					
* Copies of the Public Scoping Meeting written comments in their entirety are found in Appendix C-3 and are referenced by ID #.					

ID #1: Anonymous

-- no data

Comment 1: Commenter stated that those responsible for the hazardous contamination should be made to clean up the EDC plume, as well as maintaining continued monitoring via the state or federal government to ensure the EDC will not contaminate the Chicot Aquifer. Commenter requested the public be informed about the EDC plume and requested an investigation for all other contamination. Commenter stated that the bridge is not the primary concern, but instead it is the health of the citizenry. Commenter asked what has been done to ensure such contamination does not happen again and why Freedom of Information Act (FOIA) documents related to the EDC contamination have been redacted.

Response 1: Comment and concerns noted. The EDC contamination and its effects on the proposed project are being investigated as part of the EIS process.

Comment 2: Commenter requested the new bridge be constructed to the north of the existing bridge on "good land".

Response 2: Comment noted. In relation to the proposed project corridor, I-10 was identified by the U.S. Department of Transportation (USDOT) in September 2007 as one of six interstate routes to participate in a federal initiative to develop multi-state corridors to help reduce congestion. Accordingly, I-10 joined the USDOT "Corridors of the Future" program aimed at developing innovative national and regional approaches to reduce congestion and improve the efficiency of freight delivery. Moreover, the existing I-10 corridor has been designated as the project study area based on the purpose and need of the proposed project, which is to increase capacity and reduce congestion on I-10 between the I-210 interchanges in the Lake Charles region. Various alternatives, including different potential bridge locations, will be studied during the alternatives development phase of the proposed project, including the no-build alternative. Bridge locations extending north of the existing Calcasieu River Bridge (at distances of approximately 45 feet and 170 feet from the existing bridge centerline) have previously been evaluated and remain under consideration at this time. As part of the EIS process, alternatives will be evaluated based on design, operational and safety considerations. Results of the evaluation will be documented, including reasons for which any alternatives may have been dropped from further consideration.

ID #2: Charlie Atherton (LCHTD)

Comment 1: Commenter stated that the Calcasieu River Bridge should remain at its current 135-foot height, noting that the height was engineered to allow for the passage of ships that utilize the full carrying capacity of the Calcasieu River north where the water depth is naturally 60 to 80 feet deep. Commenter cited that after World War II, the navy docked hundreds of ships for miles along the river upstream of the bridge, proving suitability for navigability, and included the following photo as an example:



Commenter noted that Friend Ships utilizes the river crossing and that there are others who want to bring in large ships. Commenter stated it is against the law to restrict navigation or to block navigable waterways, citing 33 USC 494 (Obstruction of navigation, alterations and removals; lights and signals; draws). Commenter is requesting all documentation noting the decision to construct a 73-foot high bridge be entered into the public record and expressed concern that the decision to construct a 73-foot high bridge was made without the proper public participation process.

Response 1: Two prior navigation/bridge height studies were completed in relation to the proposed project: a 2001 Marine Use Study and a 2006 Lake Charles Port Planning Study. These two previous studies are available for viewing on the project website (http://i10lakecharles.com/). Public involvement efforts have occurred throughout the environmental process of this project, stemming back as early as 2000 during the feasibility study phase of the project; and these public involvement efforts have included information related to the aforementioned bridge height studies. FHWA and DOTD, in accordance with USCG request, will complete an updated navigation study and mitigation plan, which will evaluate existing and future navigational clearance needs for property owners north of the bridge, as well as potential economic impacts (see Section 5.1.2, Comment ID# 6). Accordingly, a bridge height for the proposed Calcasieu River Bridge has yet to be determined and is still under study as part of the EIS process. All past and future public involvement efforts have and will continue to be documented as part of the official public record,

Comment 2: Commenter stated that the lower-height bridge concept is politically driven by ConocoPhillips in order to conceal the level of EDC contamination under the bridge; and elected officials have fast tracked the decision for a lower-height bridge over the objection of the public.

Response 2: Comment and concerns noted. The EDC contamination and its effects on the proposed project are being investigated as part of the EIS process.

Comment 3: Commenter stated that the LCHTD passed a resolution (Resolution 3004-032) on May 24, 2004 that expressed support for maintaining the existing bridge height and width characteristics of the Calcasieu River Bridge for any new replacement bridge planned for future construction.

Response 3: Comment noted. For the record, the Imperial Calcasieu Regional Planning and Development Commission (IMCAL), the Lake Charles MPO, voted on December 19, 2007 in favor of a 73-foot vertical clearance for the I-10 Calcasieu River Bridge (see Appendix B-11).

Comment 4: Commenter presented an editorial from the American Press dated January 6, 2008 endorsing a 90-foot bridge height for the Calcasieu River Bridge instead of the 73-foot bridge height, noting that the 90-foot bridge height was also supported by Lake Charles Mayor Randy Roach. Commenter presented a 2007 article from the Sun Herald (serving Biloxi-Gulfport and the Mississippi Gulf Coast) discussing the potential implementation of a 73-foot bridge versus a 90-foot bridge for the I-10 Calcasieu River Bridge, including discussion of how constructing a bridge with too low of a vertical clearance could impact future economic development. Commenter presented a 2006 editorial from the Gulf Coast News (serving the Mississippi Gulf Coast) discussing potential damaging economic impacts associated with plans to rebuild the Hurricane Katrina damaged Bay St. Louis and Biloxi-Ocean Springs Bridges without a draw span, which the former bridges had, and at a height that would obstruct navigation.

plan, which will evaluate existing and future navigational clearance needs for property owners north of the bridge, as well as potential economic impacts (see Section 5.1.2, Comment ID# 6).

Response 4: Comment and concerns noted. FHWA and DOTD, in accordance with USCG request, will complete an updated navigation study and mitigation

ID #3: Steve Geiger and Steve Belin (Phillips 66)

Comment: The letter provided on behalf of Phillips 66 states that their comments are intended to "present and clarify for both DOTD and the public, the documented facts regarding impacts to the soil and groundwater beneath the proposed I-10 Calcasieu River Bridge." The statements provided on behalf of Phillips 66 are summarized as follows, with the full comment letter and reference figures included in Appendix C-3, ID #2. Note that these statements are written from the perspective of Phillips 66 alone, and do not necessarily represent the opinions or edicts of FHWA or DOTD.

- In 1994, a pipeline spilled EDC² into a ditch along Isle of Capri Boulevard, just south of the current I-10 Calcasieu River Bridge. Phillips 66 (as its predecessor companies) began an emergency response effort at the time of discovery and have continued to implement long term cleanup work, in conjunction with the Louisiana Department of Environmental Quality (LDEQ), to remediate soil and groundwater impacted by EDC in the area. Work Plans and Progress Reports have been regularly submitted to the LDEQ, and are available to the public at the LDEQ website. Virtually all of the free phase EDC has been captured and removed from the subsurface.
- Phillips 66 has worked cooperatively with the DOTD to evaluate the potential for EDC to impact future bridge construction activities, citing the installation of ground water monitoring wells in a marsh area north of the existing bridge, which identified low concentrations of EDC in groundwater below the marsh at depths of approximately 55 feet below ground surface. These impacts are above the depth of the Chicot Aquifer, are above the water intakes for the town and do not represent a threat to the drinking water of Westlake.
- Phillips 66 continues to monitor the presence of EDC in the area through the monitoring wells and has worked with LDEQ to establish a site-specific cleanup standard for EDC in the marsh area following the LDEQ RECAP³ process. Results show the concentration of EDC in the path of the new bridge to be below any concentration that would be a threat to the drinking water aguifer, to

² EDC = 1,2 dichloroethane

³ RECAP = Risk Evaluation and Corrective Action Program. A RECAP cleanup standard is a target concentration that when achieved, will be protective of human health and the environment.

- workers on the bridge or to any persons traveling over the bridge or in the area (see **Appendix C-3, ID #2, Pg. 6, Figure 1**). Accordingly, no EDC cleanup is necessary in the path of the new I-10 bridge as currently proposed.
- More aggressive remediation (proposed to be completed before any bridge construction begins) is being completed by Phillips 66 for an area beneath and to the south of the existing bridge where further EDC remediation is necessary (see Appendix C-3, ID #2, Pg. 6, Figure 1).
- Figure 2 of Appendix C-3, ID #2, Pg. 7 provides the location of the marsh monitoring points with test results from 2011 2014. Testing is performed by an independent third party engineer, shipped under chain of custody and analyzed by an independent third party laboratory; and the DOTD and LDEQ have also collected samples in the area to provide independent confirmation the results.
- The LDEQ has reviewed these data and on June 18, 2010 wrote a letter to DOTD providing their conclusions. Phillips 66 provided a copy of this letter, which expresses concern for the installation of foundation pilings for the new bridge through subsurface zones known to be contaminated with EDC. Specific comments from LDEQ includes the following, based on preliminary piling locations as of October 19, 2009 (subject to change once final piling locations are determined):
 - Without final piling locations and proposed depths, it is not possible to provide specificity in recommending depths which would be protective of the subsurface environment.
 - LDEQ has no objection to piling depths of 75 feet below current existing grade or less north of the current I-10 footprint – per the correspondence from LDEQ to DOTD on November 19, 2009.
 - No piling should exceed a depth of 40 feet below current existing grade south of the current I-10 footprint with the exception of the following: using a line drawn from CPT18, CPT7, and a point 50 feet due east of I8 as a reference, there would be no depth restrictions to the east of this line.
- Phillips 66 fully supports construction of the new I-10 Calcasieu River Bridge, will continue to remediate any remaining impacts from the 1994 spill and offer to work cooperatively with DOTD and their consultants preparing the EIS.

Response: Comment noted. The EDC contamination and its effects on the proposed project are being investigated as part of the EIS process.

ID #4: Adley Cormier (Lake Charles Historic Preservation Commission)

Comment 1: As a member of the Lake Charles Historic Preservation Commission and longtime historian, commenter requested to become a Section 106 consulting party.

Response 1: FHWA has reviewed and accepted the commenter's request to be a Section 106 consulting party.

Comment 2: Commenter expressed concern with any changes to the major vehicle corridor through central and north Lake Charles, noting that because of the unique geography, the route of I-10 displaced historic structures. Commenter would like to help mitigate any adverse change in the future.

Response 2: Comment and concerns noted. As a Section 106 consulting party, commenter will be able to provide input on the alternative and project development process. Adverse impacts will be avoided when practicable, and mitigated for if unavoidable, in accordance with federal, state, and local regulations governing historic structures.

ID #5: Ben Garber

Comment: Commenter expressed concern about the UP Railroad bridge bottleneck, citing that the railroad bridge is 107 years old and needs to be removed. Commenter would like to see a new railroad bridge incorporated into the project design, stating that development north of the Calcasieu River cannot occur without a redesign of the railroad bridge.

Response: Comment noted; however, the UP Railroad Bridge referenced by the commenter is outside the scope of the proposed project. The scope of the proposed project includes improvements to I-10, including the Calcasieu River Bridge, and the evaluation of alternatives for re-designing the at-grade Sampson Street interchange with existing railroad crossings.

ID #6: Igbal Mohammad

Comment: Request by commenter to receive future updates on the proposed project.

Response: Commenter was added to the project mailing list and will receive future quarterly project newsletters, meeting invites, and project correspondence and updates.

ID #7: Cornelius Moon

Comment 1: Regarding the *Draft Purpose and Need*, commenter states key reasons for the project include that the bridge is over 50 years old and in need of constant repair, the grade is too steep and there are safety issues.

Response 1: Comment noted. All of the commenter's suggested reasons for the project (i.e., safety issues and not meeting current design standards such as the steep bridge grade) are included within the *Draft Purpose and Need* for proposed project. The purposes of the proposed project are to increase capacity and reduce congestion, improve roadway deficiencies and traffic operations and to enhance safety. The ability of a proposed alternative to meet the needs of the project and fulfill these stated purposes will serve as evaluation criteria for the advancement of an alternative throughout the EIS process.

Comment 2: Commenter stated that the Draft Project Coordination Plan was an "excellent plan".

Response 2: Comment noted.

Comment 3: Commenter recommended constructing a new "signature bridge", specifically recommending a cable-stayed span bridge like the Audubon Bridge in West Feliciana Parish.

Response 3: Comment noted. Various alternatives will be studied during the alternatives development phase of the proposed project, including rehabilitation and the no-build alternative. Alternatives will be evaluated based on design, operational and safety considerations. Results of the evaluation will be documented, including reasons for which any alternatives may have been dropped from further consideration. The aesthetic design and construction of the proposed bridge will be based on numerous factors, including but not limited to the type/style of bridge constructed, bridge design criteria, available funding and public and governmental support. Section 106 coordination efforts, as well as future Agency Work Group and public meetings will present opportunities for discussion related to the bridge design/aesthetic. Bridge aesthetics will be formalized during final project design.

ID #8: Don and Sandra Tipton, Teri Shields (Friend Ships Unlimited)

Comment 1: Friend Ships is a maritime corporation located north of the I-10 Calcasieu River Bridge. Commenters expressed concern related to a new bridge having a navigational clearance below that of the existing bridge for the reasons that follow. Note that these statements are written from the perspective of Friend Ships United alone, and do not necessarily represent the opinions or edicts of FHWA or DOTD.

 Maintaining a vertical clearance as high as 135-feet is in the best interest of waterfront land owners for economic purposes. The Port of Lake Charles and City of Westlake own deep water frontage north of the bridge, which if unavailable to navigational traffic, could adversely affect jobs

- and diminish the value of future economic development in the low income, primarily African-American community of North Lake Charles.
- Friend Ships owns four ships and two large classic wooden boats that transit under the current bridge, with at least one ship requiring 111 foot vertical clearance and another classic vessel requiring similar clearance. The future plans of Friend Ships include the obtainment of three additional vessels, creation of a marina, job training center and repair yard for large classic boats.
- While other ship channels on the Calcasieu River have to be continually dredged at the cost of millions, this is a naturally deep channel that has never needed and never will need to be dredged.
- The channel is a protected safe harbor in times of storm and is utilized regularly for that purpose.
- Lowering the bridge is driven by an agenda to minimize the cleanup required by the chemical companies.
- It is incorrect to state that there is insufficient maritime traffic in the area. Because the bridge-lowering project has been proposed for so long, maritime entities have not been willing to risk an investment. Once it is known that navigational clearance will be maintained, interest in the deep water aspects of the navigational channel will resume.
- Commenters suggested building a new bridge with the vertical clearance height of 135-feet or to install a drawbridge to be opened once a day.

Response 1: Comment and concerns noted. FHWA and DOTD, in accordance with USCG request, will complete an updated navigation study and mitigation plan, which will evaluate existing and future navigational clearance needs for property owners north of the bridge, as well as potential economic impacts (see **Section 5.1.2, Comment ID# 6**).

Comment 2: Commenters would like to be given a chance to expound on their views in an open forum.

Response 2: The public meeting held on October 24, 2013 was an open forum format in which the general public was invited to participate and provide comments and feedback to the project team. The public was invited to submit written or verbal comments at the meeting itself, through the project website or by sending written comments via mail or email during the formal 10-day comment period following the public meeting. The comments received are summarized and addressed within this summary report, which will be published for public review on the project website and available for review by applicable agencies and elected officials. At least two other open forum public meetings will occur over the EIS project timeline during the alternatives development and selection process, as well as a formal public hearing following the publication of the Draft EIS. Members of the public will have an opportunity to submit written and/or verbal comments at all of these future public involvement opportunities.

ID #9: Michael Tritico (RESTORE)

Comment 1: Regarding the *Draft Purpose and Need*, the commenter noted that traffic congestion, safety and bridge design issues need to be carefully studied and changes made so that existing problems can be removed.

Response 1: Comment noted. The existing problems noted by the commenter (i.e., traffic congestion, safety and bridge design issues) are included within the *Draft Purpose and Need* for proposed project. The purposes of the proposed project are to increase capacity and reduce congestion, improve roadway deficiencies and traffic operations and to enhance safety. The ability of a proposed alternative to meet the needs of the project and fulfill these stated purposes will serve as evaluation criteria for the advancement of an alternative throughout the EIS process.

Comment 2: Regarding the *Draft Project Coordination Plan*, the commenter stated that this project has seemingly been stuck in the planning phase with no practical forward movement. However, the commenter explained that the public involvement opportunities (e.g., website, newsletters, etc.) are welcome improvements.

Response 2: Comment noted.

Comment 3: Commenter would like to receive future updates on the proposed project; short notifications via email and lengthy correspondence via mail.

Response 3: Commenter is included on the project mailing list and will receive quarterly project newsletters, meeting invites and updated correspondence and updates via email and mail, accordingly.

Comment 4: Commenter provided numerous statements related to constructing the new I-10 Calcasieu River Bridge north of its present location, including the following:

- Suggests constructing the bridge near Joe Miller Road, in the corridor between Moss Bluff and Gillis, or in the corridor between Gillis and Ragley. Any of these corridors are far enough north that the vertical bridge height issue would no longer be a problem, the ROW within the Gillis and Ragley corridor would likely be less expensive to acquire, and if constructed south of Ragley, the new interstate would be out of the area shown by the National Hurricane Center's Sea, Lake and Overland Surges from Hurricanes (SLOSH) model to be vulnerable to tropical storm surges.
- By moving I-10 and the new bridge north, any impediments to navigation generated by constructing a lower vertical clearance bridge would be eliminated. It is important to leave the existing navigational clearance of the Calcasieu River Bridge so that organizations like Friend Ships can complete their humanitarian work. If a low vertical bridge is implemented, it should be a draw bridge so that Friend Ships can fulfill their humanitarian duties.
- With sensible planning and proper advertisement, a more northern interstate route would not adversely affect development and the Lake Charles economy.

Response 4: Comment noted. FHWA and DOTD, in accordance with USCG request, will complete an updated navigation study and mitigation plan, which will evaluate existing and future navigational clearance needs for property owners north of the bridge, as well as potential economic impacts (see Section 5.1.2, Comment ID# 6). In relation to the proposed project corridor, I-10 was identified by the USDOT in September 2007 as one of six interstate routes to participate in a federal initiative to develop multi-state corridors to help reduce congestion. Accordingly, I-10 joined the USDOT "Corridors of the Future" program aimed at developing innovative national and regional approaches to reduce congestion and improve the efficiency of freight delivery. Moreover, the existing I-10 corridor has been designated as the project study area based on the purpose and need of the proposed project, which is to increase capacity and reduce congestion on I-10 between the I-210 interchanges in the Lake Charles region. Various alternatives, including different potential bridge locations, will be studied during the alternatives development phase of the proposed project, including the no-build alternative. Bridge locations extending north of the existing Calcasieu River Bridge (at distances of approximately 45 feet and 170 feet from the existing bridge centerline) have previously been evaluated and remain under consideration at this time. Alternatives will be evaluated based on design, operational and safety considerations. Results of the evaluation will be documented, including reasons for which any alternatives may have been dropped from further consideration.

Comment 5: Commenter provided numerous statements related to the EDC contamination within the project area, including the following:

- Commenter is concerned that the bridge has become increasingly unstable, not only because of its age but because the ground has been softened by the EDC contamination.
- Commenter stated that he has received 54 pages of material based on his FOIA requests, but that some of the information has been redacted.
- Commenter referenced two previous studies on the effect of EDC on local clays, which show that the regional clay is quickly and severely degraded by EDC, losing its ability to bear weight and slow down the movement of fluids.
- Commenter questioned the exact location of the EDC plume, stating that it may have reached the railroad tracks north of I-10 in the project area.

- Commenter stated that the concentration of the contamination is 90,000 times the LDEQ's Risk Evaluation and Corrective Action Program (RECAP)⁴ trigger level of 5 ppb.
- Commenter has not seen any boring tests for load bearing capacity, and believes these tests exist and would like the highway department to make the results public.
- Based on FOIA documents, commenter cited that as of 2009, the contamination was found within 40 feet of the top of the Chicot Aquifer, and that it is inevitable that the EDC will enter that aquifer.
- Commenter stated that FOIA materials show that the LDEQ is concerned about new bridge pilings hastening EDC contamination into the Chicot Aquifer.
- Commenter states that the EDC plume is moving in a direction contrary to the usual direction of groundwater flow in this region, caused by the heavy draft of the Westlake Municipal Water Supply well pulling the plume downward and northeastward. Accordingly, the commenter feels that Westlake may need to find a new public water supply.
- Commenter discussed the viability of recovery wells, stating that they are only sparingly-efficient and cannot remediate a problem once the contaminants pervade the subsurface to any significant extent.
- Commenter states that given the number of people depending on the groundwater, as well as that there are an additional 16 public water supply wells within two-miles of the study area, a recovery well field location within the bridge ROW would do the most good, as opposed to the construction of a new bridge.
- Commenter has requested FHWA send him more recent and extensive information (e.g., boring data, litigation discussions/status), emphasizing the need for full disclosure related to the EDC contamination.

Response 5: Comment and concerns noted. The EDC contamination and its effects on the proposed project are being investigated as part of the EIS process.

ID #10: Mike Tritico (RESTORE):

Comment: Commenter noted that he received FOIA records regarding the testing/measurement of the EDC contamination plume approximately 2 ½ years ago from the federal and state highway agencies. Commenter also stated that he received approximately 3,000 pages several months prior to this comment of mostly redacted information. Commenter requested un-redacted copies, and as of the week of this Public Scoping Meeting, received 54 pages of FOIA documents, again with redacted information. Commenter requests un-redacted, clear copies of this information.

Response: Based on the comments received, it is unclear from where (what agency/persons) the records with redacted information originated. Information related to the EDC contamination is available via a public records request through the DEQ website at the following web address: https://edms.deq.louisiana.gov/prr/RequestForm.aspx.

ID #11: Perry D. Vincent (Louisiana Radio Communications, Inc.)

Comment 1: Regarding the *Draft Purpose and Need*, the age of the bridge and capacity of the I-10 to I-210 stretch hinders the growth of the City of Lake Charles.

Response 1: Comment noted. The commenter's suggested reasons for the project are included within the *Draft Purpose and Need* for proposed project. The purposes of the proposed project are to increase capacity and reduce congestion, improve roadway deficiencies and traffic operations and to enhance safety. Additionally, the proposed project would provide better access by redesigning interchanges and improving frontage roads, thus supporting economic development in the Lake Charles metropolitan area.

⁴ RECAP = A RECAP cleanup standard is a target concentration that when achieved, will be protective of human health and the environment.

The ability of a proposed alternative to meet the needs of the project and fulfill these stated purposes will serve as evaluation criteria for the advancement of an alternative throughout the EIS process.

Comment 2: The meeting was great; and please publicize throughout the media.

Response 2: Comment noted.

(318) 473-7751

Fax: (318) 473-7626



Natural Resources Conservation Service 3737 Government Street Alexandria, LA 71302

September 11, 2013

Kate Prejean c/o HNTB Corporation kprejean@hntb.com 10000 Perkins Rowe, Suite 640 Baton Rouge, Louisiana 70810

RE: State Project No.: H.003931.2 FAP No.: BR-10-1(212)29 I-10 Calcasieu River Bridge I-10/I-210 West End to I-10/I-210 East End

Calcasieu Parish

Dear Ms. Prejean:

I have reviewed the above referenced project for potential requirements of the Farmland Protection Policy Act (FPPA) and potential impact to Natural Resources Conservation Service (NRCS) projects in the immediate vicinity.

Projects are subject to FPPA requirements if they may irreversibly convert farmland (directly or indirectly) to nonagricultural use and are completed by a federal agency or with assistance from a federal agency. For the purpose of FPPA, farmland includes prime farmland, unique farmland, and land of statewide or local importance. Farmland subject to FPPA requirements can be forest land, pastureland, cropland, or other land, but not water or urban built-up land.

The project map and narrative submitted with your request indicates that the proposed construction areas are within existing right-of-ways and therefore are exempt from the rules and regulations of the Farmland Protection Policy Act (FPPA)—Subtitle I of Title XV, Section 1539-1549. Furthermore, we do not predict impacts to NRCS work in the vicinity.

No identified cultural resources will be impacted by the above mentioned project.

For specific information about the soils found in the project area, please visit our Web Soil Survey at the following location:

http://websoilsurvey.nrcs.usda.gov/

Please direct all future correspondence to me at the address shown above.

Respectfully,

ACTING FOR

Sarah Haymaker State Conservationist

Helping People Help the Land

An Equal Opportunity Provider and Employer

April English

From: Kate Prejean

Sent: Wednesday, September 11, 2013 12:40 PM

To: Noel.Ardoin@LA.GOV
Cc: Joachim Umeozulu
Subject: FW: Status of FAA Filing
Attachments: OE_Letter_Notification.pdf

Noel - I spoke with Alice at FAA today. She gave me some feedback regarding direction to proceed. I have noted a summary of our call below.

The FAA doesn't normally provide opinions of feedback regarding projects such as this. Rather they will do a study 45 days before construction is to begin. Their studies review impacts to either runways or navigational equipment. Based on the location of the bridge currently there is potential to impact navigational equipment, really the signal coming from the equipment. As construction approaches we would need to file for a study to be completed by the FAA team. The study is good for 18 months and you can request one additional 18 month extension. She said that her suggestion is to wait until the DOTD starts design of the project and any issues could be worked out at that time. She indicated she would terminate our study, since nothing should be done now and that it would be consistent with their normal processes. I will file this email as their response to the SOV unless you have any other comments in this regard.

Thanks.

Kate Brady Prejean, P.E.

Transportation Section Manager Direct (225) 368-2818

From: noreply@faa.gov [mailto:noreply@faa.gov]
Sent: Wednesday, September 11, 2013 12:35 PM

To: noel.ardoin@la.gov; Kate Prejean

Subject: Status of FAA Filing

Your filing is assigned Aeronautical Study Number 2013-ASW-6425-OE.

The aeronautical study has been terminated. For additional information, please contact Alice Yett via phone: (817) 321-7752 or email: alice.yett@faa.gov. Please refer to the assigned ASN on all future inquiries regarding this filing.

To ensure e-mail notifications are delivered to your inbox please add <u>noreply@faa.gov</u> to your address book. Notifications sent from this address are system generated FAA e-mails and replies to this address will NOT be read or forwarded for review. Each system generated e-mail will contain specific FAA contact information in the text of the message.

Page 1 of 2 Notice Criteria Tool

ID #2



« OE/AAA

Notice Criteria Tool

The requirements for filing with the Federal Aviation Administration for proposed structures vary based on a number of factors: height, proximity to an airport, location, and frequencies emitted from the structure, etc. For more details, please reference CFR Title 14 Part 77.9.

You must file with the FAA at least 45 days prior to construction if:

- your structure will exceed 200ft above ground level
- your structure will be in proximity to an airport and will exceed the slope ratio your structure involves construction of a traverseway (i.e. highway, railroad, waterway etc...) and once adjusted upward with the appropriate vertical distance would exceed a standard of 77.9(a) or (b) your structure will emit frequencies, and does not meet the conditions of the FAA Co-location Policy

- your structure will be in an instrument approach area and might exceed part 77 Subpart C your proposed structure will be in proximity to a navigation facility and may impact the assurance of navigation signal reception
 your structure will be on an airport or heliport
- filing has been requested by the FAA

If you require additional information regarding the filing requirements for your structure, please identify and contact the appropriate FAA representative using the Air Traffic Areas of Responsibility map for Off Airport construction, or contact the FAA Airports Region / District Office for On Airport construction.

The tool below will assist in applying Part 77 Notice Criteria.

Latitude:	30	Deg	14	M	13.63	S	
Longitude:	93	Deg	14	М	28.72	S	
Horizontal Datum:	NAD83	-					
Site Elevation (SE):	4	(ne	earest fo	oot)			
Structure Height (AGL):	152	(ne	arest fo	ot)			
Traverseway:	No Tra	versev	vay				
	(Additional height is added to certain structures under 77.9(c))						
Is structure on airport:	No						
	Yes						

Results

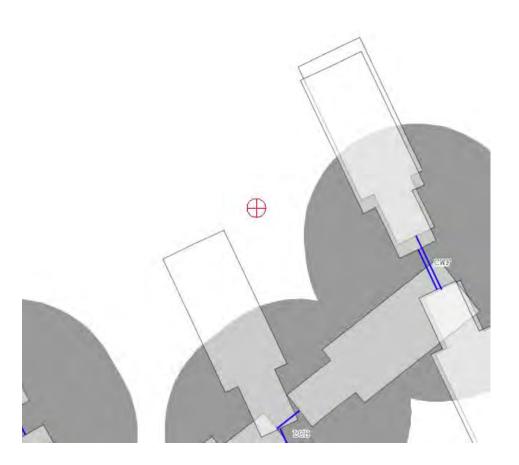
You exceed the following Notice Criteria:

Your proposed structure is in proximity to a navigation facility and may impact the assurance of navigation signal reception. The FAA, in accordance with 77.9, requests that you file.

The FAA requests that you file

Notice Criteria Tool Page 2 of 2

ID #2





UNITED STATES ENVIRONMENTAL PROTECTION AGENCY REGION 6 1445 ROSS AVENUE, SUITE 1200 DALLAS TX 75202-2733

September 12, 2013

Ms. Noel A. Ardoin Environmental Engineer Administrator LA DOTD P.O. Box 94245 Baton Rouge, LA 70804-9245

Dear Ms. Ardoin:

We have received your September 9, 2013, letter requesting our evaluation of the potential environmental impacts which might result from changes to a previously approved project:

Improvements to I-10 Calcasieu River Bridge I-10/I-210 West End to I-10/I-210 East End STP No. H.003931.2 FAP No. BR-10-1(212)29 Calcasieu Parish Lake Charles, Louisiana

The project, proposed for financial assistance through the Louisiana Department of Transportation and Development and the United States Department of Housing and Urban Development funds is located on the Chicot aquifer system which has been designated a sole source aquifer by the EPA. Based on the information provided for the project, we have determined that the project, as proposed, should not have an adverse effect on the quality of the ground water underlying the project site.

This approval of the proposed project does not relieve the applicant from adhering to other State and Federal requirements, which may apply. This approval is based solely upon the potential impact to the quality of ground water as it relates to the EPA's authority pursuant to Section 1424(e) of the Safe Drinking Water Act.

If you did not include the parish, project description, project location or the federal funding agency, please do so in future Sole Source Aquifer correspondence.

If you have any questions on this letter or the sole source aquifer program please contact me at (214) 665-7133.

Sincerely yours,

Michael Bechdol, Coordinator Sole Source Aquifer Program Ground Water/UIC Section

cc: Jesse Means, LDEQ

Kate Prejean/HINTER Comportations (URL) * http://www.epa.gov/region6

U. S. Department of Homeland Security FEMA Region 6 800 North Loop 288 Denton, TX 76209-3698



FEDERAL EMERGENCY MANAGEMENT AGENCY REGION VI MITIGATION DIVISION

(940) 898-5541

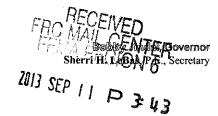
PUBLIC NOTICE REVIEW/ENVIRONMENTAL CONSULTATION

	Federal Aid Project No. BR 10-1 (212)29					
	We have no comments to offer.	\boxtimes	We offer the following comments:			
			H FLOODPLAIN ADMINISTRATOR BE			
			SSIBLE PERMIT REQUIREMENTS FOR , WE WOULD REQUEST PROJECT TO			
			H EO11988 & EO 11990.			
Asst. Calca P.O. Lake pmatt	Mattingly Planning Dir. asieu Parish Drawer 3287 Charles, LA 70602 tingly@cppj.net 721-3606					
REV.	IEWER:					
Mayı	ra G. Diaz					
Flood	lplain Management and Insurance Br	anch				
Mitig	ation Division					

DATE: October 2, 2013



Office of Engineering PO Box 94245 | Baton Rouge, LA 70804-9245 Phone: 225-379-1234



September 9, 2013

Tony Russell
Regional VI Administrator
FEMA Region IV
Federal Emergency Management Agency
800 North Loop 288
Denton, TX 76209

State Project No.: H.003931.2

Federal Aid Project (FAP) No.: BR-10-1(212)29 Project Name: I-10 CALCASIEU RIVER BRIDGE

Project Limits: I-10/I-210 West End to I-10/I-210 East End

Parish: Calcasieu Parish

Re: SOLICITATION OF VIEWS

Early in the planning stages of a transportation project, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist us with the early identification of environmental, economic, and social effects or concerns.

The Louisiana Department of Transportation and Development (DOTD) and the Federal Highway Administration (FHWA) are requesting your input for a transportation project in Calcasieu Parish, Louisiana. The proposed project is the Interstate Highway 10 (I-10) Calcasieu River Bridge (I-10/Interstate Highway 210 [I-210] West End to I-10/I-210 East End), and would add capacity and increase safety along I-10 in the Lake Charles region between the I-210 interchanges, including the Calcasieu River Bridge.

A project description and a map showing the project limits are provided. An Environmental Impact Statement (EIS) will be prepared. During this process, public and agency meetings will be held at various times, including the public scoping meeting which is anticipated to be held in summer 2013. Additional information can be found on the project website at http://www.i10lakecharles.com.

It is requested that you review the attached information and provide us with your views and comments by October 9, 2013. Replies should be addressed to Kate Prejean, c/o HNTB Corporation, 10000 Perkins Rowe, Suite 640, Baton Rouge, LA 70810, or at kbprejean@hntb.com. Please reference the project name and state project number in your response. If you have questions or need additional information, please contact me at (225) 242-4501 or Mr. Joachim Umeozulu at (225) 379-I386. Your assistance in this regard is appreciated.

Sincerely,

Noel A. Ardoin, P.E.

Environmental Engineer Administrator

7. ardon

Attachments



JAY DARDENNE

State of Conisiana

OFFICE OF THE LIEUTENANT GOVERNOR
DEPARTMENT OF CULTURE, RECREATION & TOURISM
OFFICE OF CULTURAL DEVELOPMENT

CHARLES R. DAVIS

PAM BREAUX ASSISTANT SECRETARY

October 2, 2013

Noel Ardoin Engineer Environmental Administrator LDOTD P.O. Box 94245 Baton Rouge, LA 9245

Re: Section 106 Review Request for Additional Information State Project No.: H.003931.2 I-10 Calcasieu River Bridge I-10/I-210 West End to I-10/ I-210 East End Calcasieu Parish, LA

Dear Ms Ardoin:

Thank you for your letter of September 9, 2013, concerning the above-referenced undertaking. We are unable to complete the Section 106 review at this time due to the submittal of insufficient documentation. We will need the following information to complete our review for the aforementioned project:
☐ Name of federal agency, agency involvement (Funding, license\permit, etc. and description of the undertaking (Detailed description of project).
Applicant contact information (Name, address, phone number and email address).
Agency contact information (Name, address, phone number and email address).
Description of the Area of Potential Effects (APE). The APE can be direct or indirect. It is defined as "the geographic area or areas within which an undertaking may cause changes in the character or use of historic properties, if any such properties exist." (Include the latitude\longitude of the undertaking location and APE)
Description of all historic properties within and adjacent to the APE. The historic standing structure is any structure fifty years of age and older. Under Section 106, it is the responsibility of the federal agency or its designee to identify all structures listed or eligible for listing in the National Register of Historic Places.
Detailed project scope of work including design plans.
Map and site plan showing APE and exact location of project undertaking.

Noel Ardoin October 2, 2013 Page 2

Photographs of the entire APE and project location. Photographs of all historic (fifty years of age and older) within the APE. Buildings should be documented showing diagonal views of front and side and rear and opposite side of the building. All photos should be keyed to a site map and project plans if applicable.

If you have any questions, please contact Mike Varnado in the Division of Historic Preservation at (225) 219-4596 or mvarnado@crt.la.gov.

Sincerely,

Pam Breaux

State Historic Preservation Officer

Poreaux

PB:MV:s



Office of Public Works and Water Resources PO Box 94245 | Baton Rouge, LA 70804-9245 ph: 225-379-3005 | fx: 225-379-3002

Bobby Jindal, Governor Sherri H. LeBas, P.E., Secretary

October 3, 2013

STATE PROJECT NO.: None

F.A.P. NO.: None

PROJECT DESCRIPTION: I-10 CALCASIEU RIVER BRIDGE (I-10/ I-210 WEST END TO I-10/ I-

210 EAST END) ROUTE: I-10

PARISH: CALCASIEU

Ms. Noel Ardoin Environmental Engineer Administrator LADOTD P.O. Box 94245 Baton Rouge, LA 70804-9245

Subject: Solicitation of Views

Dear Ms. Ardoin:

The above mentioned proposed project length extends through Calcasieu Parish which includes the City of Lake Charles and the City of Westlake. The project runs in and out of the flood zone and crosses the Kayouchee Coulee which is a designated floodway.

During and after the project, consideration must be given for the occurrence of a base flood inundation. At this time, consideration should also be given to the responsibility for clearing debris and keeping the area cleared so as not to interfere with its function.

In order to assure compliance with the National Flood Insurance Program (NFIP), and ensure that appropriate permits are obtained, please contact the floodplain administrator for Calcasieu Parish, the City of Lake Charles and the City of Westlake. The contact persons are:

Ms. Pam Mattingly, CFM Assist. Director of Planning Calcasieu Police Jury P.O. Drawer 3287 Lake Charles, LA 70602 Telephone No.: 337-721-3600

Mr. Doug Burguieres Assist. Director of Planning City of Lake Charles P.O. Box 900 Lake Charles, LA 70602

Telephone No.: 337-491-1395

Ms. Noel Ardoin October 3, 2013 Page 2

Ms. Terri Hawes, CFM
Permit Clerk
City of Westlake
P.O. Drawer 700
Westlake, LA 70669

Telephone No.: 337-433-0691

We thank you for the opportunity to comment on this project. If you need additional information, please contact our office, (225) 379-3005.

Sincerely,

Susan Veillon, CFM

Floodplain Management Program Coordinator

pc: Ms. Pam Mattingly, CFM Mr. Doug Burguieres Ms. Terri Hawes, CFM



BOBBY JINDAL GOVERNOR

State of Louisiana DEPARTMENT OF NATURAL RESOURCES OFFICE OF CONSERVATION

STEPHEN CHUSTZ SECRETARY

JAMES H. WELSH
COMMISSIONER OF CONSERVATION

October 3, 2013

TO: Ms. Kate Prejean

HNTB Corporation

10000 Perkins Rowe, Suite 640 Baton Rouge, Louisiana 70810

RE: Solicitation of Views

State Project No. H.003931.2 I-10 Calcasieu River Bridge

Calcasieu Parish

Dear Ms. Prejean:

In response to your letter dated September 9, 2013, concerning the referenced matter, please be advised that the Office of Conservation collects and maintains many types of information regarding oil and gas exploration, production, distribution, and other data relative to the petroleum industry as well as related and non-related injection well information, surface mining and ground water information and other natural resource related data. Most information concerning oil, gas and injection wells for any given area of the state, including the subject area of your letter can be obtained through records search via the SONRIS data access application available at:

http://www.dnr.louisiana.gov

A review of our computer records for the referenced project area indicates that there are oil and/or gas wells located near the project area. Additionally, our DNR water well database indicates that registered water wells are present in the vicinity of the project area. Please be aware that unregistered water wells may also be located in the area.

SPN.H. 003931.2 Page Two

The Office of Conservation maintains records of all activities within its jurisdiction in paper, microfilm or electronic format. These records may be accessed during normal business hours, Monday through Friday, except on State holidays or emergencies that require the Office to be closed. Please call 225-342-5540 for specific contact information or for directions to the Office of Conservation, located in the LaSalle Building, 617 North Third Street, Baton Rouge, Louisiana. For pipelines and other underground hazards, please contact Louisiana One Call at 1-800-272-3020 prior to commencing operations. Should you need to direct your inquiry to any of our Divisions, you may use the following contact information:

Division	Contact	Phone No.	E-mail Address
Engineering	Jeff Wells	225-342-5638	jeff.wells@la.gov
Pipeline	Steven Giambrone	225-342-2989	steven.giambrone@la.gov
Injection & Mining	Brad Bourgoyne	225-342-4286	brad.bourgoyne@la.gov
Geological	Mike Kline	225-342-3335	mike.kline@la.gov
Environmental	Gary Snellgrove	225-342-7222	gary.snellgrove@la.gov

If you have difficulty in accessing the data via the referenced website because of computer related issues, you may obtain assistance from our technical support section by selecting Help on the SONRIS tool bar and submitting an email describing your problems and including a telephone number where you may be reached.

Sincerely,

James H. Welsh

Commissioner of Conservation

JHW:MSK:msk

April English

From:

Kate Prejean

Sent:

Friday, October 04, 2013 2:07 PM

To:

April English

Subject:

FW: Lake Charles I-10 Project

Kate Brady Prejean, P.E. Transportation Section Manager

Direct (225) 368-2818

From: Roger Thomas [mailto:rthomas@CPSO.com]

Sent: Friday, October 04, 2013 1:55 PM

To: Kate Prejean

Subject: Lake Charles I-10 Project

Kate, Sheriff Mancuso forwarded to me your DOTD letter dated September 9, 2013---regarding <u>State Project No > H.003931.</u>2. Note: I have reviewed your attached documents.

The bottlenecks as relative to the I-10 Bridge have certainly been an issue for sometime. Glad to see that this future I-10 project will address and fix those issues. Additionally, one of the biggest complaints regarding past construction projects is the I-210 detour route. Drivers who attempted to enter I-210 (ramp) had some difficulty due to large trucks traveling in the right outside lane of I-210. Many motorists voiced their concerns and asked that large trucks be directed to the left inside lane---so that traffic can easily transition onto I-210 from the entrance ramps.

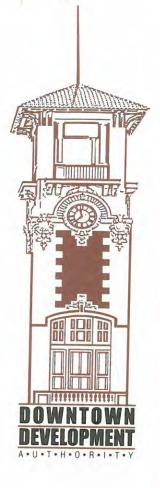
I'm a retired La State Police Sergeant. I was assigned to the Troop D office in Lake Charles for twenty five (25) years-retiring in 2005. During my tenure with the State I worked on several DOTD projects regarding traffic enforcement, along with traffic crash information data. I was one of nine Troopers around the state in 2000 who was selected and assigned to a (DOTD) committee in order to generate a traffic crash manual along with a new traffic crash report. I currently work for the Calcasieu Parish Sheriff's Office, as head CPSO Crash Reconstructionist. I was hired by Sheriff Mancuso in 2005.

Thanks for sending our department the project description letter. Please know that if my office can be of any future assistance---don't hesitate in contacting me. Note: My office is located in the Magnolia Bld downtown Lake Charles, Suite 308 > (Office # > 337-431-1343).

Lastly, Steve Jiles, LC DOTD Head Engineer, is a good friend of mine. Steve is a great guy who has proudly served our state in a leadership role for many years.

Take Care & God Bless!

Roger Thomas



Lori Marinovich
Executive Director
326 Pujo Street
Lake Charles, LA. 70601
337-491-1429
Fax: 337-491-1206
lmarinovich@cityoflc.us
www.cityoflakecharles.com

October 7, 2013

Kate Prejean c/o HNTB Corporation 10000 Perkins Rowe, Ste. 640 Baton Rouge, LA 70810

State Project no.: H.003931.2

Federal Aid project (FAP) No.: BR-10-1(212)29 Project name: I-10 CALCASIEU RIVER BRIDGE

Project Limits: I-10/I-210 East End

Parish: Calcasieu

Re: SOLICITATION OF VIEWS

Dear Ms. Prejean:

Recently, information regarding the above mentioned project was forwarded to the Downtown Development Authority Board by Noel Ardoin, Environmental Engineer Administrator with the DOTD Office of Engineering. This information has been thoroughly reviewed by our Board.

In response to this request for review, please find attached our views and opinions of the proposed project.

Sincerely,

Bill Shearman

Chairman

Downtown Development Authority

City of Lake Charles

Cc: Lori Marinovich

DOWNTOWN DEVELOPMENT AUTHORITY

CITY OF LAKE CHARLES

PARISH OF CALCASIEU

STATE OF LOUISIANA

Resolution 2013 - 010

A RESOLUTION BY THE LAKE CHARLES DOWNTOWN DEVELOPMENT AUTHORITY SUPPORTING THE PROJECT PROPOSED BY THE LOUISIANA DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT AND BY THE FEDERAL HIGHWAY ADMIISTRATION INVOLVING INTERSTATE HIGHWAY 10 (I-10) CALCASIEU RIVER BRIDGE (I-10/INTERSTATE HIGHWAY 210 [I-210] WEST END TO I-10/I-210 EAST END).

WHEREAS, The I-10 corridor is a major economic development driver that supports commerce from coast to coast and this interstate corridor supports a majority of the Lake Charles metropolitan area's commerce including commuter traffic to our region's major industrial developments;

WHEREAS, The City of Lake Charles has embarked on implementation of a major Lakefront Downtown redevelopment plan of the properties adjacent to the shoreline of Lake Charles; has supported and implemented several significant attractions related to our cultural economy including: recreational businesses, historic districts, the historic downtown district, parks and multi-use developments, the proposed National Hurricane Museum and Science Center and our Convention and Visitor's Bureau, all of which depend on direct access from both the east and westbound I-10 exits located closest to the base of the Calcasieu River bridge;

WHEREAS, The bridge replacement proposals include updates of the Interchange Justification Report for the 1-10 eastbound exit ramp in conjunction with the evaluation of the 1-10 Bridge replacement project. This IJR completes the east bound Ryan Street exit including the designated Ryan Off ramp Phased approach- Phase 2 flyover;

WHEREAS, Vehicular circulation is currently constricted to a one-way turn- around feeder road scheme which directs west bound traffic underneath the existing bridge; and there are limited river crossings that can accommodate existing traffic during peak hours and traffic delays when one bridge is out of service;

WHEREAS, The Downtown Historic District and the National Register Charpentier Historic District are accessed directly from the affected corridor.

NOW THEREFORE, BE IT RESOLVED, in regular session convened on October 7, 2013 that:

1. The DDA encourages LADOTD to hold a public meeting as soon as practical to explain the status of the project, the anticipated timeline and seek public input on the matters addressed herein;

2. The DDA requests that the LA DOTD keep the existing bridge open for traffic during all construction phases, including access to the businesses and activities dependent on the "turn around" style feeder road;

3. The DDA supports the recommendation that any land removed from DOTD use along the frontage road on the south side of the I-10 bridge replacement project be reconfigured for more convenient local access and/or allocated to the City to be used for reconfiguring access to adjacent City owned lakefront properties to enhance economic development opportunities;

4. The DDA recommends that the bridge improvements and designs include Transportation Enhancement considerations including historic district "brown" signage, gateway signage, appropriate landscaping, and other artistically inspired interpretations of highway features be included as part of the bridge construction to enhance the gateway into the Downtown Lakefront Development District of the City of Lake Charles.

NOW THEREFORE, BE IT RESOLVED BY THE DOWNTOWN DEVELOPMENT AUTHORITY OF THE CITY OF LAKE CHARLES THAT IT DOES HEREBY APPROVE AND SUPPORT THE PROJECT PROPOSED BY THE LOUISIANA DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT AND BY THE FEDERAL HIGHWAY ADMIISTRATION INVOLVING INTERSTATE HIGHWAY 10 (I-10) CALCASIEU RIVER BRIDGE (I-10/INTERSTATE HIGHWAY 210 [I-210] WEST END TO I-10/I-210 EAST END).

PASSED AND ADOPTED at Lake Charles, Louisiana on this 7th day of October, 2013.

Bill Shearman, Chairman

Downtown Development Authority Lakefront Development Subdistrict

City of Lake Charles

ATTEST:

Lori Marinovich, Executive Director Downtown Development Authority Lakefront Development Subdistrict

City of Lake Charles



CITY OF LAKE CHARLES OFFICE OF THE CITY COUNCIL

P.O. Box 1178 Lake Charles, LA 70602-1178 337-491-1290 • FAX 337-491-1463

October 7, 2013

COUNCIL MEMBERS

Mary Morris District A

Luvertha W. August District B

> Rodney Geyen District C

John Ieyoub District D

Stuart Weatherford District E

Dana Carl Jackson District F

> Mark Eckard District G

HNTB Corporation

Attention: Ms. Kate Prejean 10000 Perkins Rowe, Suite 640 Baton Rouge, LA 70810

Re: State Project No. H.003931.2

Project Name: I-10 Calcasieu River Bridge

Solicitation of Views

Dear Ms. Prejean:

In response to a letter from Noel A. Ardoin, P.E. with the Louisiana Department of Transportation & Development dated September 9, 2013, attached please find a copy of Resolution number 222-13 adopted by the Lake Charles City Council at its regular meeting on October 2, 2013. The resolution is in response to the solicitation of views and outlines the City's views and comments which should be addressed with regard to the I-10 Calcasieu River Bridge project in Lake Charles, Calcasieu Parish, Louisiana.

Your support of the Lake Charles City Council in this matter is greatly appreciated.

Sincerely,

Lynn F. Thibodeaux Clerk of the Council

Enclosure

cc: Noel A. Ardoin, P.E.

LA Department of Transportation & Development

Hilodean



City of Lake Charles

Signature

Resolution Number: 222-13

326 Pujo Street P.O. Box 1178 Lake Charles, LA 70602-1178

A resolution responding to the solicitation of views request from Louisiana Department of Transportation and the Federal Highway Administration on a proposed transportation project, I-10 Calcasieu River Bridge.

WHEREAS, the I-10 corridor is a major economic development driver that supports commerce from coast to coast and this corridor supports a majority of the Lake Charles metropolitan area's commerce including commuter traffic to our region's major industrial developments; and

WHEREAS, the City Council does hereby reaffirm Resolution No. 161-11 to the State of Louisiana, Department of Transportation and Development which expresses Council approval for the proposed sequencing of construction in connection with the I-10 Ryan Street Exit Ramp project including the proposed procedure for reevaluating the I-10 eastbound exit ramp in conjunction with the evaluation of the I-10 Bridge Replacement project and the planning of an updated Interchange Justification Report to complete the eastbound Ryan Street exit including the designated Ryan Off Ramp Phased approach - Phase 2 flyover; and

WHEREAS, the City of Lake Charles has embarked on implementation of a major redevelopment plan of the properties adjacent to the shoreline of Lake Charles and completed several significant attractions related to our cultural economy including recreational businesses, historic districts, the historic downtown district and our Convention and Visitor's Bureau, all of which are accessed from the I-10 exits located directly adjacent to the base of the Calcasieu River Bridge and vehicular circulation is currently constricted to a one-way turn-around feeder road scheme which directs west bound traffic underneath the existing bridge; and there are limited river crossings that can accommodate existing traffic during peak hours and traffic delays when one bridge is out of service; and

WHEREAS, the eastern bank of the river and adjacent neck of Lake Charles is the gateway into the City of Lake Charles and hosts natural and embellished features such as a white sandy beach; and

WHEREAS, the Calcasieu River bridge is included in the State of Louisiana, Department of Transportation and Development Historic Bridge Inventory which is the first major component in the Louisiana Department of Transportation and Development (LADOTD)'s effort to evaluate its statewide inventory of historic bridges constructed through 1970 and the LDH Bridge Section had a strong tradition of designing most of the State's major crossings and has also built minor structures; for example, in 1948-49 alone it built 140 bridges and among the 130 large river crossing bridges under construction or completed in the postwar period, was included the Bridge at Lake Charles over the Calcasieu River (extant), underway in 1948-49 and completed in 1952; and

WHEREAS, the region expects unprecedented industrial expansions over the next several years, which will include growth in number of households and needs for goods and other services required to serve these industries.

NOW BE IT THEREFORE RESOLVED, by the Lake Charles City Council in regular session convened on October 2, 2013 and with the concurrence of Mayor Randy Roach as attested below, as follows:

- 1. The recitations set forth above are hereby adopted by reference; and
- 2. The City requests that the State of Louisiana, Department of Transportation and Development keep the existing bridge open for traffic during all construction phases; and

- 3. The City requests that the iconic features including bridge railings incorporated into the design of the current bridge structure and the cross pistols which have become a symbol within the community, be preserved and reused wherever possible, or otherwise duplicated, along with other features and included within the new bridge design to retain the historic character of the structure; and
- 4. The City encourages the State of Louisiana, Department of Transportation and Development to hold a public meeting as soon as practical to explain the status of the project, the anticipated timeline, and seek public input on the matters addressed herein; and
- 5) The City requests that any land removed from Department of Transportation and Development use along the frontage road on the south side of the I-10 bridge replacement project be reconfigured for more convenient local access and/or allocated to the City or adjacent landowners to be used for reconfiguring access to adjacent City owned lakefront properties to enhance economic development opportunities; and
- 6) The City requests that the bridge improvements and designs include transportation enhancement considerations including gateway signage, appropriate landscaping, and other artistically inspired interpretations of highway features be included as part of the bridge construction to enhance the gateway into the cities of Lake Charles and Westlake; and
- 7) The City requests the State of Louisiana, Department of Transportation and Development address the safety considerations and revisit the curve and the approach to the Opelousas Street exit to eliminate traffic safety hazards.

At a meeting of the City Council on 10/2/2013, this Resolution was adopted as amended by the following vote:

For: John leyoub, Mark Eckard, Dana Carl Jackson, Rodney Geyen, Mary Morris, Stuart Weatherford and Luvertha August

Passed and Adopted $\underline{\mathscr{D}}$

Luvertha August, President

or Presiding Officer

Attest Jmm

Date /() / -/

Date 10-2-1-

Lynn F. Thibodeaux Clerk of the Covincil

Approved by

Randy Roa¢h Mayor

City of Lake Charles, Louisiana

Date 10-7-13



October 8, 2013

Kate Prejean HNTB Corporation 10000 Perkins Rowe Suite 640 Baton Rouge, LA 70810

Re: State Project No.: H.003931.2

Federal Aid Project (FAP) No.: BR-10-1(212)29 Project Name: I-10 CALCASIEU RIVER BRIDGE

Project Limits: I-10/I-210 West End to I-10/I-210 East End

Parish: Calcasieu Parish

Subject: Solicitation of Views State Project NO. H.003931.2

Dear: Ms. Prejean

IMCAL, acting in its capacity as the Metropolitan Planning Organization for the Lake Charles Urbanized Area, supports this project. This project would add capacity and increase safety along I-10 in the Lake Charles region between the I-210 interchanges, including the Calcasieu River.

However, we would like to bring up past concerns and reiterate a few resolutions and options previously discussed in regards to the on-going projects stated above. The Transportation Committee, on the 11th Day of December 2008 passed Resolution #2037, adopting option four (4) of the I-10 Bridge Options presented by LADOTD on October 15, 2008. IMCAL, on the 27th Day of April 2009 passed Resolution # 2040m adopting option four (4) of the I-10 Bridge Option presented by Secretary Dr. William Anker.

Option Four (4) Reads as follows:

- Replace bridge with parallel bridge-build new bridge next to existing bridge while maintaining traffic on existing bridge
- Will address capacity, user costs and safety concerns associated with existing bridge
- Will maintain traffic on I-10
- Will avoid most impacts to I-210 regular users and community along I-210 although some traffic will shift to I-210 at certain times during construction
- Most expensive of options

- · Will require longest construction period
- Improvements to I-210 interchanges and mainline will not be address as part of the project and will require extra funding.

If any questions arise concerning this matter arise or you would like to receive a copy of Resolutions mentioned above, please contact IMCAL at (337) 433-1771

Sincerely,

Grant Bush

Executive Director, IMCAL

MPO Transportation Director

Enclosed (3) Resolution # 2037

Resolution # 2040

I-10 Calcasieu River Bridge (Agenda)

Copy: Mayor Randy Roach

RESOLUTION NO. 2040

BE IT RESOLVED. I. Chris Duncan. Chairman of the Board for the Imperial Calcasieu Regional Planning and Development Commission (IMCAL), do hereby certify that an IMCAL Full Board meeting was duly convened in Lake Charles, Louisiana, at 6:30 P.M. on April 27, 2009, and the following resolution was adopted:

WHEREAS, a resolution to urge and request Senator Landrieu, Senator Vitter and Congressman Boustany to include the replacement of the Interstate (1) 10 Galcasieu River Bridge in the next Federal Highway Bill (Commonly referred to as SAFETEA-LU (Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users) and to express support for the construction of a new bridge in an approved right-of-way location north of the existing I-10 Bridge. It is pertinent to consider including the City of Westlake's Sampson Street interchange, a particularly crucial component which falls within the I-10 Bridge boundary:

WHEREAS, this project has serious long term public safety, economic and social implications and failure to fund this project not only poses a serious threat to the traveling public, it also threatens the flow of trade and commerce among the Southern states. The 2003 "National I-10 Freight Corridor Study" documented the importance of the I-10 corridor to the nation's economy. The report pointed out that the corridor's economic value exceeds \$1.38 trillion and directly contributes to the amployment of 10.4 million employees. The I-10 Calcasieu River Bridge was one of the bridges cited in the report as being in substandard condition and in need of replacement or repair. Currently, the I-10 Calcasieu River Bridge is considered safe for travel, but is more than fifty years old and ranked structurally deficient by the Louisiana Department of Transportation and Development. Under the U.S. Department of Transportation's rating system, the I-10 Calcasieu River Bridge actually scored lower than the I-35W Bridge in Minneapolis;

WHEREAS, this resolution acknowledges the Lake Charles Urbanized Area Metropolitan Planuing Organization's Transportation Policy Committee approval of Resolution # 2037 for option four of the I-10 Bridge options 1-4 presented by Secretary Dr. William Ankner, LADOTO, and passed on December 2008:

WHEREAS, this resolution acknowledges the Lake Charles Urbanized Area Metropolitan Planning Organization's Transportation Policy Committee decision that a bridge height recommendation of 73 feet presented by the Louisiana Department of Transportation and Development is acceptable and was resolved on December 2007; and

BE IT RESOLVED that the Full Board for the Imperial Calcasieu Regional Planning and Development Commission does hereby approve and adopt the above-mentioned and submit to urge and request the inclusion for the replacement of the I ID Calcasieu River Bridge in the next Federal Highway Bill.

PASSED AND ADOPTED in Lake Charles, Louisiana, on this the

Mr. Chris Duncan. Chairman [MGAL Full Board of Directors

Imperial Calcasien Regional Planning and Development Commission

Mr. James Porter, Executive Director

Imperial Calcasieu Regional Planning and Development Commission

TRANSPORTATION POLICY COMMITTEE

RESOLUTION # 2037

Adoption of Option Four of the I-10 Bridge Options Presented by LADOTD, October 15, 2008

I. Ron LeLeux, Chairman of the Transportation Policy Committee, do hereby certify that a meeting of the Transportation Policy Committee was duly convened in Lake Charles, Louisiana, at 9:00 a.m. on December 11, 2008, and the following resolution was adopted:

WHEREAS, the Transportation Policy Committee of the Lake Charles Urbanized Area Metropolitan Planning Organization has reviewed and approved option four of the I-10 Bridge options presented by Secretary Dr. William Ankner, LADOTD, on October 15, 2008; and

WHEREAS, the Transportation Policy Committee has determined that option four is the most suitable option of the four options proposed by LADOTD on October 15, 2008, and with this action demonstrate its cooperation to ensure full coordination with the applicable metropolitan and statewide planning process; and

WHEREAS, the Transportation Policy Committee has recommended approval and adoption of said option four and processes to the Imperial Calcasieu Regional Planning Commission (IMCAL) serving in its capacity as the Lake Charles Urbanized Area Metropolitan Planning Organization;

BE IT RESOLVED that the Transportation Policy Committee does hereby approve and adopt the above-mentioned option and processes and directs the staff of IMCAL to submit said option to the appropriate federal and state agencies, and to comply with any minor revisions necessary to facilitate submission requirements.

Signed and executed this 11th day of December 2008.

Mayor Ron LeLeux, Chairman Transportation Policy Committee

Lake Charles Urbanized Area Metropolitan Planning Organization

AGENDA

I-10 CALCASIEU RIVER BRIDGE CHAMBER OF COMMERCE

October 15, 2008

A. Options for I-10 Bridge

- Do nothing-make minor repairs and keep bridge in service as long as possible
 - -cheapest alternative
 - -commerce will be forced onto I-210 as load limits are posted and lowered
 - -bridge will eventually have to be closed resulting in substantial community disruption
- 2) Major repairs-make major structural repairs to existing bridge
 - -second least expensive
 - -keeps existing bridge in service for 20 to 30 years
 - -repairs can be phased
 - -capacity, user costs, and safety concerns would not be addressed
- 3) Replace bridge at current location-close and demolish existing bridge, use I-210 as mainline during construction
 - -new bridge can be constructed in 3 years
 - -make upgrades to I-210 interchanges and mainline to handle traffic
 - -will address capacity, user costs, and safety concerns associated with existing bridge.
 - -will adversely impact I-210 regular users and community along I-210 -will adversely affect some commercial establishments along I-10 between I-210 W and I-210 E.
- 4) Replace bridge with parallel bridge-build new bridge next to existing bridge while maintaining traffic on existing bridge.
 - -will address capacity, user costs and safety concerns associated with existing bridge
 - -will maintain traffic on I-10
 - -will avoid most impacts to I-210 regular users and community along I-210 although some traffic will shift to I-210 at certain times during construction
 - -most expensive of options was a long grante with
 - -will require longest construction period
 - -improvements to I-210 interchanges and mainline will not be addressed as part of the project and will require extra funding.



DEPARTMENT OF CULTURE, RECREATION & TOURISM

OFFICE OF STATE PARKS

State of Contistanta
Office of the Lieutenant Governor

CHARLES R. DAVIS

STUART JOHNSON, PH.D. ASSISTANT SECRETARY

JAY DARDENNE LIEUTENANT GOVERNOR

October 8, 2013

Mr. Noel A. Ardoin, P.E. Louisiana DOTD 1201 Capitol Access Road Baton Rouge, LA 70802

Re: Inte

Interstate 10 (I-10) Calcasieu River Bridge (I-10/I-210 West End to I-10/I-210 East End)

Calcasieu Parish

Dear Mr. Ardoin,

I am in receipt of your letter of October 7, 2013 forwarded to me by Stuart Johnson, Assistant Secretary of the Office of State Parks. Your letter was passed to me for consideration in regards to potential conflicts with existing Land and Water Conservation Fund (LWCF) projects in proximity to the proposed project.

The Division of Outdoor Recreation within the Office of State Parks is tasked with administration of the LWCF for Louisiana. All areas receiving assistance through the LWCF are encumbered through Section 6(f) and must remain an outdoor recreation facility in perpetuity. Any infringement upon the boundaries of those sites with development other than outdoor recreation requires a conversion in accordance with the LWCF Act.

In regards to this proposed project, LWCF Project #22-00201 Lakefront Recreation Area is the only facility in proximity to the project boundaries identified in your letter. This project is along the I-10 Service Road on the south side of I-10 on the beach at the lake and in immediate proximity to the Lake Charles/Southwest Louisiana Visitors and Convention Bureau. It does not appear this location will be disrupted by the proposed project.

While I will be unable to attend the Agency Scoping Meeting for the project on October 24, 2013, I trust you will appropriately consider restrictions of the LWCF in regards to development of this project. I welcome any further questions you might have in this matter as you move forward.

Sincerely,

Cleve Hardman

Director of Outdoor Recreation



Jena Band of Choctaw Indians

P. O. Box 14 • Jena, Louisiana 71342-0014 • Phone: 318-992-2717 • Fax: 318-992-8244

October 10, 2013

Louisiana Department of Transportation and Development Attention: Ms. Noel Ardoin P.O. Box 94245 Baton Rouge, LA 70804-9425

RE:

Interstate 10 Calcasieu River Bridge

Interstate 210 West End to I-10/ I-210 East End Invitation to Become a Participation Agency

Dear Madam,

We defer to the Coushatta tribe of Louisiana to determine if this project will impact culturally significant areas.

Should you have any questions, please contact Mrs. Dana Masters, THPO Officer/Cultural Director, at 318-992-1205 or danamasters@aol.com.

Sincerely

Dana Masters

THPO/ Cultural Director

Dane moste

Council Member



Office of the Secretary
PO Box 94245 | Baton Rouge, LA 70804-9245
ph: 225-379-3005 | fx: 225-379-3002

Bobby Jindal, Governor Sherri H. LeBas, P.E., Secretary

October 11, 2013

STATE PROJECT NO.: H.003931.2

F.A.P. NO: BR-10-1(212)29

NAME: I-10 CALSADIEU RIVER BRIDGE

LOCATION: I-10/1-210 WEST END TO I-10/I-210 EAST END

PARISH: CALCASIEU

Ms. Kate Prejean HNTB Corporation 10000 Perkins Rowe, Suite 640 Baton Rouge, LA 70810

Subject: Solicitation of Views

Dear Ms. Prejean:

The above mentioned project extends through three (3) communities, which are Calcasieu Parish, the City of Lake Charles, and the City of Westlake, running in and out of special flood hazard areas including a designated floodway.

During the construction, there must be allowance for the adequate flow of water and assurance that there will be no back up of water. There must be no instance of the creation of flooding where there was no flooding prior to construction. At this time, consideration must be given to the responsibility for cleaning debris and keeping the surrounding area clear so as not to interfere with its function.

In order to assure compliance with the requirements for the National Flood Insurance Program (NFIP), and ensure that appropriate permits are obtained, please contact the floodplain administrator for all three (3) communities. Their contact information is as follows: Calcasieu Parish, Ms. Pam Mattingly, P.O. Drawer 3287, Lake Charles, LA, 70602, telephone no. 337-721-3600; for the City of Lake Charles, Mr. Doug Burguieres, P.O. Box 900, Lake Charles, LA 70602, telephone no. 337-491-1395; and for the City of Westlake, Ms. Terri Hawes, P.O. Drawer 700, Westlake, LA 70669, telephone no. 337-433-0691.

Ms. Kate Prejean October 11, 2013 Page No. 2

We thank you for the opportunity to comment on this project. If you need additional information, please contact our office, (225) 379-3005.

Sincerely,

Pam Lightfoot, CFM

Floodplain Management Program Coordinator

Enclosures

pc: Ms. Pam Mattingly Mr. Doug Burguieres Ms. Terri Hawes



REGION 6 1445 ROSS AVENUE, SUITE 1200 DALLAS TX 75202-2733

October 17, 2013

Kate Prejean c/o HNTB Corporation 10000 Perkins Rowe Suite 640 Baton Rouge, Louisiana 70810

Dear Ms. Prejean:

The U.S. Environmental Protection Agency (EPA) has completed your request for a solicitation of views concerning the I-10 Calcasieu River Bridge (State Project No. H.003931.2). The comments that follow are being provided relative to the EPA's 404(b)(1) Guidelines for Specification of Disposal Sites for Dredged or Fill Material (40 CFR Part 230).

Our preliminary review revealed that jurisdictional waters of the U.S. do occur along the proposed project route. At this time, the EPA recommends coordination with the U.S. Army Corps of Engineers at the New Orleans District Office to verify which permits, if any, are needed. The EPA will review the project to ensure that impacts to waters of the U.S. are minimized, and any unavoidable impacts are fully compensated. Thanks for the opportunity to review the proposed project. If you have any questions or would like to discuss the issue further, please do not hesitate to contact me at Gutierrez.raul@epa.gov or 214-665-6697.

Sincerely yours,

Raul Gutierrez, Ph.D.

Wetlands Section

Water Quality Protection Division

BOBBY JINDAL GOVERNOR



PEGGY M. HATCH SECRETARY

State of Louisiana

DEPARTMENT OF ENVIRONMENTAL QUALITY OFFICE OF ENVIRONMENTAL COMPLIANCE

November 5, 2013

Ms. Noel Ardoin, P.E. Louisiana Department of Transportation and Development P.O. Box 94245 Baton Rouge, LA 70804-9245

RE: Solicitation of Views
I-10 Calcasieu River Bridge
(AI Number 2538)
Calcasieu Parish

Dear Ms. Ardoin:

The Department of Environmental Quality (LDEQ), has received your request for comments on the above referenced project. After reviewing your request, the Department has no objections based on the information provided in your submittal. For your information, the following general comments have been included.

- Please take any necessary steps to obtain and/or update all necessary approvals and environmental permits regarding this proposed project.
- If your project results in a discharge to waters of the state, submittal of a Louisiana Pollutant Discharge Elimination System (LPDES) application may be necessary.
- If the project results in a discharge of wastewater to an existing wastewater treatment system, that wastewater treatment system may need to modify its LPDES permit before accepting the additional wastewater.
- All precautions should be observed to control nonpoint source pollution from construction activities. LDEQ has stormwater general permits for construction areas equal to or greater than one acre. It is recommended that you contact the LDEQ Water Permits Division at (225) 219-9371 to determine if your proposed project requires a permit.
- If any of the proposed work is located in wetlands or other areas subject to the jurisdiction of the U.S. Army Corps of Engineers, you should contact the Corps directly regarding permitting issues. If a Corps permit is required, part of the application process may involve a water quality certification from LDEQ.
- All precautions should be observed to protect the groundwater of the region.

Ms. Ardoin November 5, 2013 Page 2

• If any solid or hazardous wastes, or soils and/or groundwater contaminated with hazardous constituents are encountered during the project, notification to LDEQ's Single-Point-of-Contact (SPOC) at (225) 219-3640 is required. Additionally, precautions should be taken to protect workers from these hazardous constituents.

Based on LDEQ's previous involvement in the area potentially impacted by this project, we offer these specific comments:

- In the absence of final determinations of piling locations and LDOTD-proposed depths, it is not possible to provide specificity in recommending depths which would be protective of the subsurface environment, i.e., avoiding exacerbation of current groundwater conditions.
- As per the correspondence from LDEQ to LDOTD dated November 19, 2009, LDEQ would have no objection to piling depths of 75' below current existing grade or less north of the current I-10 footprint. While pilings may be placed within contamination zones, this depth would not penetrate zones currently uncontaminated by EDC.
- South of the current I-10 footprint, no piling should exceed a depth of 40' below current existing grade with the exception of the following: Using a line drawn from CPT18, CPT7, and a point 50' due east of I8 as a reference, there would be no depth restrictions to the east of this line (See EDMS Document ID # 6754900 for reference points).

Please contact Laura LeBouef by phone at (225) 219-3033 or by email at laura.lebouef@la.gov with any further questions or concerns.

Sincerely,

Cheryl Sonnier Nolan, Assistant Secretary Office of Environmental Compliance

lq1

c: Imaging Operations – GW



UNITED STATES ENVIRONMENTAL PROTECTION AGENCY Region 6

1445 Ross Avenue, Suite 1200 Dallas, TX 75202-2733

November 14, 2013

Noel Ardoin Louisiana Department of Transportation and Development P.O. Box 94245 Baton Rouge, LA 70804-9245

Subject: Scoping Comments for Interstate 10 (I-10) Calcasieu River Bridge

Dear Ms. Ardoin,

The Region 6 office of the U.S. Environmental Protection Agency (EPA) has reviewed your letter dated October 7, 2013 requesting EPA's attendance at Agency Scoping Meetings and requesting scoping comments. We understand a Notice of Intent to Prepare an Environmental Impact Statement (EIS) was published on July 25, 2013. The proposed project is approximately nine miles in length and includes alternatives for I-10 in the Lake Charles Region between the I-210 interchanges, including the Calcasieu River Bridge. We are providing these scoping comments pursuant to the National Environmental Policy Act (NEPA), Council on Environmental Quality (CEQ) regulations (40 CFR Parts 1500-1508) and Section 309 of the Clean Air Act.

To assist in the scoping process for this project, we have identified several issues for your attention in the preparation of the EIS and enclosed detailed scoping comments for your consideration. EPA is most concerned about the following issues: alternatives development, impacts to water, including wetlands, biological resources, invasive species management, habitat protection, air quality, indirect and cumulative impacts, climate change, environmental justice, and mitigation.

EPA appreciates the opportunity to review and provide comments on the proposed project. Please send one hard copy of the draft EIS and four digital copies to this office when completed and submitted for public comment. When you are ready to file the draft EIS with EPA, you may now electronically file it by using our *e-NEPA Electronic Filing* website http://www.epa.gov/compliance/nepa/submiteis/index.html. If you have any questions or concerns, please contact me at 214-665-8006.

Sincerely,

Rhonda Smith

Chief, Office of Planning and

Coordination

Enclosure

DETAILED SCOPING COMMENTS FOR THE NOTICE OF INTENT (NOI) FOR THE FEDERAL HIGHWAY ADMINISTRATION AND THE LOUISIANA DEPARTMENT OF TRANSPORATION AND DEVELOPMENT TO PREPARE AN ENVIRONMENTAL IMPACT STATEMENT FOR THE PROPOSED I-10 CALCASIEU RIVER BRIDGE

Proposed Project

In compliance with the National Environmental Policy Act of 1969 (NEPA), as amended, the Federal Highway Administration (FHWA) and the Louisiana Department of Transportation and Development (LADOTD) intends to prepare an Environmental Impact Statement (EIS) to evaluate the impacts of the I-10 Calcasieu River Bridge project.

Statement of Purpose and Need

The EIS should clearly identify the underlying purpose and need to which FHWA/LADOTD is responding in proposing the alternatives¹. The purpose of the proposed action is typically the specific objectives of the activity, while the need for the proposed action may be to eliminate a broader underlying problem or take advantage of an opportunity.

Alternatives Analysis

NEPA requires evaluation of reasonable alternatives, including those that may not be within the jurisdiction of the lead agency². A robust range of alternatives will include options for avoiding significant environmental impacts. The EIS should "rigorously explore and objectively evaluate all reasonable alternatives" by developing a defined screening process. The screening process should rate each alternative against a set of pre-determined criteria. Each alternative should then be analyzed for its level of impact on a resource, e.g. no effect, negligible effect, minor effect, major effect, significant effect. Only that alternative that effectively meets or best meets all of the screening criteria should be recommended as the preferred alternative. The EIS should provide a clear discussion of the reasons for the elimination of alternatives which are not evaluated in detail.

Section 1502.14(d) requires the alternatives analysis to "include the alternative of no action." No Action means the proposed activity would not take place, and the resulting environmental effects from taking no action would be compared with the effects of permitting the proposed activity or an alternative activity to go forward.

^{1 40} CFR 1502.13

² 40 CFR Section 1502.14(c)

³ 40 CFR 1502.14(a)

Affected Environment

This section should provide information on the existing resources and condition of the natural and built environment. It is a description of baseline conditions. These baseline conditions provide the context for evaluating environmental consequences and should include historical cumulative effects to the extent feasible.

Environmental Consequences

A majority of EISs contain a well-written section on the affected or existing environment. However, where most EISs fall short is the discussion of the environmental consequences of the proposed project. An analysis should follow an equation. First, what is the action? Second, what is the intensity or extent of impacts? Third, is that significant? The EIS must answer that question and prove that the action is not significant by providing reasons. If the action is significant and adverse, then the EIS must provide appropriate measures to avoid, minimize, or mitigate those impacts.

Water Resources

Water Supply and Water Quality

Public drinking water supplies and/or their source areas often exist in many watersheds. Source water is water from streams, rivers, lakes, springs, and aquifers that is used as a supply of drinking water. Source water areas are delineated and mapped by the state for each federally-regulated public water system. The 1996 amendments to the Safe Drinking Water Act require federal agencies to protect sources of drinking water for communities. The EIS should address the potential effects of project discharges, if any, on surface water quality. Specific discharges should be identified and potential effects of discharges on designated beneficial uses of affected waters should be analyzed.

Stormwater

The EIS should describe the original (natural) drainage patterns in the project locale, as well as the drainage patterns of the area during project operations. Also, the EIS should identify whether any components of the proposed project are within a 50 or 100-year floodplain. The EIS should note that, under the Clean Water Act (CWA), any construction project disturbing a land area of one or more acres requires a construction stormwater discharge permit.

Waters of the United States, including Wetlands

The project applicant should coordinate with the U.S. Army Corps of Engineers (USACE) to determine if the proposed project requires a Section 404 permit under the CWA. Section 404 regulates the discharge of dredged or fill material into waters of the United States, including wetlands and other *special aquatic sites*. The EPA recommends the EIS include a jurisdictional delineation for all waters of the U.S., including ephemeral drainages, in accordance with the 1987 *Corps of Engineers Wetlands Delineation Manual* and the December 2006 *Region*

Interim Regional Supplement to the Corps of Engineers Wetland Delineation Manual. A jurisdictional determination will confirm the presence or absence of waters of the U.S. in the project area and help determine whether or not the proposed project would require a Section 404 permit. If a permit is required, the EPA will review the project for compliance with Federal Guidelines for Specification of Disposal Sites for Dredged or Fill Materials (40 CFR 230), promulgated pursuant to Section 404(b)(1) of the CWA. EPA encourages on-going coordination with the USACE during the planning, scoping, and construction phases.

The EIS should demonstrate planning efforts to avoid, minimize, and compensate for wetland losses associated with dredging, dredged material disposal, and other construction and operation activities. The EIS should also include an analysis of the potential for contaminated sediments to adversely impact the aquatic environment during construction and operation of the terminal.

Impaired Waters

The CWA requires states to develop a list of impaired waters that do not meet water quality standards, establish priority rankings, and develop action plans, called Total Maximum Daily Loads (TMDL), to improve water quality. The EIS should provide information on CWA Section 303(d) impaired waters in the project area, if any, and efforts to develop and revise TMDLs. The EIS should describe existing restoration and enhancement efforts for those waters, and any mitigation measures that will be implemented to avoid further degradation of impaired waters.

Sole Source Aquifer

EPA defines a sole or principal source aquifer as an aquifer that supplies at least 50 percent of the drinking water consumed in the area overlying the aquifer. These areas may have no alternative drinking water source(s) that could physically, legally, and economically supply all those who depend on the aquifer for drinking water. For convenience, all designated sole or principal source aquifers are referred to as "sole source aquifers" (SSAs). The EIS should identify if the project lies over a SSA and how the project may affect the aquifer.

Section 4(f) and Section 6(f)

Section 4(f) of the Department of Transportation Act of 1966 includes a special provision that protects publicly owned parks, recreational areas, wildlife and waterfowl refuges, or public and private historical sites from being acquired for transportation projects. Section 6(f) of the Land and Water Conservation Fund Act (LWCF) states that either the land or park appurtenances acquired with LWCF funds cannot be eliminated or acquired without coordination with the National Park Service and mitigation that replaces the eliminated items. If any 4(f) or 6(f) properties are within or near project corridors, they should be documented. Please be aware that vibration and noise can also adversely impact 4(f) and 6(f) properties.

Project Specific Locations (PSL)

PSLs outside the right-of-way that would be established to support construction of the roadway such as borrow and disposal sites, staging and storage areas, and concrete and aggregate plants, must be identified as direct impacts. The project team should strive to locate PSLs so they avoid environmentally sensitive areas. Ideally, PSLs should be located in previously disturbed, upland areas.

Biological Resources, Habitat, and Wildlife

The EIS should identify all petitioned and listed threatened and endangered species and critical habitat that might occur within the project area. The EIS should identify and quantify which species or critical habitat might he directly, indirectly, or cumulatively affected by each alternative and mitigate impacts to these species. EPA recommends that FHWA/LADOTD consult with the U.S. Fish and Wildlife Service and the Louisiana Department of Wildlife and Fisheries to ensure that current and consistent surveying, monitoring, and reporting protocols are applied in protection and mitigation efforts. The EIS should identify and quantify vegetation types within the project corridor. In addition, the EIS should identify and evaluate impacts to wetland and special aquatic sites, as defined by the CWA Section 404.

Invasive Species

Executive Order 13112, *Invasive Species* (February 3, 1999), mandates that federal agencies take actions to prevent the introduction of invasive species, provide for their control, and minimize the economic, ecological, and human health impacts that invasive species cause. Executive Order 13112 also calls for the restoration of native plants and tree species. If the proposed project will entail new landscaping, the EIS should describe how the project will meet the requirements of Executive Order 13112.

In addition, we encourage alternative management practices that limit herbicide use (as a last resort), focusing instead on other methods to limit invasive species vegetation and decrease fire risk. Possible alternatives include mowing and weed control fabric, which may need a layer of soil to prevent degradation due to ultraviolet light.

Air Quality

The EIS should provide a detailed discussion of ambient air conditions (baseline or existing conditions), National Ambient Air Quality Standards (NAAQS) and non-NAAQS pollutants, criteria pollutant nonattainment areas, and potential air quality impacts of the proposed project (including cumulative and indirect impacts). Such an evaluation is necessary to understand the potential impacts from temporary, long-term, or cumulative degradation of air quality.

The EIS should describe and estimate air emissions from potential construction and maintenance activities, as well as proposed mitigation measures to minimize those emissions. EPA recommends an evaluation of the following measures to reduce emissions of criteria air pollutants and hazardous air pollutants (air toxics).

- Existing Conditions The EIS should provide a detailed discussion of ambient air conditions, National Ambient Air Quality Standards, and criteria pollutant nonattainment areas in the vicinity of the project.
- Quantify Emissions The EIS should estimate emissions of criteria and hazardous air
 pollutants (air toxics) from the proposed project and discuss the timing for release of
 these emissions over the lifespan of the project. The EIS should describe and estimate
 emissions from potential construction activities, as well as proposed mitigation measures
 to minimize these emissions.
- Specify Emission Sources The EIS should specify all emission sources by pollutant from mobile sources (on and off-road), stationary sources (including portable and temporary emission units), fugitive emission sources, area sources, and ground disturbance. This source specific information should be used to identify appropriate mitigation measures and areas in need of the greatest attention.
- Construction Emissions Mitigation Plan The EIS should include a draft Construction Emissions Mitigation Plan and ultimately adopt this plan in the Record of Decision. In addition to all applicable local, state, or federal requirements, we recommend the following control measures (Fugitive Dust, Mobile and Stationary Source and Administrative) be included in the Construction Emissions Mitigation Plan in order to reduce impacts associated with emissions of particulate matter and other toxics from construction-related activities:
- Fugitive Dust Source Controls: The EIS should identify the need for a Fugitive Dust Control Plan to reduce Particulate Matter 10 and Fine Particulate Matter 2.5 emissions during construction and operations. We recommend that the plan include these general commitments:
 - O Stabilize heavily used unpaved construction roads with a non-toxic soil stabilizer or soil weighting agent that will not result in loss of vegetation, or increase other environmental impacts.
 - O During grading, use water, as necessary, on disturbed areas in construction sites to control visible plumes.
 - o Vehicle speed
 - Limit speeds to 25 miles per hour on stabilized unpaved roads as long as such speeds do not create visible dust emissions.
 - Limit speeds to 10 miles per hour or less on unpaved areas within construction sites on un-stabilized (and unpaved) roads.
 - Post visible speed limit signs at construction site entrances.
 - o Inspect and was construction equipment vehicle tires, as necessary, so they are free of dirt before entering paved roadways, if applicable.
 - o Provide gravel ramps of at least 20 feet in length at tire washing/cleaning stations, and ensure construction vehicles exit construction sites through

- treated entrance roadways, unless an alternative route has been approved by appropriate lead agencies, if applicable.
- O Use sandbags or equivalent effective measures to prevent run-off to roadways in construction areas adjacent to paved roadways. Ensure consistency with the project's Storm Water Pollution Prevention Plan, if such a plan is required for the project.
- O Sweep the first 500 feet of paved roads exiting construction sites, other unpaved roads en route from the construction site, or construction staging areas whenever dirt or runoff from construction activity is visible on paved roads, or at least twice daily (less during periods of precipitation).
- Stabilize disturbed soils (after active construction activities are completed)
 with a non-toxic soil stabilizer, soil weighting agent, or other approved soil
 stabilizing method.
- O Cover or treat soil storage piles with appropriate dust suppressant compounds and disturbed areas that remain inactive for longer than 10 days. Provide vehicles (used to transport solid bulk material on public roadways and that have potential to cause visible emissions) with covers. Alternatively, sufficiently wet and load materials onto the trucks in a manner to provide at least one foot of freeboard.
- O Use wind erosion control techniques (such as windbreaks, water, chemical dust suppressants, and/or vegetation) where soils are disturbed in construction, access and maintenance routes, and materials stock pile areas. Keep related windbreaks in place until the soil is stabilized or permanently covered with vegetation.

Mobile and Stationary Source Controls:

- o If practicable, lease new, clean equipment meeting the most stringent of applicable Federal⁴ or State Standards. In general, commit to the best available emissions control technology. Tier 4 engines should be used for project construction equipment to the maximum extent feasible⁵.
- o Where Tier 4 engines are not available, we recommend use of EPA-verified particulate traps, oxidation catalysts and other appropriate controls where suitable to reduce emissions of diesel particulate matter and other pollutants to no more than Tier 2 levels.
- O Consider using electric vehicles, natural gas, biodiesel, or other alternative fuels during construction and operation phases to reduce the project's criteria and greenhouse gas emissions.
- o Plan construction scheduling to minimize vehicle trips.
- o Limit idling of heavy equipment to less than 5 minutes and verify through unscheduled inspections.

⁴ EPA's website for nonroad mobile resources is http://www.epa.gov/nonroad/.

⁵ Diesel engines < 25 hp rated power started phasing in Tier 4 Model Years in 2008. Larger Tier 4 diesel engines will be phased in depending on the rated power (e.g., 25 hp - <75 hp: 2013; 75 hp - <175 hp: 2012-2013; 175 hp - <750 hp: 2011-2013; and \geq 750 hp 2011 – 2015).

 Maintain and tune engines per manufacturer's specifications to perform at EPA certification levels, prevent tampering, and conduct unscheduled inspections to ensure these measures are followed.

Administrative controls:

- Develop construction traffic and parking management plan that maintains traffic flow and plan construction to minimize vehicle trips.
- O Identify any sensitive receptors in the project area, such as children, elderly, and the infirmed, and specify the means by which impacts to these populations will be minimized (e.g. located construction equipment and staging zones away from sensitive receptors and building air intakes).
- Include provisions for monitoring fugitive dust in the fugitive dust control plan and initiate increased mitigation measures to abate any visible dust plumes.

Climate Change

Scientific evidence supports the concern that continued increases in greenhouse gas emissions resulting from human activities will contribute to climate change. Global warming is caused by emissions of carbon dioxide and other heat-trapping gases. On December 7, 2009, the EPA determined that Greenhouse Gases (GHGs) contribute to air pollution that "endangers public health and welfare" within the meaning of the Clean Air Act. Higher temperatures and increased winter rainfall will be accompanied by a reduction in snow pack, earlier snowmelts, and increased runoff. Some of the impacts, such as reduced groundwater discharge, and more frequent and severe drought conditions, may impact the proposed projects. The EIS should consider how climate change could potentially influence the proposed project, specifically within sensitive areas, and assess how the projected impacts could be exacerbated by climate change and strategies for climate change adaptation planning. For example, measures for climate change adaptation should consider potentially increased drainage needs.

Greenhouse Gases

The Council on Environmental Quality (CEQ) recently released draft guidance for the consideration of greenhouse gas (GHG) emissions in NEPA documents⁶. We recommend that FHWA/LADOTD consider this guidance when evaluating emissions as well as disclosing potential climate change impacts on the project. EPA supports an accounting in the EIS of those GHG emissions from proposed construction and operation activities. We also recommend identification and incorporation of appropriate mitigation measures to reduce GHG emissions.

Hazardous Materials/Hazardous Waste/Solid Waste

The EIS should address potential direct, indirect, and cumulative impacts of hazardous waste from construction and operation of the proposed project. The document should identify projected hazardous waste types and volumes, and expected storage, disposal, and management

 $^{^6 \} http://ceq.hss.doe.gov/nepa/regs/Consideration_of_Effects_of_GHG_Draft_NEPA_Guidance_FINAL_02182010.pdf$

plans. It should identify any hazardous materials sites within the project's study area and evaluate if those sites would impact the project in any way.

Tribal Consultation

The United States has a unique legal relationship with federally-recognized tribes based on the Constitution, treaties, statutes, Executive Orders, and court decisions. This relationship includes recognition of the right of tribes as sovereign governments to self-determination, and an acknowledgment of the federal government's trust responsibility to tribes. The precise nature of this relationship will vary depending upon the identity of the tribes, nature of trust resources, and federal agencies involved.

In addition, some specific duties of federal agencies related to consultation with tribal governments are set forth in Executive Order 13175 (65 FR 67249; November 9, 2000), the Presidential Memo of November 5, 2009, and the July 30, 2010, the Office of Management and Budget guidance for implementing the Presidential Memo.

The EIS should contain adequate information to document that: 1) potentially affected Tribes, tribal resources and citizens were identified, and 2) appropriate contact was made with the Tribal officials of potentially affected Tribes (beyond the narrow context of working with THPOs or SHPOs on issues related to historic properties (NHPA), or 3) that the agency otherwise concluded that there were not tribes or tribal resources that would be affected and there was no need for such contact or consultation.

National Historic Preservation Act

Consultation for tribal cultural resources is required under Section 106 of the National Historic Preservation Act (NHPA). Historic properties under the NHPA are properties that are included in the National Register of Historic Places (NRHP) or that meet the criteria for the National Register. Section 106 of the NHPA requires a federal agency, upon determining that activities under its control could affect historic properties, consult with the appropriate State Historic Preservation Officer (SHPO)/Tribal Historic Preservation Officer (THPO). Under NEPA, any impacts to tribal, cultural, or other treaty resources must be discussed and mitigated. Section 106 of the NHPA requires that Federal agencies consider the effects of their actions on cultural resources, following regulation in 36 CFR 800.

The EIS should address the existence of cultural and historic resources, including Indian sacred sites, in the project areas, and address compliance with Section 106 of the NHPA. It should also address Executive Order 13007, distinguish it from Section 106 of the NHPA, and discuss how FHWA/LADOTD will avoid adversely affecting the physical integrity, accessibility, or use of sacred sites, if they exist. The EIS should provide a summary of all coordination with Tribes and with the SHPO/THPO, including identification of NRHP eligible sites, and development of a Cultural Resource Management Plan.

Environmental Justice and Impacted Communities

Executive Order 12898, Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations (February 11, 1994) and the Interagency Memorandum of Understanding on Environmental Justice (August 4, 2011) directs federal agencies to identify and address disproportionately high and adverse human health or environmental effects on minority and low-income populations, allowing those populations a meaningful opportunity to participate in the decision-making process. Guidance by CEQ clarifies the terms low-income and minority population (which includes Native Americans) and describes the factors to consider when evaluating disproportionately high and adverse human health effects. The EIS should include an evaluation of environmental justice populations within the geographic scope of the project. If such populations exist, the EIS should address the potential for disproportionate adverse impacts to minority and low-income populations, and the approaches used to foster public participation by these populations. Assessment of the project's impact on minority and low-income populations should reflect coordination with those affected populations. The EIS should also describe outreach and public involvement conducted to all other communities that could be affected by the project, since rural communities may be among the most vulnerable to health risks associated with the project. Please refer to EPA's EJ website⁸ for additional information.

Children's Health and Safety

Executive Order 13045 on Children's Health and Safety directs that each Federal agency shall make it a high priority to identify and assess environmental health and safety risks that may disproportionately affect children, and shall ensure that its policies, programs, activities, and standards address these risks. Analysis and disclosure of these potential effects under NEPA is necessary because some physiological and behavioral traits of children render them more susceptible and vulnerable than adults to health and safety risks. Children may be more highly exposed to contaminants because they generally eat more food, drink more water, and have higher inhalation rates relative to their size. Also, children's normal activities, such as putting their hands in their mouths or playing on the ground, can result in higher exposures to contaminants as compared with adults. Children may be more vulnerable to the toxic effects of contaminants because their bodies and systems are not fully developed and their growing organs are more easily harmed.

Based on current EPA policy and guidance, an analysis of impacts to children should be included in a NEPA analysis if there is a possibility of disproportionate impact on children related to the proposed action. EPA views childhood as a sequence of lifestages, from conception through fetal development, infancy, and adolescence. Therefore, exposures to children at each lifestage, as well as pregnant and nursing women, are relevant and should be considered when addressing health and safety risks for children.

⁷ Environmental Justice Guidance under the National Environmental Policy Act, Appendix A (Guidance for Federal Agencies on Key Terms in Executive Order 12898), CEQ, December 10, 1997.

⁸ http://www.epa.gov/environmentaljustice/

⁹ http://www.epa.gov/compliance/resources/policies/nepa/children-health-risks-pg.pdf

Because children can be more susceptible to noise levels, mobile source air pollution, construction dust, and the chemicals associated with building and construction materials, we recommend that the NEPA document specifically address the potential direct, indirect, and cumulative impacts of the proposed project on children's health, including consideration of prenatal exposures (exposures that may be experienced by pregnant women).

The EIS should characterize and address children's exposures and susceptibilities to the pollutants of concern, which should include the following:

- Identification of the pollutants and sources of concern: Consider whether the pollutants and sources of concern pose a particular hazard to children's health (for example, lead or other heavy metals, or air pollution from near roadway exposures).
- Exposure Assessment: Describe demographics of affected neighborhoods/populations/communities and focus exposure assessments on schools, recreation areas, childcare centers, parks, and residential areas in close proximity to the proposed project, and other areas of apparent frequent and/or prolonged exposure.
- Baseline health conditions: Consider obtaining and including available relevant health data/records for the neighborhoods/populations/communities of concern.
- Respiratory Impacts/Asthma: Consider data on existing asthma rates and asthma severity among children and the general community living, working, playing, and attending school and daycare near the project site. To the extent feasible, identify impacts of the project on asthma rates and severity in children near the project site and quantify associated costs.
- Noise Impacts: Consider impacts from noise on health and learning, especially near homes, schools and daycare centers.
- Impacts Regarding Obesity Factors: Consider potential impacts that could influence childhood obesity factors, such as impacts on school commutes, and on the accessibility of neighborhood parks, green spaces, and recreation areas.
- Impacts from Air Pollutant Emissions: Consider exposure and impacts to children from mobile source air pollutants, including proximity to transportation corridors, transportation hubs, and ports, and project construction emissions. Combine these with other area sources/baseline air quality.
- Impacts from Other Chemical or Physical Exposures: Consider impacts to children from other site activities, such as pesticide application, demolition, etc.

Indirect and Cumulative Impacts

The indirect impacts analysis should identify how resources, ecosystems, and communities in the vicinity of the project are affected by the proposed project later in time or father removed in distance. We recommend focusing on induced growth and development and other effects related to induced changes in the pattern of land use, population density or growth rate, and related effects on air, water and other natural systems, including ecosystems.

The cumulative impacts analysis should identify how resources, ecosystems, and communities in the vicinity of the project have already been, or will be, affected by past, present, or future activities in the project area. These resources should be characterized in terms of their

response to change and capacity to withstand stresses. Trends data should be used to establish a baseline for the affected resources, to evaluate the significance of historical degradation, and to predict the environmental effects of the project components.

For the cumulative impacts assessment, we recommend focusing on resources of concern or resources that are "at risk" and/or are significantly impacted by the proposed project, before mitigation. For this project, the EIS should include a thorough assessment of the cumulative impacts to aquatic and biological resources and air quality, especially in the context of the other developments occurring and proposed in the area.

The EIS should also delineate appropriate geographic boundaries, including natural ecological boundaries, whenever possible, and should evaluate the time period of the project's effects. For instance, for a discussion of cumulative wetland impacts, a natural geographic boundary such as a watershed or sub-watershed could be identified. The time period, or temporal boundary, could be defined as from 1972 (when the CWA established Section 404) to the present.

Please refer to the Council on Environmental Quality's "Considering Cumulative Effects Under the National Environmental Policy Act" and EPA's "Consideration Of Cumulative Impacts In EPA Review of NEPA Documents" for assistance with identifying appropriate boundaries and identifying appropriate past, present, and reasonably foreseeable future projects to include in the analysis.

Mitigation and Monitoring

Within the process of developing the EIS, if impacts to waters of the U.S. and wetlands require a USACE permit and are significant, a draft mitigation plan should also be developed and made available to EPA prior to the release of the Final EIS. A draft mitigation plan should strive for avoidance and minimization first and should outline appropriate compensation and enhancement measures for unavoidable impacts to wetlands and special aquatic sites. A draft plan should include the evaluation of the least environmentally damaging practicable alternative, according to the Section 404(b)(1) guidelines and should outline a monitoring plan. Please note that any compensatory mitigation plan must fully comply with the *Compensatory Mitigation for Losses of Aquatic Resources; Final Rule (Mitigation Rule)* effective June 9, 2008.¹²

Coordination with Land Use Planning Activities

The EIS should discuss how the proposed action would support or conflict with the objectives of federal, state, tribal or local land use plans, policies and controls in the project areas. The term "land use plans" includes all types of formally adopted documents for land use planning, conservation, zoning and related regulatory requirements. Proposed plans not yet developed should also be addressed if they have been formally proposed by the appropriate government body in a written form (CEQ's Forty Questions, #23b).

¹⁰ http://ceq.hss.doe.gov/nepa/ccenepa/ccenepa.htm

¹¹ http://www.epa.gov/compliance/resources/policies/nepa/cumulative.pdf

¹² http://water.epa.gov/lawsregs/guidance/wetlands/wetlandsmitigation_index.cfm



CALCASIEU PARISH POLICE JU GOVERNING AUTHORITY OF CALCASIEU PARISH, LOUISI

DIVISION OF PLANNING AND DEVELOPMENT

P.O. Urawer 3267 Lake Charles, Louisiana, 70602-3287 337 / 721-3600 800 / 826-6092 Fax 337 / 437-4100

November 15, 2013

Mrs. Kate Prejean **HNTB** Corporation 10000 Perkin Rowe, Suite 640 Baton Rouge, LA 70810

RE:

State Project No. H.003931.2

F.A.P. NO: BR-10-1(212)29

NAME: I-10 CALCASIEU RIVER BRIDGE

LOCATION: I-10/I-210 WEST END TO: I-10/I-210 EAST END

Dear Mrs. Prejean:

As per your request of solicitation of views, I have reviewed the documentation sent for the I-10 Calcasieu River Bridge project. As far as floodplain management purposes, the proposed project portion located in the unincorporated areas of Calcasieu Parish is outside the 100-year floodplain and has no elevation or permit requirements.

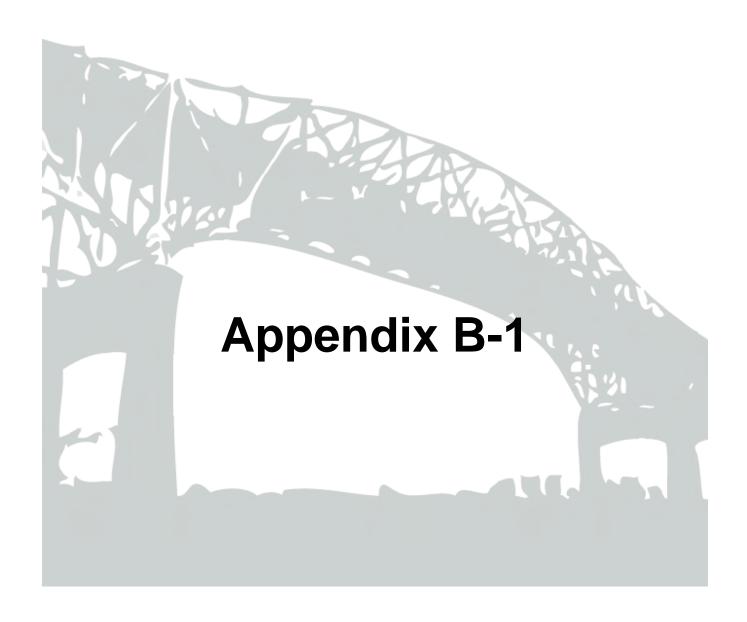
Sincerely,

Floodplain Administrator

Appendix B

Materials

	NOI	B-1
S	ection 106 Consulting Party Invite	B-2
	Agency Scoping Meeting Invite	B-3
Pub	lic Scoping Meeting Invite / Postcard	B-4
Pul	oic Scoping Meeting Advertisement	B-5
	Fall 2013 Newsletter	B-6
	Sign-In Sheets	B-7
Age	ency Scoping Meeting Presentation	B-8
F	Public Scoping Meeting Handouts	B-9
Puk	olic Scoping Meeting Exhibit Boards	B-10
IMCAL	Bridge Height Resolution (Dec. 2007)	B-11





incorporation of green energy technologies and a bike path will also be considered; and (7)-Hybrid Corridor. This would consist of a combination of the previously identified alternatives, whose elements (TSM/TDM, Freeway, Expressway, Tollway, HSR Feeder Service, Green Energy Technologies, bike path) would be pieced together to best fit the needs of each section of the corridor. The determination of which elements to use, and at which locations, would be based on the results of the traffic study, environmental studies and public input. It is anticipated that the proposed project may require the following federal approvals and permits: A Biological Opinion from the United States Fish and Wildlife Service; approval of a PM10 and PM2.5 Hot Spot Analysis determination by the Conformity Working Group for transportation conformity under the Clean Air Act; Section 401, 402 and 404 permits under the Clean Water Act; and a Farmland Conversion Impact Rating under the Farmland Protection Policy Act.

Letters describing the proposed action and soliciting comments will be sent to appropriate Federal, State and local agencies, Participating Agencies, Tribal governments, and to private organizations and citizens who have previously expressed or are known to have an interest in this proposal. To ensure that the full range of issues related to this proposed action are addressed and all significant issues identified, comments and suggestions are invited from all interested parties. Comments or questions concerning this proposed action and the draft EIS should be directed to Caltrans at the address provided above.

(Catalog of Federal Domestic Assistance Program Number 20.205, Highway Planning and Construction. The regulations implementing Executive Order 12372 regarding intergovernmental consultation on Federal programs and activities apply to this program.)

Issued on: July 22, 2013.

Matt Schmitz,

Director State Programs, Federal Highway Administration, Sacramento, California.

[FR Doc. 2013-18515 Filed 7-31-13; 8:45 am]

BILLING CODE 4910-22-P

DEPARTMENT OF TRANSPORTATION

Federal Highway Administration

Environmental Impact Statement; Calcasieu Parish, LA

AGENCY: Federal Highway Administration (FHWA), DOT.

ACTION: Notice of intent.

SUMMARY: The Federal Highway Administration is issuing this notice to advise the public that an Environmental Impact Statement (EIS) will be prepared for a proposed transportation project in Calcasieu Parish, Louisiana.

FOR FURTHER INFORMATION CONTACT:

FHWA Carl Highsmith, Project Delivery Team Leader, FHWA, 5304 Flanders Drive, Suite A, Baton Rouge, Louisiana 70808. Project information can be found at the project Web site http:// www.i10lakecharles.com.

SUPPLEMENTARY INFORMATION: The FHWA, in cooperation with the DOTD, will prepare an EIS on alternatives for additional capacity along I-10 in the Lake Charles region between the I-210 interchanges including the Calcasieu River Bridge. A feasibility and environmental study was previously conducted in accordance with the National Environmental Policy Act (NEPA) for this project. The feasibility study involved four phases: (1) Information and Data Gathering; (2) Preliminary Study; (3) Refined Alternatives; and (4) Preparation and Submission of a Final Report. Based on the preliminary studies which included input from the local community, four feasible alternatives have been recommended for further study. A nobuild alternative will also be evaluated in accordance with NEPA. The preliminary studies were completed in spring 2004; however the proposed project was placed on hold to evaluate the bridge height and due to the discovery of hazardous materials contamination within the proposed right-of-way. Because of the potential for impacts and issues associated with various socioeconomic and environmental resources and the highlevel of public interest, FHWA will prepare an EIS. The total project length is approximately 9 miles. In addition to bridge alternatives, improvements to be investigated within the study limits include: A redesign of Sampson Street from Sulphur Avenue to I-10 to provide grade separations with existing railroads; a redesign of the access to and from I-10 on the west side of the bridge between Sampson Street and PPG Drive; a redesign of the access to and from I-10 near the east end of the bridge; and consideration of the implementation of one-way frontage roads from PPG Drive to US 90 East. Consideration will be given to using the existing bridge for the frontage roads. Proposed changes to the existing bridge to be investigated include: (a) Designing the proposed bridge structure to accommodate three

travel lanes and one auxiliary lane, with inside and outside shoulders and two frontage roads in each direction, (b) a reduction in navigational clearance, (c) reducing the existing 420 foot truss span to two main spans, and (d) determining if the existing vertical clearance for marine traffic can be reduced. Letters describing the proposed project and soliciting comments will be sent to appropriate Federal, State, and local agencies, and to private organizations and the public who have previously expressed or are known to have interest in this project. Numerous public meetings will be held throughout the term of the project. The first of these meetings, a series of public scoping meetings, will be conducted to provide the public information about the project and an opportunity to assist in formulating and revising the scope of the study. The public scoping meetings will be scheduled in the future and will be posted to the project Web site http://www.i10lakecharles.com.

In addition, a public hearing will be held. Public notice will be given of the time and place of the meetings and hearing.

To ensure that the full range of issues related to this proposed project are addressed and all significant issues identified, comments and suggestions are invited from all interested parties. Comments or questions concerning this proposed action and the EIS should be directed to the FHWA at the address provided above.

(Catalog of Federal Domestic Assistance Program Number 20.205, Highway Research, Planning, and Construction. The regulations implementing Executive Order 12372 regarding intergovernmental consultation on Federal programs and activities apply to this program.)

Issued on: July 25, 2013.

Charles Bolinger,

Division Administrator, Baton Rouge, Louisiana.

[FR Doc. 2013–18531 Filed 7–31–13; 8:45 am] BILLING CODE 4910–22–P

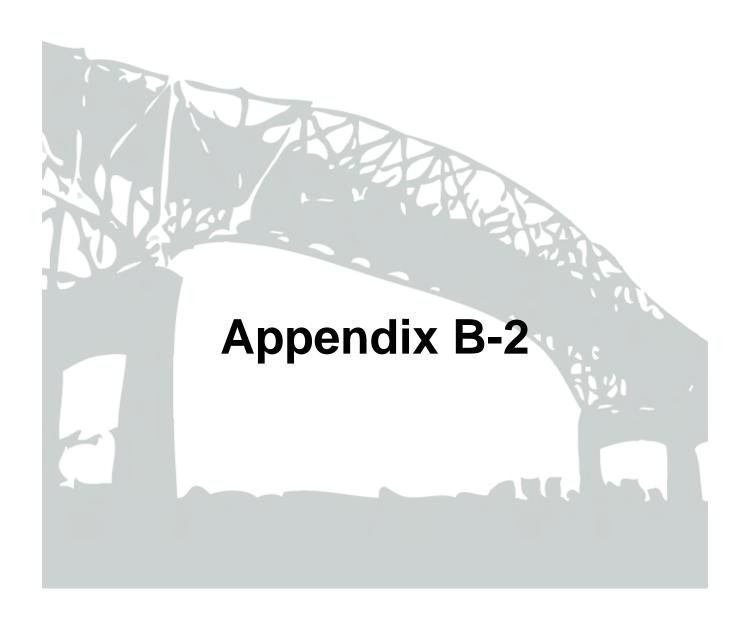
DEPARTMENT OF TRANSPORTATION

Federal Motor Carrier Safety Administration

[Docket No. FMCSA-2013-0051]

Agency Information Collection Activities; New Information Collection Request: Commercial Motor Vehicle Marking Requirements

AGENCY: Federal Motor Carrier Safety Administration (FMCSA), DOT.





Environmental Section

PO Box 94245 | Baton Rouge, LA 70804-9245 Phone: 225-242-4502 Fax: 225-242-4500 **Bobby Jindal**, Governor **Sherri H. LeBas**, P.E., Secretary

October 10, 2013

STATE PROJECT NO. H.003931.2 FEDERAL AID PROJECT NO. BR-10-1(212)29 I-10 CALCASIEU RIVER BRIDGE (1-10/I-210 WEST END TO I-10/I-210 EAST END) CALCASIEU PARISH

SUBJECT: REQUEST TO BE A CONSULTING PARTY FOR SECTION 106 PROCESS

The Louisiana Department of Transportation and Development (DOTD) and the Federal Highway Administration (FHWA) are proposing federally-funded capacity improvements to I-10 between the I-210 interchanges, a distance of approximately nine (9) miles. The proposed project includes the potential reconstruction of the Calcasieu River Bridge. The Calcasieu River Bridge was originally constructed in the late 1940s and early 1950s as part of the US 90 system and then integrated as part of I-10 in the 1960s. The Calcasieu River Bridge has been deemed eligible for the National Register of Historic Places (NRHP). Attached is a location map of the proposed project and photos of the Calcasieu River Bridge.

The NRHP eligible Calcasieu River Bridge, as well as other historic and/or potentially historic properties, may be affected by the proposed project. Section 106 of the National Historic Preservation Act requires the FHWA and the DOTD, in consultation with the Louisiana State Historic Preservation Officer, to identify potential parties for consultation in order to assure that historic properties are taken into consideration at all levels of project planning and development.

For additional information, or to request to be a consulting party, please contact Ms. Noel Ardoin, P.E., with the DOTD by phone at (225) 242-4501 or e-mail at Noel.Ardoin@la.gov, or Mr. Joachim Umeozulu, P.E., with the DOTD by phone at (225) 379-1386 or e-mail at Joachim.Umeozulu@la.gov. You may also send a request via mail to the Louisiana Department of Transportation and Development, Attention: Ms. Noel Ardoin, P.O. Box 94245, Baton Rouge, LA 70804-9245. Please provide reasons for requesting to be a consulting party. FHWA will make the determination of who will be accepted as a consulting party. Responses would be appreciated by November 25, 2013. If you are aware of other individuals or other organizations that may be interested in the I-10 Calcasieu River Bridge Project, please forward their names and contact information to Ms. Noel Ardoin or Mr. Joachim Umeozulu at the contact information provided above.

Sincerely,

Noel Ardoin

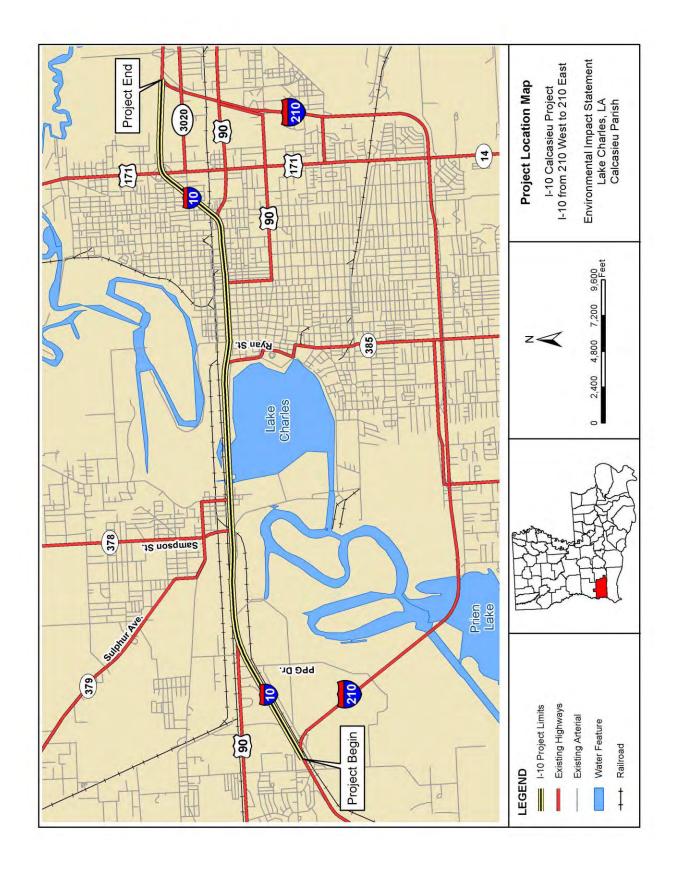
Environmental Engineer Administrator

D. andown

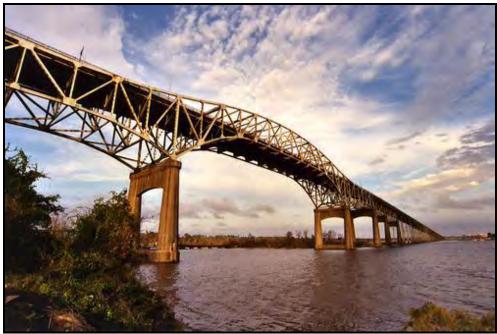
Attachments

cc: FHWA

NA/



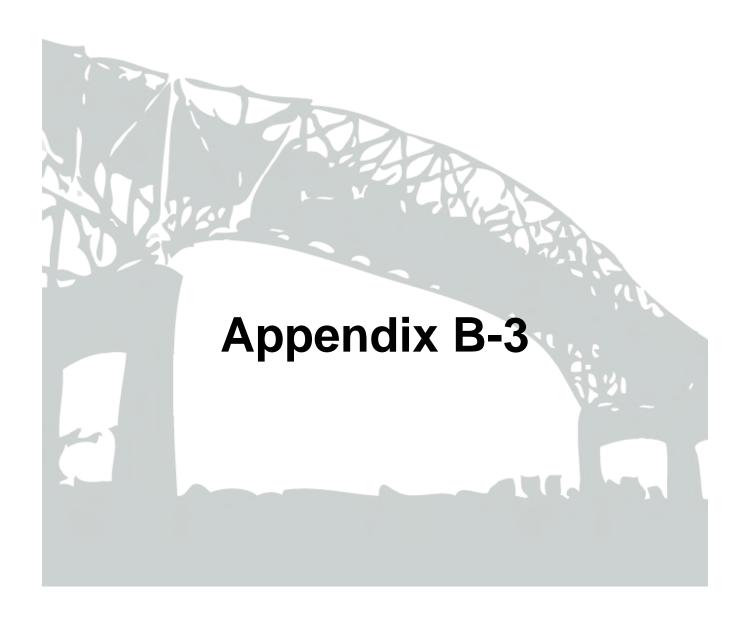
Calcasieu River Bridge Photographs



Photograph 1.



Photograph 2.





Office of Engineering
PO Box 94245 | Baton Rouge, LA 70804-9245
Phone: 225-379-1234

Bobby Jindal, Governor Sherri H. LeBas, P.E., Secretary

October 7, 2013

RE: Interstate 10 (I-10) Calcasieu River Bridge (I-10/I-210 West End to I-10/I-210 East End)
Agency Scoping Meeting Invitation

The Louisiana Department of Transportation and Development (DOTD) and the Federal Highway Administration (FHWA) have initiated the preparation of an Environmental Impact Statement (EIS) for the I-10 Calcasieu River Bridge (I-10/I-210 West End to I-10/I-210 East End) Project in Calcasieu Parish, Louisiana. A Notice of Intent (NOI) dated July 25, 2013 was published in the Federal Register on August 1, 2013.

You are invited to an Agency Scoping Meeting for the above-captioned project. The proposed project is approximately 9 miles in length and includes alternatives for I-10 in the Lake Charles region between the I-210 interchanges, including the Calcasieu River Bridge (see enclosed project location map). The primary purpose of the proposed project is to improve traffic congestion, but will also address safety and roadway/bridge design issues. The EIS will involve an analysis of several proposed alternatives and their associated environmental concerns.

The Agency Scoping Meeting will be held at the Lake Charles Civic Center, Jean Lafitte Room at 900 Lakeshore Drive, Lake Charles, LA 70601 from 2:00 p.m. to 3:00 p.m. on Thursday, October 24, 2013. The consultant team will present a project overview. Representatives from the DOTD, FHWA, and consultant team will facilitate a discussion on issues material to the Draft Project Coordination Plan, Draft Purpose and Need and proposed study area, as well as solicit input on specific issues/resources to be addressed in the EIS.

We would also like to remind you that a Public Scoping Meeting will be held at the same location on the same day from 5:00 p.m. to 8:00 p.m. The public has been invited to this meeting to learn more about the project (the Draft Purpose and Need, Draft Project Coordination Plan, and proposed study area will be presented), discuss issues, and ask questions. Comments will be accepted at the meeting, through the project website (www.i10lakecharles.com) or by mail postmarked no later than Monday, November 4, 2013.

If you have any questions or would like to discuss in the proposed project in more detail, please contact Ms. Noel A. Ardoin, P.E. with the DOTD at (225) 242-4501; Mr. Joachim Umeozulu, P.E. with the DOTD at (225) 379-1386; or Mr. Bob Mahoney with the FHWA at (225) 757-7624.

Thank you in advance for your interest in this project.

Sincerely,

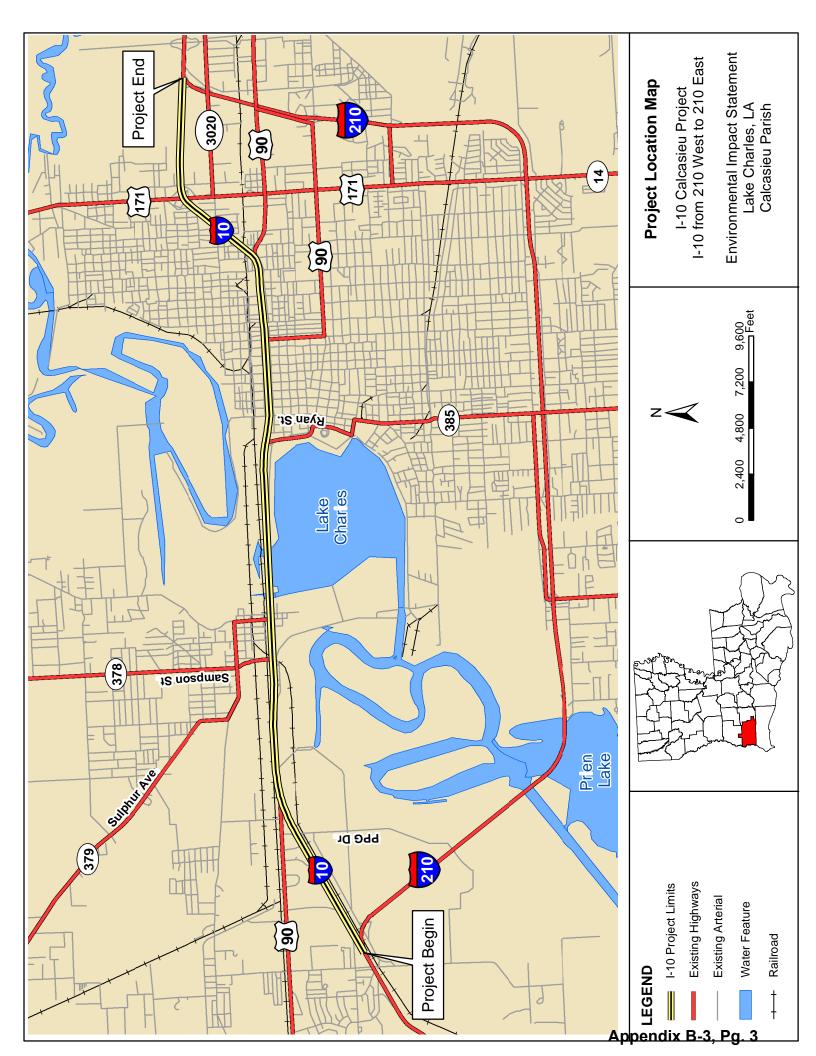
Noel A. Ardoin, P.E.

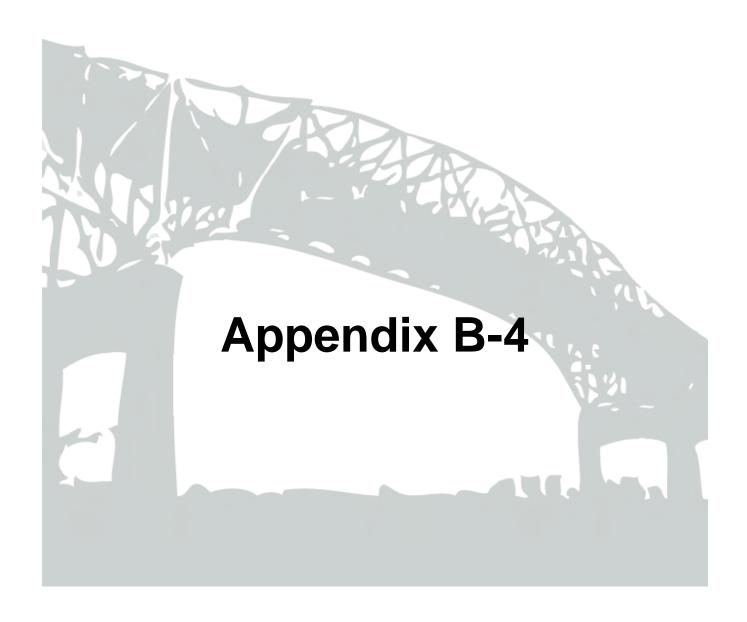
n. ardoin

Environmental Engineer Administrator
Louisiana Department of Transportation and Development

Enclosure: Project Location Map

cc: Project File





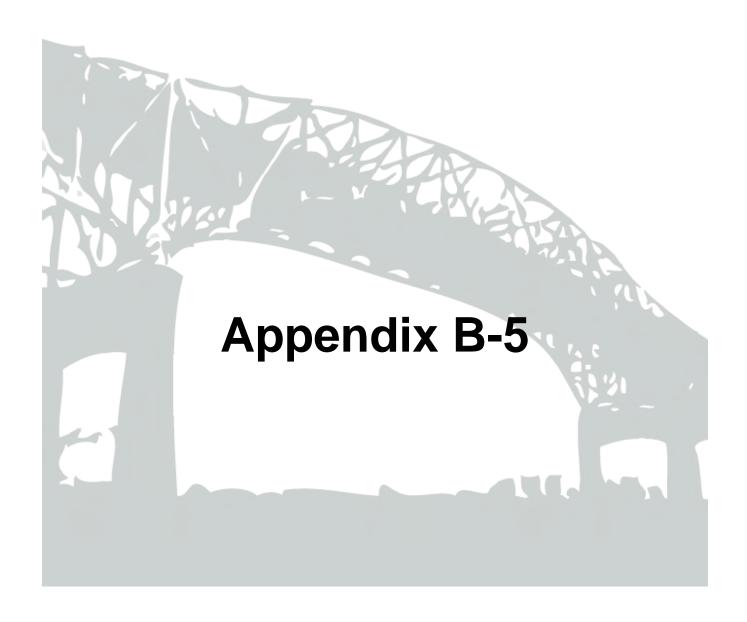
ou are invited to a Public Scoping Meeting of the

1-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End)



Appendix B-4



NOTICE OF PUBLIC SCOPING MEETING

Notice is hereby given that the Louisiana Department of Transportation and Development (DOTD) and the Federal Highway Administration (FHWA) will conduct an open-forum public meeting for:

State Project No. H.003931.2 Federal Aid Project No. BR-10-1(212)29 I-10 Calcasieu River Bridge (I-10/I-210 West End to I-10/I-210 East End) Calcasieu Parish, Louisiana

The meeting will be held at the following place and time:

THURSDAY

October 24, 2013 5:00 p.m. to 8:00 p.m.

Lake Charles Civic Center Jean Lafitte Room 900 Lakeshore Drive Lake Charles, LA 70601

The purpose of the public meeting is to present an overview of the I-10 Calcasieu River Bridge Project, including the project study area, purpose and need and future coordination efforts, as well as the process for preparing an Environmental Impact Statement (EIS). The EIS will evaluate environmental impacts associated with the proposed widening and infrastructure improvements to I-10 in Lake Charles between the I-210 intersections, including the Calcasieu River Bridge. The primary purpose of the proposed project is to improve traffic congestion, but will also address safety and roadway/bridge design deficiencies.

The Calcasieu River Bridge has been deemed eligible for the National Register of Historic Places. The bridge, as well as other historic and/or potentially historic properties may be affected by the proposed project. Section 106 of the National Historic Preservation Act requires the FHWA and the DOTD, in consultation with the Louisiana State Historic Preservation Officer, to identify potential consulting parties and to invite them to participate in the Section 106 process.

Persons interested in the proposed project, as well as interested Section 106 consulting parties, are invited to be present at the above time and place to review the study materials and comment on the information presented. The meeting will be an open-house format and there will be no formal presentation. Information on the proposed project can also be viewed at the project website, www.i10lakecharles.com.

All comments received during the public meeting and written comments post-marked within ten (10) calendar days of the meeting will become part of the official public record. If you are unable to attend the meeting, you may mail your comments to the address listed below:

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe Baton Rouge, LA 70810

Comments may also be submitted, and questions answered, by logging on to the project website and selecting *Contact Us.*

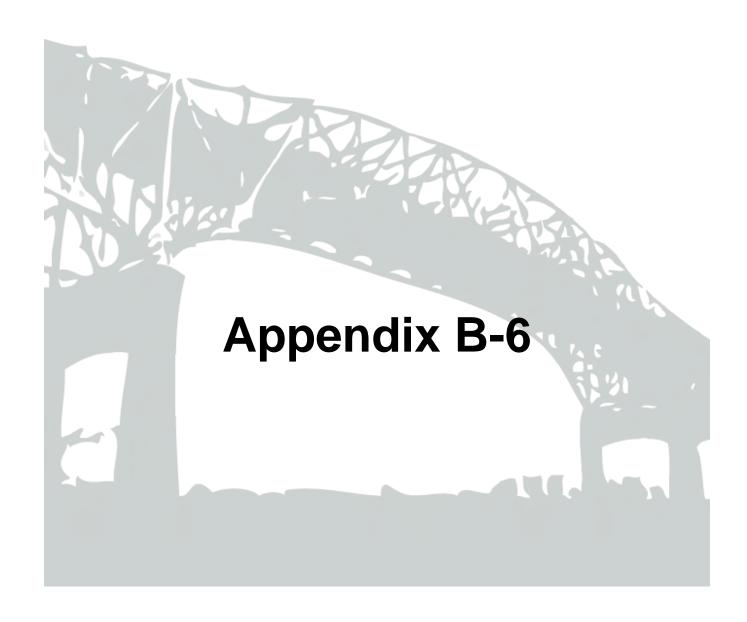
For additional information or to request to be a Section 106 consulting party, please contact either Ms. Noel Ardoin, P.E., with the DOTD by phone at (225) 242-4501 or e-mail at Noel-Ardoin@LA.GOV, or Mr. Joachim Umeozulu, P.E., with the DOTD by phone at (225) 379-1386 or e-mail at Joachim.Umeozulu@LA.GOV. You may also send a request via mail to the Louisiana Department of Transportation and Development, Attention: Ms. Noel Ardoin, P.O. Box 94245, Baton Rouge, LA 70804. Please provide reasons for requesting to be a consulting party. Requests would be appreciated by November 25, 2013. If you are aware of other individuals or other organizations that may be interested in the I-10 Calcasieu River Bridge Project, please forward their names and contact information to Ms. Noel Ardoin or Mr. Joachim Umeozulu at the contact information provided above.

If you require special assistance due to a disability or require an interpreter

to participate in this meeting, please contact Ms. Adriane McRae with HNTB Corporation at least five (5) working days prior to the meeting date by email at AMCRae@HNTB.com, phone at (225) 368-2840, or mail at HNTB Corporation, 10000 Perkins Rowe, Baton Rouge, LA, 70810.



0833261



Project History

The I-10 Calcasieu River Bridge was constructed in 1952 as a part of the U.S. 90 highway system, but was integrated as part of I-10 in the 1960s. The bridge helped ease congestion in the Lake Charles region by eliminating traffic delays caused by the original drawbridge and allowing ships to pass freely beneath its 135 foot-high span. However, continued growth in the area over the last 60 years has increased demand along I-10 from the east and west interchanges with I-210, including over the Calcasieu River Bridge.

The proposed project includes alternatives for additional capacity along this stretch of I-10 in the Lake Charles region. A feasibility and environmental study for the project was previously completed in 2004.

Because of the potential for impacts and issues associated with various socioeconomic and environmental resources and the high-level of public interest, Federal Highway Administration (*FHWA*) and the Louisiana Department of Transportation and Development (*LADOTD*)

have initiated the preparation of an Environmental Impact Statement (EIS) for the proposed project. The EIS will serve as a tool that assists with decision making and will evaluate and document the environmental impact of each alternative.



The Problem

Declining Capacity and Increased Congestion

• Existing capacity of the Calcasieu River Bridge is approximately 53,000 vehicles per day (*vpd*), but existing traffic volumes within the proposed project limits exceed 64,000 vpd. In the future, traffic volumes are expected to continue to increase.

Lack of System Connectivity

• Existing I-10 within the project limits (including the Calcasieu River Bridge) is 2 lanes in each direction, whereas I-10 immediately outside of the project limits is 3 lanes in each direction, creating a lack of connectivity and continuity on I-10.

Roadway Deficiencies

• The existing load limit of the Calcasieu River Bridge is inadequate for an interstate highway; and the lack of shoulders and vertical geometry on the existing bridge do not meet current roadway design criteria.

Safety Concerns

- Reduction of lanes (from 3 to 2 lanes in each direction) in the project area creates a bottleneck, limiting maneuverability and reducing travel speeds.
- Vehicles experiencing trouble have no place to pull over with the lack of shoulders on the bridge.
- Steep roadway grades slow traffic on the up-slope and make it more difficult to stop on the down-slope, further compromising safety.
- The existing low vertical clearance of the bridge has resulted in over-height vehicle collisions.

A Proposed Solution

Proposed improvements to be investigated include:

- Designing the proposed bridge structure to accommodate 3 travel lanes and 1 auxiliary lane, with inside and outside shoulders and potential frontage roads in each direction
- · Lowering the height of the bridge
- Reducing the existing 420 foot truss span of the bridge to 2 main spans
- Beyond the bridge limits, reconstructing the I-10 mainlanes to accommodate 3 travel lanes in each direction to match the existing typical sections of I-10 outside the proposed project limits
- Redesigning the Sampson Street interchange including review of crossings with existing railroads
- Redesigning the access to and from I-10 on the west side of the bridge between Sampson Street and PPG Drive and near the east end of the bridge
- Consideration of frontage roads from PPG Drive to US 90 East



Project Benefits

- Reduce congestion and facilitate connectivity on I-10
- · Improve roadway deficiencies
- Reduce the existing steep profile of the bridge
- Address long-standing public safety concerns and traffic congestion problems associated with the existing at-grade railroad crossings
- Improve response time during industry emergency evacuations
- Provide a facility in accordance with the Louisiana State Transportation Plan and the National I-10 Freight Corridor Study
- Provide better access by redesigning interchanges and improving frontage roads, thus supporting economic development in the Lake Charles metropolitan area

"...the lower the profile the less effect the [Calcasieu] bridge would have on heavy vehicle speeds and a lower number of accidents would be anticipated. Alternatively, the higher the profile the greater effect on heavy vehicle speeds, resulting in a higher number of accidents."

Excerpt from the I-10 Calcasieu River Bridge Height Special Study (2007) - Accident Rate Comparison of Heavy Trucks

What is an EIS?

An EIS is a full-disclosure document that details the process through which a transportation project was developed, includes a considerable range of reasonable alternatives, analyzes the potential impacts resulting from the alternatives and demonstrates compliance with applicable environmental laws, as well as provides a means for public input into the decision making process. The EIS is carried out for major federal actions in response to the National Environmental Policy Act of 1969 (NEPA). An EIS is the most thorough and comprehensive level of NEPA documentation. The EIS process is completed in the following major steps: Notice of Intent (NOI), Draft EIS, Public Hearing, Final EIS and Record of Decision (ROD). New laws now allow the option of combining the Final EIS and the ROD.

When will construction begin?

Construction would begin after the environmental review process (*NEPA process*) and design efforts are completed. The NEPA process is anticipated to be completed in approximately three years. Once funding is identified for design of the improvements, the design is anticipated to take two years. As with design, funding identification will be required prior to the beginning of construction.



Preliminary NEPA Timeline Public & Agency fall 2013 Scoping Meetings **Alternatives Development** fall 2013 & Screening Process - summer 2014 Recommendation of summer 2014 Reasonable Alternative(s) - fall 2014 fall 2014 Prepare Draft EIS - summer 2015 summer 2015 **Public Hearing** - fall 2015 fall 2015 **Prepare Final EIS** - spring 2016 Anticipated FEIS Approval spring 2016

Study Area Map



What's Next?

- First, public and agency scoping meetings will be held October 24, 2013 (see meeting invite on Page 4).
- Subsequent to the scoping meetings, the alternatives development process will begin and preliminary alternatives evaluation criteria will be generated.
- Public Meeting #1 Next, the final Purpose and Need, Project Coordination Plan, and preliminary alternatives and evaluation criteria will be presented at Public Meeting #1 and comments solicited (date to-be-determined).
- With consideration given to agency and public comments, the alternative evaluation criteria will be finalized and the reasonable alternative(s) will be identified.
- Public Meeting #2 Thereafter, the reasonable alternatives will be presented to the public and comments solicited (date to-be-determined).

After that...begin preparation of the draft EIS!

For more information on the project please visit our website at: www.i10lakecharles.com



I-10 Calcasieu River Bridge

We Need Your Input



You are invited to a Public Scoping Meeting I-10 Calcasieu River Bridge Project

Thursday, October 24, 2013 Lake Charles Civic Center, Jean Lafitte Room 900 Lakeshore Drive, Lake Charles, LA 70601

OPEN HOUSE FORMAT - Come and go anytime between 5:00 p.m. and 8:00 p.m. There will be no formal presentation. Join us for review and provide comments on:

- 1. Draft Project Coordination Plan
- 2. Draft Purpose and Need of the Project
- 3. Project Study Area

Comments will be accepted at the Public Scoping Meeting and during a formal comment period lasting up to 10 days following the meeting. Please contact Ms. Adriane McRae with HNTB at (225) 368-2840 at least 5 days prior to the Public Scoping Meeting if special assistance or an interpreter is needed for meeting participation.

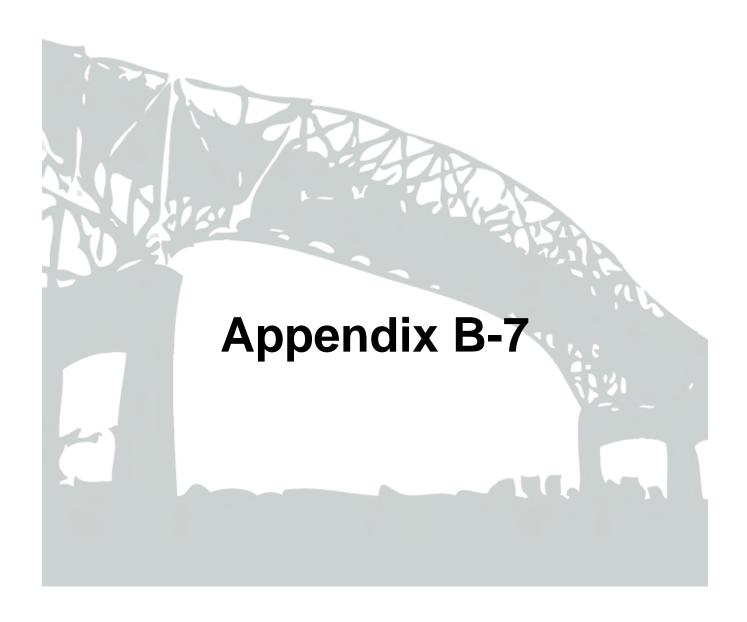
Comments may also be submitted, and questions answered, by logging on to the project web site at <u>www.i10lakecharles.com</u> and selecting **Contact Us.**



Calcasieu website: www.i10lakecharles.com



I-10 Calcasieu River Bridge Project 10000 Perkins Rowe, Suite 640 Baton Rouge, LA 70810



I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End)
Thursday, October 24, 2013 | 2:00 pm – 3:00 p.m.

Lake Charles Civic Center Jean Lafitte Room 900 Lakeshore Drive Lake Charles, LA 70601

SIGN-IN SHEET - (please print) EMAIL NAME ORGANIZATION **ADDRESS** PHONE (Agency Work Group Meeting Invites, Project Newsletter & Updates) CAL JASON DERISE LCPD 1028 Enterprise Blod. NOERISE @ City of LC. US 337) 660.4066 Mad Ardoin 1201 Capital Access noel ardoin@ la. sov. DOTO 978749ARDI Joe Umeozulu 1201 Capital Access Rd DOTA 225-379-1386 wachim umeozulu @ 6.gor Kelly Kemp DOTED PACES Rd. (25) 379-1809 MumphREY er (225) 319-1067 DOTO Ray. MunpHREGELA. 600 3000 Post Oak Houston DMSonth MOY ALLSOMY Darny and samy edms with con



I-10 Calcasieu River Bridge Project (1-10/l-210 West End to I-10/l-210 East End) Thursday, October 24, 2013 | 2:00 pm – 3:00 p.m.

NAME	ORGANIZATION	ADDRESS	PHONE	EMAIL (Agency Work Group Meeting Invites, Project Newsletter & Updates)
Vikita J. Sim	OI- LADOTO	LGK CHARGE, LA 70615	(337) 437-9278	Nikita. Simon@la. gov
Don Duberville	- LA DUTO	5827 Huy 90 LC LA 70615		
Justin Helton	United States Continued	127 W Broad St Lake Chales	337-721-5767	justin.K. helton Duscg,
RHa HUDSON	LADOTD	5827 Hay 90E Lailw Chas LA Torol 5	337- 437-9250	Rita, Jackson-Hudson®
Keith murra	X LAKe Charles	4200 Kirkmanst	4941358	HWAGIC
exiJackson	Senator Johns	1011 Lakeshore Dr. Suite 515 LC70601	337- 491-2016	johnsr@legis.la.gov
BOBMAHONE	y FILWA	BRLA	225-757-7624	robert. Mahoney Edit. g
Jerry Pitts	FHNIA	BELA	225-757-7618	Jerry P. Hoodat. gov
Mister Educados			337-491-1308	meduands OCATyoflans
Connie Dumt	- DOTD			Vonnie dupmte / 2, 901





I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End)
Thursday, October 24, 2013 | 2:00 pm – 3:00 p.m.

NAME	ORGANIZATION	ADDRESS	PHONE	EMAIL (Agency Work Group Meeting Invites, Project Newsletter & Updates)
ght Guilloy dr.	Cofic	326 PujaSt	491-1201	eligha. gullory dity of
An Bruce	Cof Soltha	101 N Huntigh-	527-4510	Joruce @ Sulphur or
eve Jiles	LA DOTD	P.D. Box 1430	437-9101	Steve. jilesala.gov
ES CRAIN	CPPJ		(337)721.3600	
LAYLON BUSBY	25P		491-2511	WAYLON. BUSBYELA-GOV



I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End)
Thursday, October 24, 2013 | 2:00 pm – 3:00 p.m.

NAME	ORGANIZATION	ADDRESS	PHONE	EMAIL (Agency Work Group Meeting Invites, Project Newsletter & Updates)
TIM GANGER	CPPJ	1114 RYAN L.C.	721-4100	tonnere = pj. net
ALLEN WAINWRIGHT	CPPZ	L.	721-3700	AWAIN WRIGHT @ CPPS. NE
Alicia Ball	IMCAL		433-1771	aliaa e uncal. 19
Veronica Albson	Sulphue City Council	P.O. Box 1309 Sulphur LA nobles	527-4571	
Robort Daughdril	CA)		721-3800	enaghdri ocppin
Dows Zett Lamay er	GOHSEP	SOU Chay Drive LAKE Charles LA 70611	225	Doug Zettlemoyer @LA. gov
				700



I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End)
Thursday, October 24, 2013 | 2:00 pm – 3:00 p.m.

NAME	ORGANIZATION	ADDRESS	PHONE	EMAIL (Agency Work Group Meeting Invites Project Newsletter & Updates)
Jill Lavender	IMCAL	4310 Ryan Street, Snite 330 Lake Charles, LA 70605	0 337-433-171	
Peter O'Grvol1	O'Carroll Group	300 F. McNesse St 25 Lake choly in 706	337 478 5 7396	pocarrollouocarroll. com
rAnt Brsh	IMCAL		237-433-1711	gbushermal.org
John CARdone	City of LC	Lake Chaeles	491-1381	Jearpore Ocity of Le. U
LOR, Marinovich	City of LC	Lake Charles	491-1429	Imarinovich Ocityofle.



Section 106 Consulting Party Request

I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End) Thursday, October 24, 2013 |

Lake Charles Civic Center Jean Lafitte Room 900 Lakeshore Drive Lake Charles, LA 70601

SIGN-IN SHEET -	(please print)			
NAME	ORGANIZATION	ADDRESS	PHONE	EMAIL
LORI Marinovich	City of Lake Charles	P.O. Box 900 Lake Charles, 70007	337-491	Marinovich @ city of levs

Appendix B-7, Pg. 6

Page ___ of |



PUBLIC SCOPING MEETING

I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End)
Thursday, October 24, 2013 | 5:00 pm – 8:00 p.m.

Lake Charles Civic Center Jean Lafitte Room 900 Lakeshore Drive Lake Charles, LA 70601

AGENCY - ELECTED OFFICIAL - MEDIA SIGN-IN SHEET - (please print)

NAME	ORGANIZATION	ADDRESS	PHONE	EMAIL (For Project Newsletter / Updates)
Ktanhe	Octs: Marino	100 E Vermilion	337-333-1/00	
RAY MumphREY	DOTO	1201 CAPITOL ACCES RD		,
10BAL MOHAD		1000 NLAKOSL	337-532-023	8
Lee A. Bruney	Star Concrete	e 265 N. Post OAKR	337-302-2746	Labruseye yahoo.con
Nikita J. Simon	LA DOTO	Sulphu LA. 5027 Hay 90 E. LCK Charles, LA 7045	(337)	Nikita. Simon@la. gov
NoelAndoin	POTO		107 1070	noel. andowela. gov
Je Umeozulu	DOTO	1201 Capital Access Rd, B.	(225-379-1386	joachim. umeszulu Plo-gov
Didragarbid	DOTD	428 Hugh Wallis Rd., Lafayette		deidra, druithet clargov
Math young	O'Carrell DOTD Group	2001 La Cache Dr. 70601		0
		10001	311-1313	myoung @ Ocarroll. Com

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Appendix B-7, Pg. 8

PUBLIC SCOPING MEETING

I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End)
Thursday, October 24, 2013 | 5:00 pm – 8:00 p.m.

NAME	ADDRESS	PHONE	EMAIL (For Project Newsletter / Updates)
Michael Tritico	P.o. Box 233, Longo: (le, LA 7065)	(337)725-3690	11 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1
Pert Cappel	1/2 DIVISION St. LC LA 70601	337/721-8133	choils cappel @att.net
Steve Jiles	PO. 1430 LC LA 70602	337-437-9101	Steveiilesala.gov
ORNELIUS MOON	1805 CAPPY DR LC. LATOGOS	1	CURNIEMOUN3654@ CIMI
Soupert Hugnis	5080 Harin 26. Batan Range per ration	126.246- 4141	Soubert. Harris @
EN SHIELDS	1019 N 15T ALE	337-263-	Pshipsteri Roman.
Peter O'Carroll	300 E. McWeese St. Ste 2B, LC, LA 70605		Pocarrolle ocarrolle com
BOBMAHONEY		225-757-7624	robert. Mahoney Edot. go
PATRICK BERNIER	105 INDUSTRIAL DR SULPHUR LA	337 499 4067	PATRICK B & TOPCOR, COM
Lee DeRouer	516 Hilma St. Westlake		17





PUBLIC SCOPING MEETING

I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End)
Thursday, October 24, 2013 | 5:00 pm – 8:00 p.m.

Lake Charles Civic Center Jean Lafitte Room 900 Lakeshore Drive Lake Charles, LA 70601

GENERAL PUBLIC SIGN-IN SHEET - (please print)

NAME	ADDRESS	PHONE	EMAIL (For Project Newsletter / Updates)
Jim Poe		817-319-014	sim-poelkiewit-com
FARRUNG MAHMES	DON N. LAKESMOREON	5.4-377-1019	
Kirk Mache	529 Ash	527-1946	
Dong Sondra TIPA	1019 M. ISTAVE LE, LA	4335022	dstipton Caol.com.
John CARdone	City of LC	491-1381	
Perry Vincent	2713 LORIZONNE LN	436-7573	odu @LRCWIreless a
BLAKE SOTO		337 769 6156	pdv@LRCW/re/ess. Co blahesoto@naclahecharles.
Don Duberville	5827 Huy 90E LC LA 70015	337 4379102	don-duberalle @ la. sov
Therest Schmidt	KPLC-TV	377-6436	tschmidtakpletv.



PUBLIC SCOPING MEETING

I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End)
Thursday, October 24, 2013 | 5:00 pm – 8:00 p.m.

Lake Charles Civic Center Jean Lafitte Room 900 Lakeshore Drive Lake Charles, LA 70601

GENERAL PUBLIC SIGN-IN SHEET - (please print) NAME **ADDRESS** EMAIL PHONE (For Project Newsletter / Updates) Baton Rouge Jery. P. Hs @ dot. gov 225-757-7610 7225-379-1809 Keny Kemp@19.90v Borbe St, LC, LA 70001 337-526-9647 5827 they 90 Engs 337-437-905 Bobby Mullins 318-381-1947 bmullins @ M baker. com 1330 W Maylease 3t CLASE SOUTHWEST CALL CENTER COM 337-625-7613 Suddenlink, met

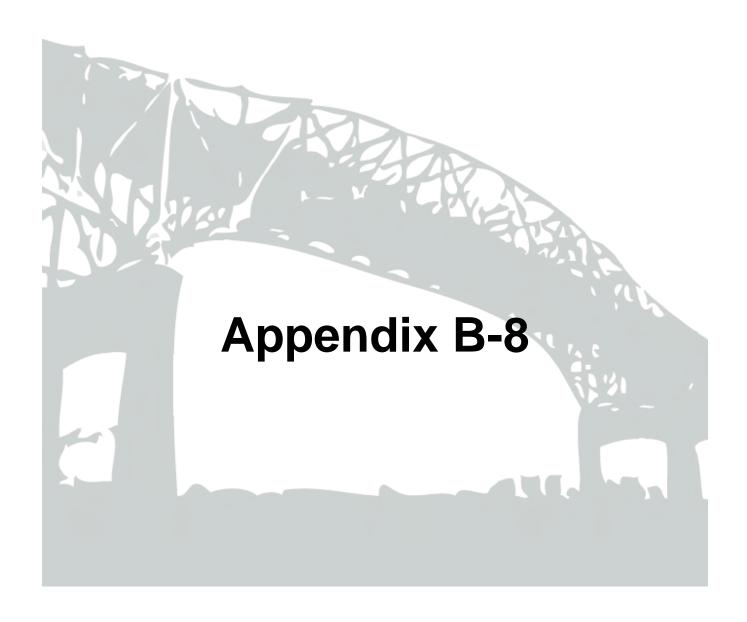


PUBLIC SCOPING MEETING

I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End) Thursday, October 24, 2013 | 5:00 pm – 8:00 p.m.

Lake Charles Civic Center Jean Lafitte Room 900 Lakeshore Drive Lake Charles, LA 70601

NAME	ADDRESS	PHONE	EMAIL (For Project Newsletter / Updates)
PAUL HUTCHENS	4931 W. WESTRINGEPARK LAKE CHARLES, LA 70605	337-319-1312	paul. hutchens eiskerp.com
Vestlake Avenue lake, LA 70669 lox 3290 Charles, LA 70602 337-430-2444 337-430-2983 utchens@islecorp.com sleofcapricasinos.com			
Paul E. Hutc Vice President / General	hens Manager		





CALCASIEU RIVER BRIDGE 1-10 LAKE CHARLES

(I-10/1-210 West to I-10/I-210 East)

Agency Scoping Meeting

Thursday, October 24, 2013

2:00 p.m. to 3:00 p.m.





I-10 Calcasieu River Bridge Project (I-10/I-210 West to I-10/210 East)

Why Are We Here Today?



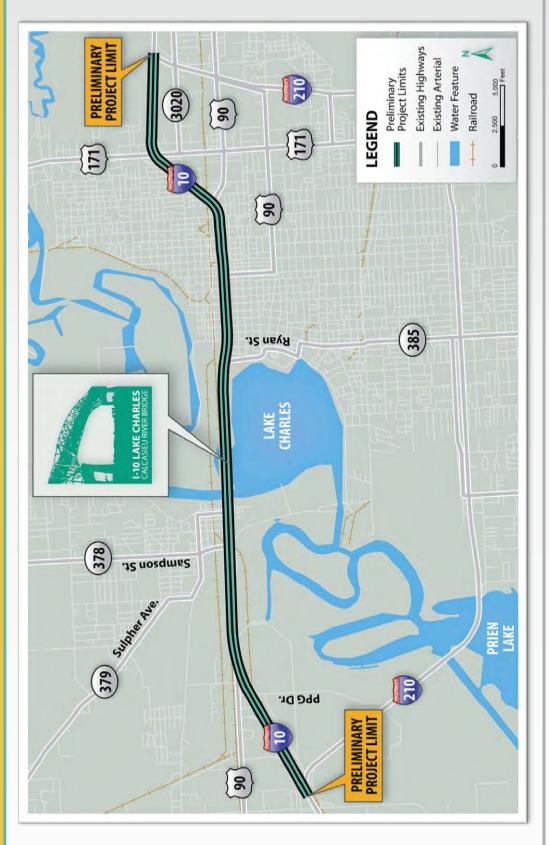
Project History

The EIS Process

Draft Purpose and Need

Draft Project Coordination Plan

Project Location Map







Project History

Construction and 1950's 1960's 1960's

- 1950's I-10 Calcasieu River Bridge is constructed as part of
- 1960's Bridge is integrated as part of I-10.

Engineering and Environmental Feasibility Study

Examines alternatives for replacement of existing high-level bridge (135-foot vertical clearance) and the existing Sampson Street Interchange.

Comprehensive Preliminary Alternatives Report + 6 Technical Memoranda

Concludes that replacement of the existing bridge on a new parallel alignment with the mid-level bridge (73-foot) is the best solution, feasible and should be advanced.

2001

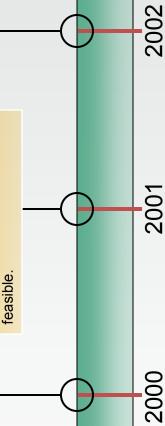
Marine Use Study

Bridge Improvements Investigated

- 1970's Due to numerous accidents,
 DOTD investigates skid resistance and
 an epoxy overlay for the bridge.
 - Epoxy overlay abandoned in favor of a bridge replacement feasibility study.

Determines mid-level (73-foot) and high-level (118 to 125-foot)

vertical clearance bridges are





1970's -

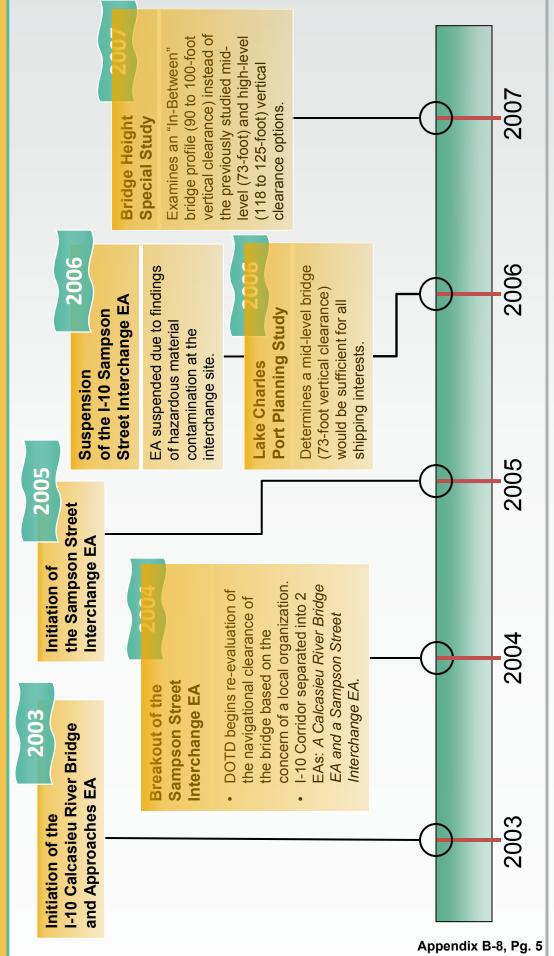
1950's -1960's

Appendix B-8, Pg. 4

1980s



Project History



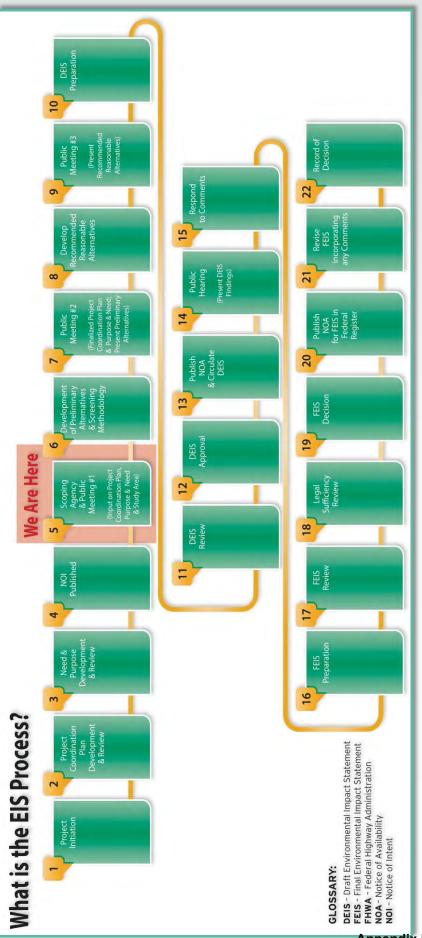


Project History

EIS will evaluate capacity improvements interchanges within the study limits. Will and study various alternatives along the to I-10 between the I-210 interchanges. IJR will include an operational analysis and other interchanges along the I-10 Calcasieu River Bridge; Sampson St. EIS will include improvements to the aid in the alternatives development 1-10 corridor, including various I-10 Calcasieu River **Bridge EIS and IJR** Present Day corridor. truss connection repairs, pin plate and repair project. Includes main spans, cleaning and spot painting DOTD completes a maintenance local areas, bridge railing repairs connection repairs on approach and resealing bridge joints. Bridge Maintenance 2012 and Repair than an EA due to the discovery of NEPA process as an EIS rather FHWA approves to restart the hazardous contamination. Approval to Re-start 2010 **NEPA Process** 2000 The Lake Charles MPO adopts the alternative and requests the DOTD proceed with development of that mid-level bridge (73-foot vertical clearance) as its preferred **IMCAL** Resolution 2008 proposal

Appendix B-8, Pg. 6

1-10 Calcasieu River Bridge Project (I-10/I-210 West to I-10/210 East)

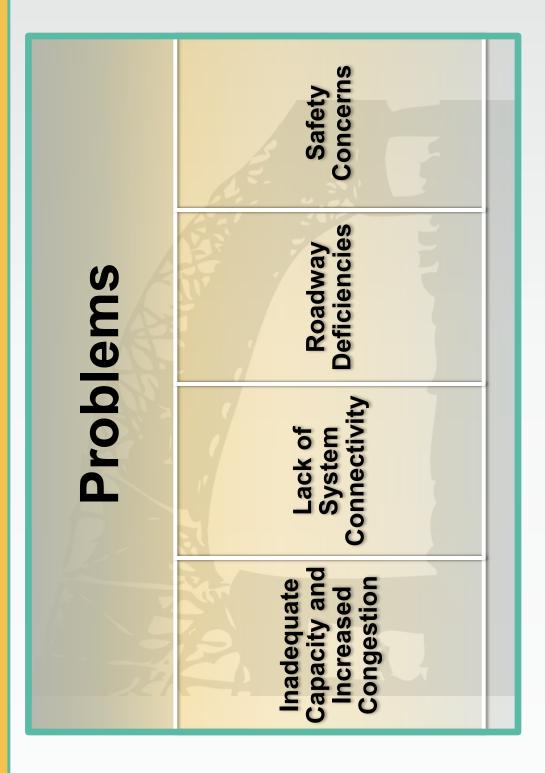




The EIS Process

I-10 Calcasieu River Bridge Project (I-10/I-210 West to I-10/210 East)

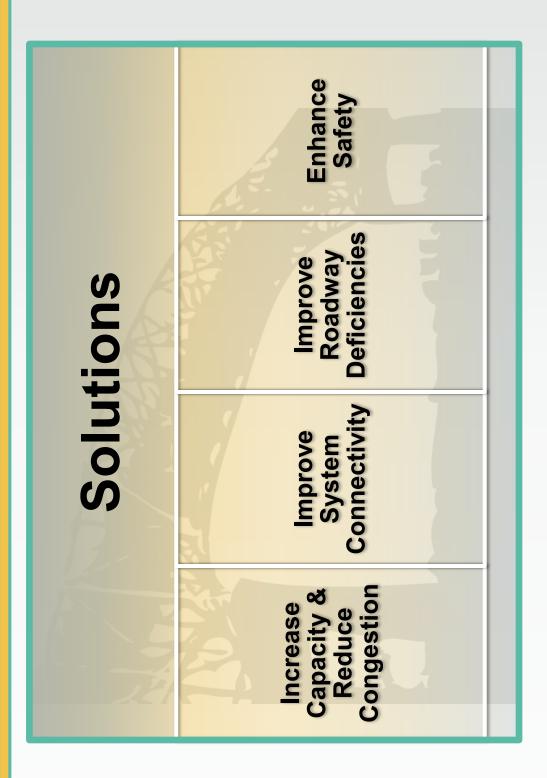
Draft Project Need





I-10 Calcasieu River Bridge Project (I-10/I-210 West to I-10/210 East)

Draft Project Purpose







Draft Project Coordination Plan

Coordination Framework

Project Management Committee (PMC)

Agency Work Group (AWG)

Lead Agencies







Consultant



Appendix B-8, Pg. 10

Lead Agencies





Cooperating Agencies







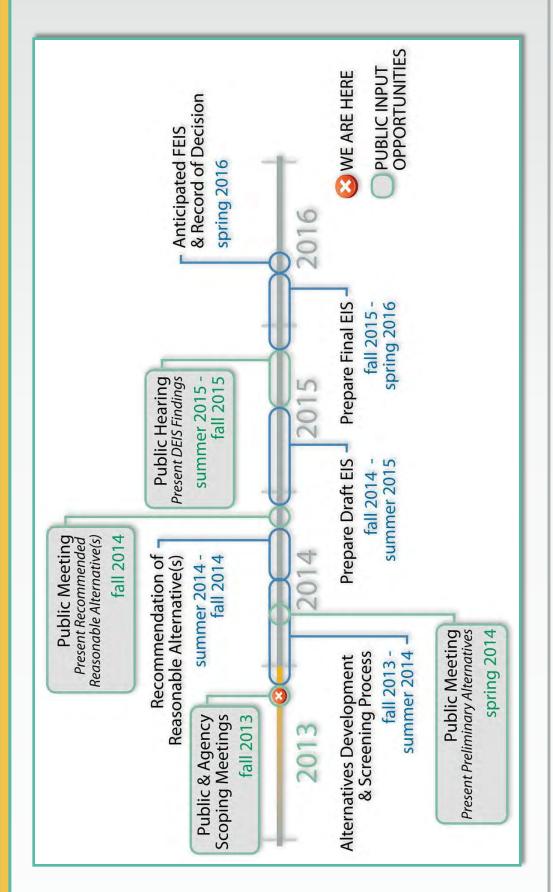
Participating Agencies

- Invites sent to Calcasieu Parish, local cities, Tribes, and Federal, State and Local Agencies
 - Participate in the AWG meetings, provide input
- Non-federal agencies are designated as participating agencies only if they accept the invitation.

I-10 Calcasieu River Bridge Project (I-10/I-210 West to I-10/210 East)

Draft Project Coordination Plan VEPA (Environmental) Timeline

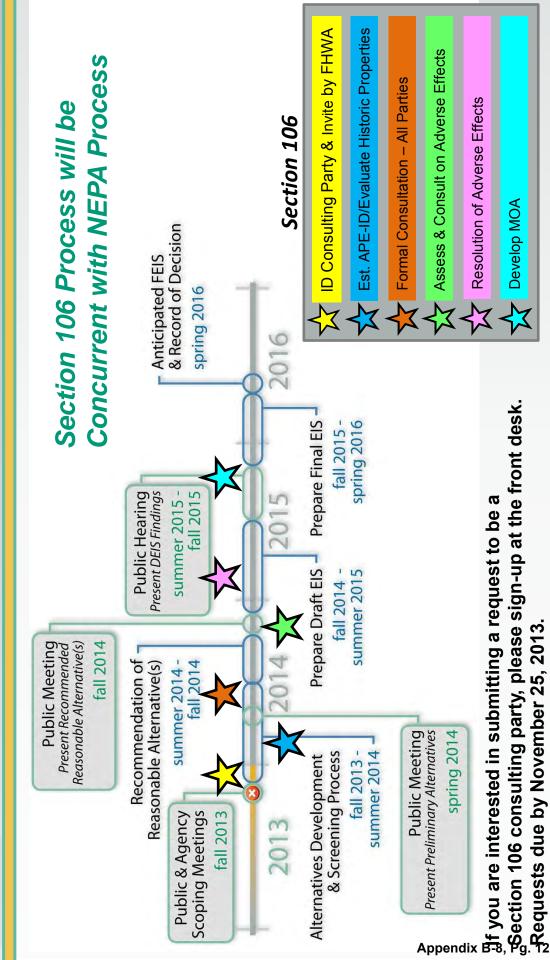
I-10 LAKE CHARLES
CALCASIEU RIVER BRIDGE





Draft Project Coordination Plan

Section 106 Process



I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE

Draft Project Coordination Plan Public Involvement Tools

3 Public Meetings & Hearing Fact Sheets

Newsletters

E-mails

Website

PUBLIC MEETING

PROJECT WEBSITE

Written comments will be accepted at this public meeting at the Written Comment Table or by mailing your comment to the following address:

Written comments will also be accepted by logging on to the project website and selecting the Public Feedback Form under the **Contact Us**

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe, Suite 640 Baton Rouge, LA 70810 **Verbal comments** will be accepted tonight at the Verbal Comment Table.

www.i10lakecharles.com

NOTE: Comments must be submitted / postmarked by Monday, November 4, 2013 to become part of the official public meeting record.



STATION 8: Let Us Hear From You





Let Us Hear From You

Next Steps:

- Develop / Refine Alternatives
- Establish Preliminary Evaluation Criteria
- Conduct an Agency Work Group (AWG) Workshop to discuss/present the above two items

Your Input is Requested:

- Comments on the Draft Purpose and Need
- Comments on the Draft Project Coordination Plan
- Input on Resources / Issues
- Input on Constraining Factors to Alternatives Development (Please See the Constraints Map on Display)
- Sign-up if you would like to request to be a Section 106 Consulting Party

Mail

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe, Suite 640 Baton Rouge, LA 70810

Appendix B-8, Pg. 14

E-Mail AMcRae@HNTB.com

Project Website
www.i10lakecharles.com
Select Contact Us – Project Feedback Form

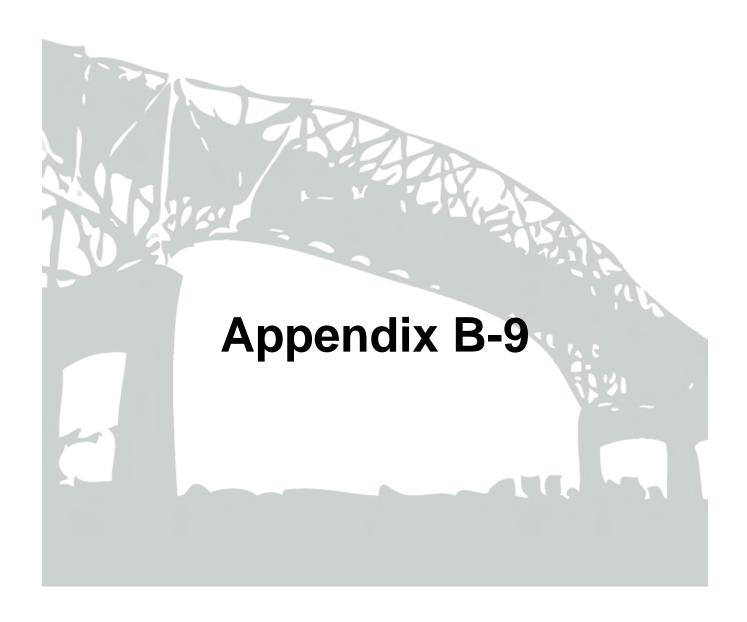


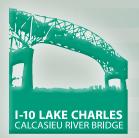
1-10 Calcasieu River Bridge (1-10/1-210 West End to 1-10/1-210 East End)

THANK YOU FOR COMING!

BEFORE YOU LEAVE

phone number and email for future project correspondence. Let us know if you would like to be a participating agency to Please review your agency contact information at the front desk and verify we have the correct contact name, address, participate in future AWG meetings!





I-10 Calcasieu River Bridge Project

I-10/I-210 West End to I-10/I-210 East End

Public Meeting Station Checklist

Sign-in here to receive future meeting notices on the I-10 Calcasieu River Bridge Project. Also sign-up here if you are interested in becoming a National Historic Preservation Act Section 106 Consulting Party (Note: Please provide reasons for requesting to be a consulting party. FHWA will make the final determination of who is accepted as a consulting party).

Station 2: Project Location Map

View the overall project location map with the proposed project limits: (I-10/I-210 West End to I-10/I-210 East End).

☐ Station 3: The EIS Process

Learn what an EIS is, why it is necessary, and how the EIS process works.

☐ Station 4: Draft Project Timeline

Throughout the EIS process, you will have several opportunities to tell us your opinion. Find out when these opportunities will occur along with the estimated timeline for EIS completion (Note: The information presented here is from the Draft Project Coordination Plan).

Station 5: Draft Purpose and Need

Review the I-10 Calcasieu River Bridge Project's Draft Purpose and Need.

☐ Station 6: Preliminary Typical Sections

The proposed project will evaluate alternatives for widening existing I-10 between the I-210 interchanges from the existing 4 through lanes (2 lanes in each direction) to 6 through lanes (3 lanes in each direction). After this Public Scoping Meeting, the **Alternatives Development and Screening Process** will begin. It is during this phase of the EIS process that various alternatives for improving capacity on I-10, as well as improvements to the Calcasieu River Bridge and Sampson Street Interchange, will be developed and screened for viability. These alternatives will be presented and you will have an opportunity to give your input at the next public meeting scheduled in spring 2014.

Station 7: Constraints Mapping

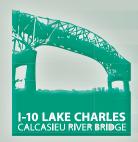
Constraints are any environmental, topographical or other consideration that may affect the location, development or other aspect of a project within the study area. Constraints are identified to ensure a comprehensive understanding of the study area. Feel free to draw, outline or note any potential constraining factors directly on the maps provided at this station.

Station 8: Let Us Hear From You

Take the opportunity to fill out a written comment form or provide a verbal comment on the items presented at this public meeting. Be sure to ask the project team if you have any unanswered questions. Also learn about other ways to provide comments on the proposed project, such as through the project website at www.i10lakecharles.com.







I-10 Calcasieu River Bridge Project

I-10/I-210 West End to I-10/I-210 East End







Timeline	Action		
1950's	■ The I-10 Calcasieu River Bridge is originally constructed as a part of US 90.		
1960's	■ The I-10 Calcasieu River Bridge is integrated as part of I-10.		
1970's	 Due to numerous accidents on the bridge, the DOTD investigates skid resistance and an epoxy overlay for the bridge. Bridge inspections identify areas of concern. 		
1980's	 Consideration of an epoxy overlay is abandoned in favor of a bridge replacement feasibility study. 		
2000	 The DOTD initiates an engineering and environmental feasibility study for an area of I-10 extending from PPG Drive to US 90. The feasibility study examines numerous project alternatives for replacement of the existing high-level bridge (135-foot vertical clearance for ship traffic) with different bridge profiles and heights, as well as replacing the existing Sampson Street interchange (including geometric improvements to current standards and a grade separation with the adjacent Union Pacific mainline railroad). The first public meeting is held. 		
2001	■ A Marine Use Study determines that mid-level (73-foot) and high-level (118 – 125-foot) vertical clearance bridges are feasible.		
2002	 Six technical memoranda and a Comprehensive Preliminary Alternatives Report are prepared and conclude that replacement of the existing bridge on a new parallel alignment with a lower level bridge (73-foot vertical clearance) is the best solution, is feasible and should be advanced. The second public meeting is held to present the findings. 		
2002	■ In accordance with NEPA, the I-10 Calcasieu River Bridge and Approaches EA is initiated.		
2003			
2004	■ The first public meeting is held for the I-10 Calcasieu River Bridge and Approaches EA.		
	 The DOTD begins a re-evaluation of the navigational clearance based on the concern of a local organization. 		
	 As to not delay improvements to the Sampson Street interchange during the bridge height resolution process, the DOTD separates the Calcasieu River Bridge component and the Sampson Street interchange component of the overall I-10 corridor into two separate EAs. 		
2005	■ The I-10 Sampson Street Interchange EA is initiated and the public meeting is held.		
2006	■ A Lake Charles Port Planning Study is prepared and determines that the mid-level bridge (73-foot vertical clearance) would be sufficient for all reasonable shipping interests.		
	■ The I-10 Sampson Street Interchange EA is suspended pending resolution of a hazardous contamination matter near the existing interchange.		
	 Based on public comments, a Bridge Height Special Study is prepared with the purpose of examining an "In-Between" bridge profile 		

2007

Based on public comments, a Bridge Height Special Study is prepared with the purpose of examining an "In-Between" bridge profile (approximately 90-100-foot vertical clearance) to replace the I-10 Calcasieu River Bridge instead of the previously studied mid-level (73-foot) and high-level (118-125-foot) bridge profiles.

2008

The Lake Charles MPO (IMCAL) adopts the mid-level bridge (73-foot vertical clearance) as its preferred alternative and requests the DOTD proceed with development of that proposal.

2010

FHWA approves to restart the NEPA process as an EIS rather than an EA due to the discovery of hazardous contamination. Note: An EIS is prepared for major federal actions that significantly affect the environment.

2012

The DOTD completes a maintenance and repair project. Includes main truss connection repairs, pin plate connection repairs on approach spans, cleaning and spot painting local areas, bridge railing repairs, and resealing bridge joints.

Present

The DOTD and the FHWA publish a NOI to prepare an EIS for capacity improvements to I-10 between the I-210 interchanges, which includes the Calcasieu River Bridge and Sampson Street interchange.

Glossary of Terms

Louisman Department of Transportation and Development EA = Environmental Assessment

EIS = Environmental Impact Statement

IMCAL = Imperial Calcasieu Regional Planning and Development Commission

MPO = Metropolitan Planning Organization

NEPA = National Environmental Policy Act of 1969

NOI = Notice of Intent

What is the I-10 Calcasieu River Bridge Project?

The proposed project includes the evaluation of improvements to I-10 between the I-10/I-210 west and I-10/I-210 east interchanges, including over the Calcasieu River Bridge, in Lake Charles, LA. The total project length is approximately 9 miles and includes the roadway and bridge approaches. The primary purpose of the proposed project is to improve traffic congestion, but the project will also address safety and roadway/bridge design issues.

A feasibility study for the project was completed in 2004. Because of the potential for impacts and issues associated with various socioeconomic and environmental resources, and the high level of public interest, the Federal Highway Administration (FHWA) and the Louisiana Department of Transportation and Development (DOTD) have initiated the preparation of an Environmental Impact Statement (EIS) for the proposed project.

What is an EIS?

An EIS is a full-disclosure document that details the process through which a transportation project was developed, includes a considerable range of reasonable alternatives, analyzes the potential impacts resulting from the alternative and demonstrates compliance with applicable environmental laws, as well as provides a means for public input into the decision making process. The EIS is carried out for major federal actions in response to the National Environmental Policy Act of 1969 (NEPA). An EIS is the most thorough and comprehensive level of NEPA documentation. The EIS process is completed in the following major steps: Notice of Intent (NOI), Draft EIS, Public Hearing, Final EIS, Record of Decision (ROD). New laws now allow the option of combining the Final EIS and the ROD.

Why is the proposed project needed?

Inadequate Capacity and Increased Congestion

• Existing traffic volumes exceed existing capacity and in the future, traffic volumes are expected to continue to increase.

Lack of System Connectivity

 Existing I-10 within the project limits is 2 lanes in each direction, whereas I-10 outside the project limits is 3 lanes in each direction. This creates a lack of connectivity and continuity on I-10.

Roadway Deficiencies

 Existing load limit, lack of shoulders and vertical geometry of bridge do not meet current roadway design criteria.

Safety Concerns

- Safety is compromised by steep bridge grades, limited maneuverability (*lack of shoulders*), and bottlenecks generated from the transition from 3 to 2 lanes on I-10 within the project limits.
- Existing low vertical clearance of bridge has resulted in over-height vehicle collisions.

What are some of the proposed improvements to be investigated?

- Designing the proposed bridge structure to accommodate 3 travel lanes and 1 auxiliary lane, with inside and outside shoulders and potential frontage roads in each direction
- Lowering the height of the bridge
- Reducing the existing 420 foot truss span of the bridge to 2 main spans
- Beyond the bridge limits, reconstructing the I-10 mainlanes to accommodate 3 travel lanes in each direction
- Redesigning the Sampson Street interchange including review of crossings with existing railroads
- Redesigning access to and from I-10 on the west side of the bridge between Sampson Street and PPG Drive and near the east end of the bridge
- Consideration of frontage roads from PPG Drive to US 90 East

Will I have any input on the project?

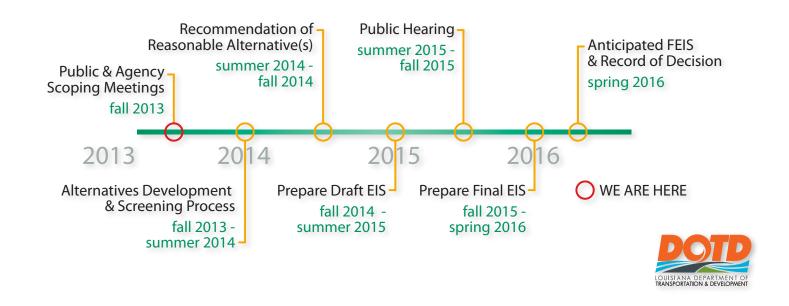
Several public meetings will be held to solicit comments and suggestions from the public. Comments may also be submitted, and questions answered, by logging on to the project website at www.i10lakecharles.com and selecting Contact Us.

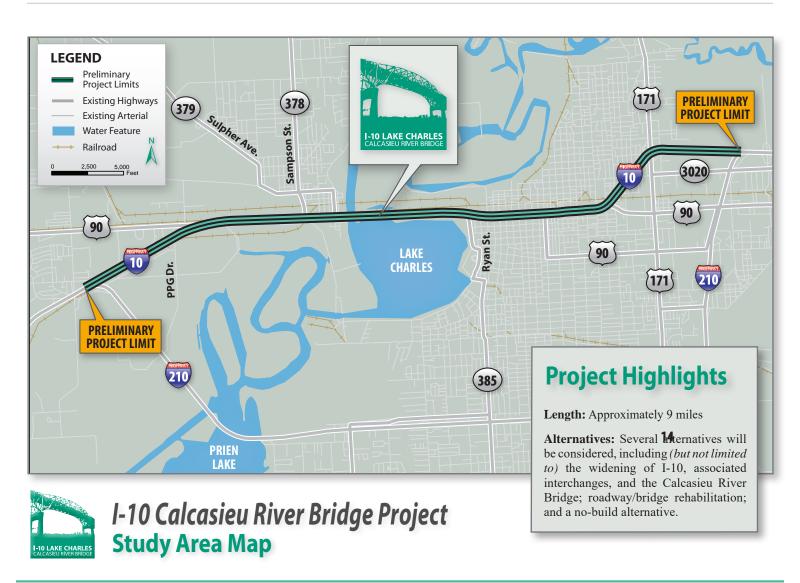


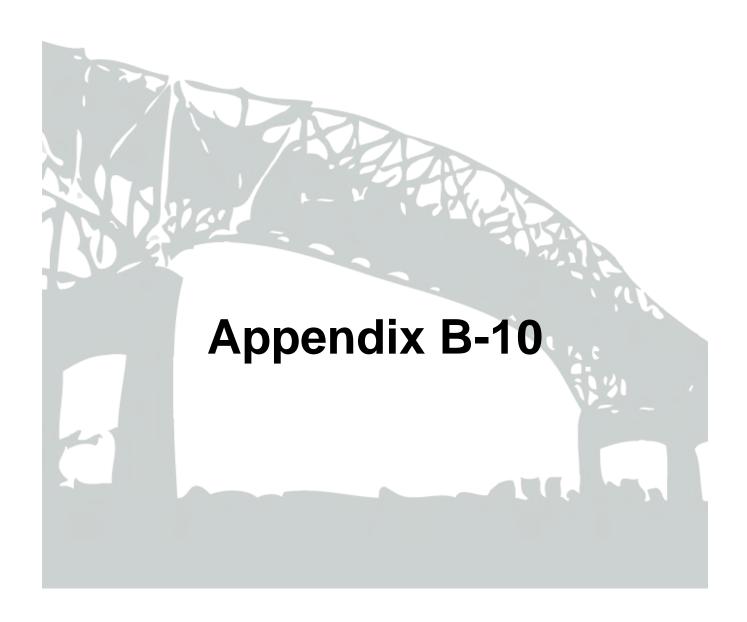


U.S. Department of Transportation
Federal Highway Administration

Preliminary NEPA Timeline







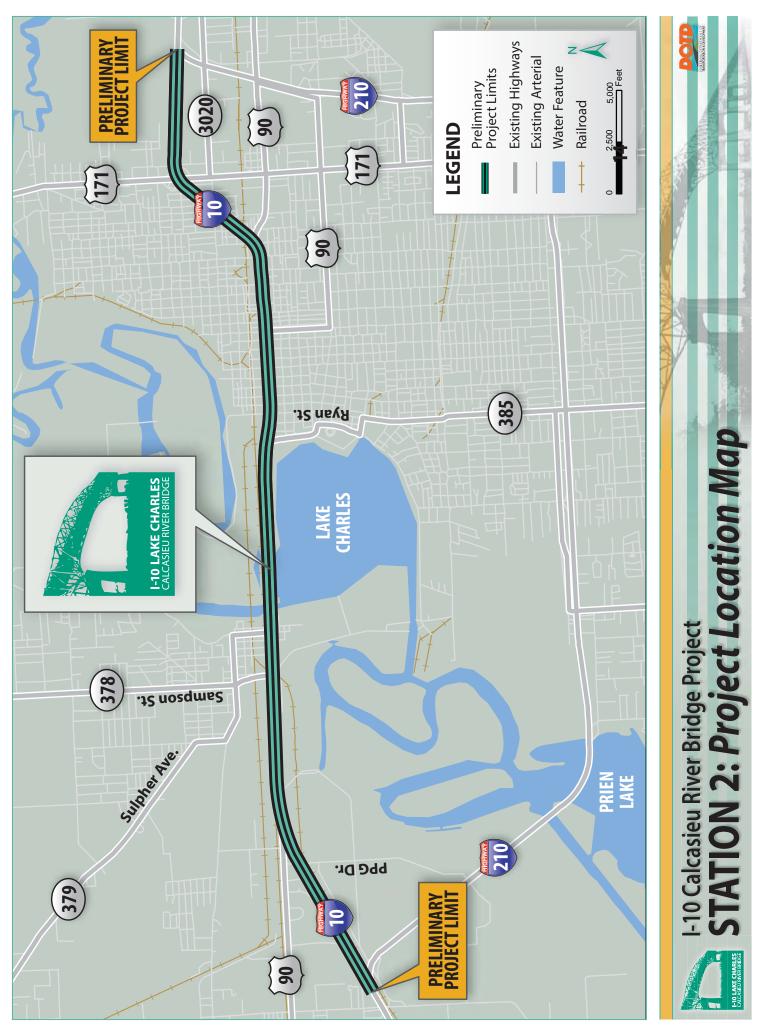
Public Meeting at the Front Desk Please Sign-In for the

Vational Historic Preservation Act Section 106 of the

If you are interested in submitting a request to become a consulting party, please sign-up at the front desk.







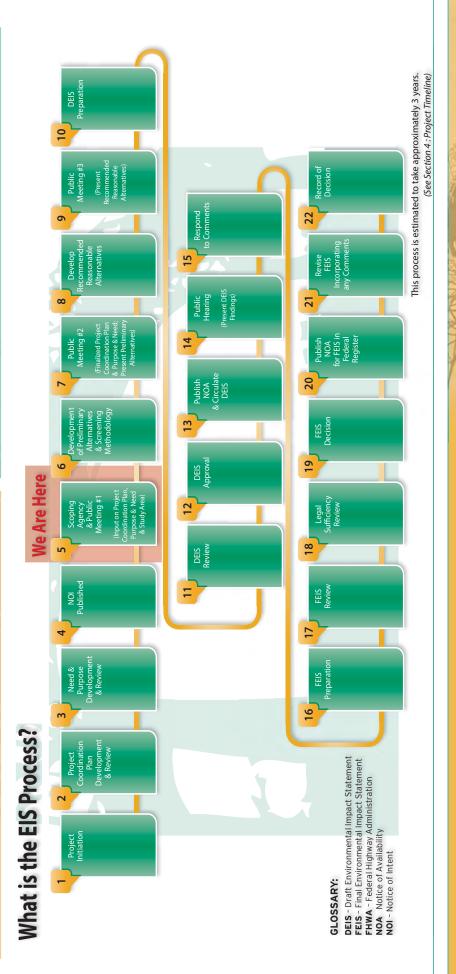
Appendix B-10, Pg. 2

What is an EIS?

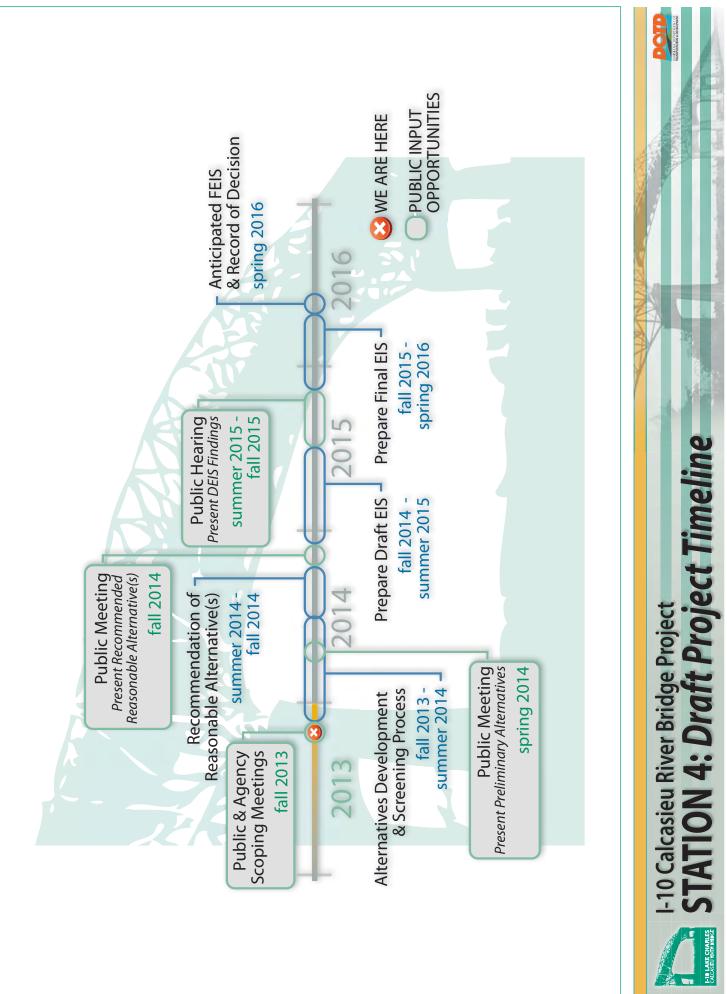
An Environmental Impact Statement (EIS) is a full-disclosure document that details the process through which a transportation project is developed. It includes a considerable range of alternatives, demonstrates compliance with environmental laws and provides a means for public input into the decision making process.

Why Prepare an EIS?

An EIS is carried out for major federal actions that significantly affect the quality of the environment. An EIS is prepared in accordance with the National Environmental Policy Act of 1969 (NEPA), which establishes a process for analyzing and disclosing the impacts of federal actions on the environment.









Inadequate Capacity & Increased Congestion

Existing traffic volumes exceed capacity, and traffic volumes are anticipated to increase in the future.

Lack of System Connectivity

Existing I-10 is generally 2-thru lanes in each direction within the I-210 interchanges, and 3-thru lanes outside these interchanges.

Roadway Deficiencies

Existing load limit, lack of shoulders and vertical geometry of bridge do not meet current roadway design criteria. Existing at-grade railroad crossings at Sampson Street adversely affect traffic operations.

Safety Concerns

Safety is compromised due to the at-grade railroad crossings at Sampson Street; bottlenecks and a lack of shoulders throughout the project corridor; and steep bridge grades and low vertical clearance of the bridge.

Do you have comments on the Draft Purpose & Need? Please be sure to provide your written or verbal comments tonight, or log on to the project website at www.i10lakecharles.com and fill out the Project Feedback Form!

PURPOSE

(Solutions)

Increase Capacity and Reduce Congestion

By providing additional infrastructure to I-10, including improvements to the Calcasieu River Bridge, to accommodate growth and aid in congestion relief.

Improve System Connectivity

By providing a consistent number of through lanes both within and outside the I-210 interchanges.

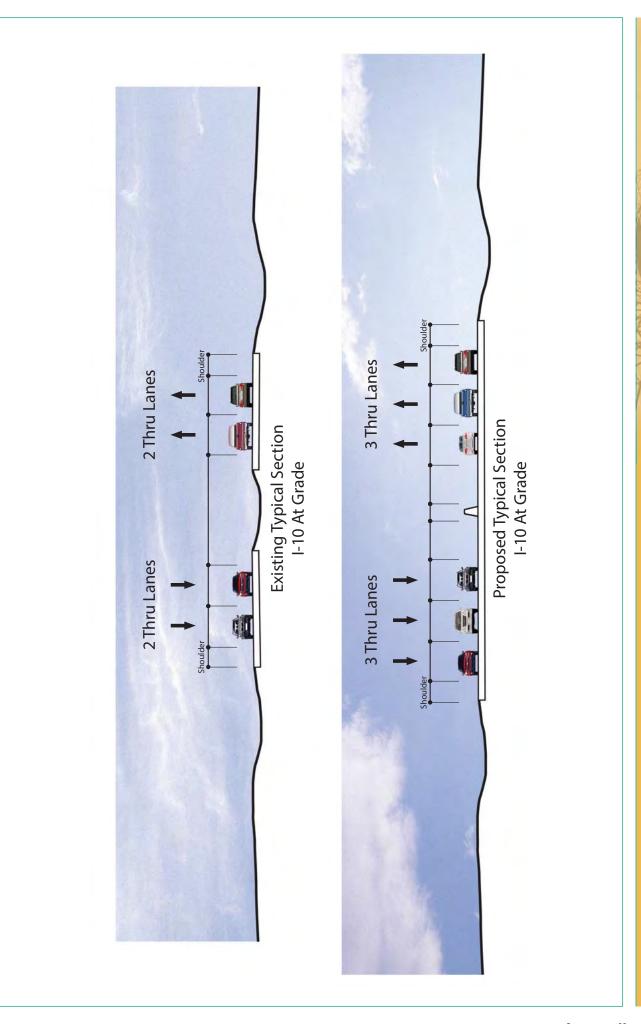
Improve Roadway Deficiencies and Safety

By providing infrastructure improvements to I-10 and the Calcasieu River Bridge, redesigning the at-grade railroad crossings at Sampson Street, and improving access ramps to and from I-10 within the project limits.

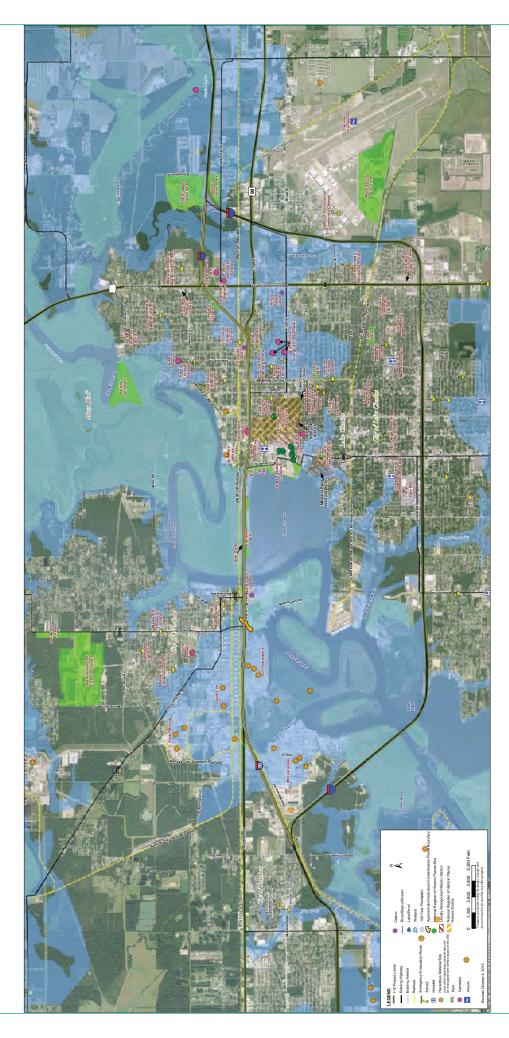


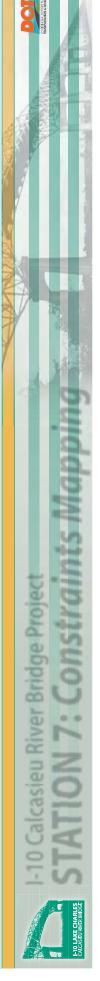
STATION 5: Draft Purpose and Need 1-10 Calcasieu River Bridge Project











PUBLIC MEETING

Written comments will be accepted at this public meeting at the Written Comment Table or by mailing your comment to the following address:

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe, Suite 640 Baton Rouge, LA 70810 **Verbal comments** will be accepted tonight at the Verbal Comment Table.

PROJECT WEBSITE

Written comments will also be accepted by logging on to the project website and selecting the Public Feedback Form under the **Contact Us**

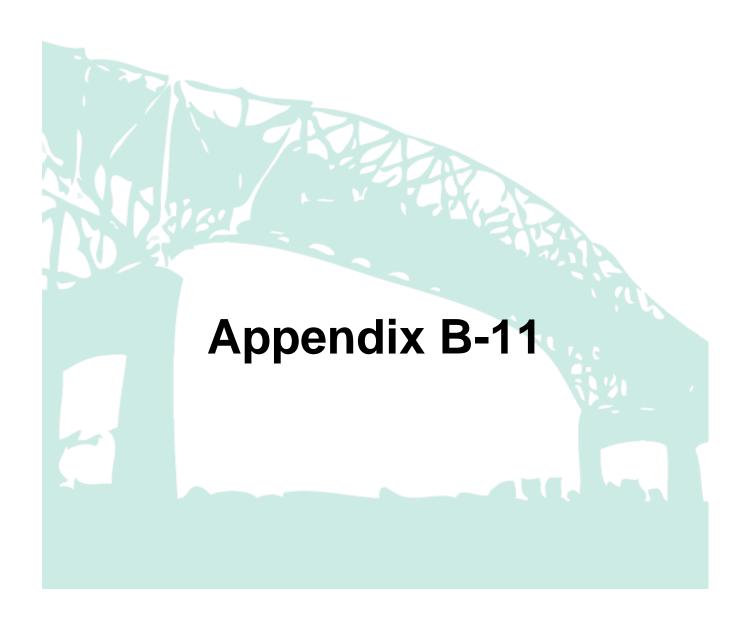


www.i10lakecharles.com

NOTE: Comments must be submitted / postmarked by Monday, November 4, 2013 to become part of the official public meeting record.



STATION 8: Let Us Hear From You 1-10 Calcasieu River Bridge Project





Jos C.
FYI Scon -

December 20, 2007

Mr. Johnny Bradberry, Secretary LA DOTD P. O. Box 94245 Baton Rouge, LA 70804-9245

Dear Secretary Bradberry:

The Lake Charles Area Metropolitan Planning Organization (MPO) Policy Committee, in a meeting conducted on Wednesday, December 19, 2007, voted in favor of a 73-foot vertical clearance for the new Interstate 10 Calcasieu River Bridge. This vote concurs the MPO support of a LA. DOTD study and report recommending a bridge height of a 73-foot clearance for marine traffic.

The MPO looks forward to working with the DOTD in helping to clear the environmental issues and obtaining the construction funding.

Sincerely

Ron LeLeux, Mayor, City of Sulphur

Chairman, Transportation Policy Committee

Lake Charles Urbanized Area

RL/AF/ddt

cc:

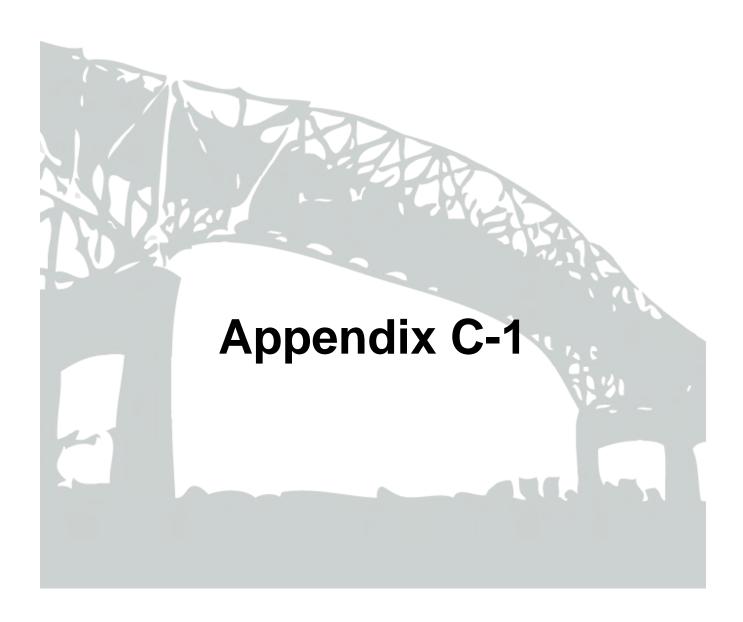
MPO Policy Committee Members SW Louisiana Delegation Eric Kalivoda, Assistant Secretary, DOTD Tony Ducote, Mega Projects, DOTD James Porter, Executive Director, IMCAL

Abe Fontenot, Transportation Director, Lake Charles MPO

Appendix C

Comments

	NOI Comments	C-1
Agency	Scoping Meeting Written Comments	C-2
Public	Scoping Meeting Written Comments	C-3



April English

To:

Kate Prejean

Subject:

RE: NOI for EIS I-10 Calcasieu River Bridge Project (H003931)

From: Leslie Barras [mailto:lebarras@gmail.com]

Sent: Friday, August 09, 2013 10:57 AM

To: Highsmith, Carl (FHWA)

Cc: Mahoney, Robert (FHWA); arturo.aquirre@dot.gov; jan.grenfell@la.gov; nhmorris@crt.la.gov; mvarnado@crt.la.gov

Subject: NOI for EIS Calcasieu River Project

Dear Mr. Highsmith:

I am responding to the Louisiana Division's Notice of Intent to prepare an Environmental Impact Statement (EIS) for the Calcasieu Parish I-10/bridge project that was published in the August 1 Federal Register.

I would like to be involved in the NEPA scoping and review process for this project. I and my family routinely travel to and from Lake Charles for business, shopping, and recreation and, therefore, are vitally affected by this transportation system and proposed improvements.

I am also hereby requesting status as a consulting party in the Section 106 process of the National Historic Preservation Act because of my concerns and interest in preserving the existing bridge. As you are aware, the existing I-10 Calcasieu River Bridge is identified on the FHWA's list of Nationally Significant Features of the Federal Interstate Highway System (and, therefore, has been removed from the federal interstate highway exemption under Section 106/Section 4f, or, in other words, is subject to Section 106/Section 4f review in the pending project). Additionally, the bridge was determined as eligible for the National Register in 2006.

Also, I would like to call to FHWA's attention an innovative capacity expansion of another truss bridge of the same era as the I-10 bridge. I took the two attached photos of the Waitemata Harbour Bridge (Auckland Harbour Bridge) in the center of Auckland, New Zealand (population 1.4 million). It is the longest bridge in the country and is also known as the "clip on" bridge because of the addition of lanes in the late 1960s on either side of the original bridge. The link below (from the website of Engineering Heritage New Zealand) provides an engineering overview of the "clip on" project and lessons learned. I am hopeful that innovative approaches will be implemented in the pending project.

http://www.ipenz.org.nz/heritage/itemdetail.cfm?itemid=117

I look forward to your response to my request for consulting party status and the opportunity to participate in this important project.

Sincerely, Ms. Leslie Barras 912 W. Cypress Avenue Orange, TX 77630 409-768-4797 or 502-298-1505 October 9, 2013

Sent via email to kbprejean@hntb.com

Ms. Kate Prejean HNTB Corporation 10000 Perkins Rowe, Ste. 640 Baton Rouge, LA 70810

RE: State Project No.: H.003931.2

Federal Aid Project No.: BR-10-1(212)29 I-10 Calcasieu River Bridge, Calcasieu Parish

Dear Ms. Prejean:

This submittal provides preliminary comments on the above-referenced project. Although I live in Orange, I frequently cross the Calcasieu River via the historic bridge in order to conduct business and shop in Lake Charles and as a through-way to Lafayette, Baton Rouge, and beyond. Additionally, with the replacement of the I-10 Neches River Bridge in Beaumont underway, and the planned construction to rehabilitate and expand the capacity of I-10 through Orange County to the Sabine River, it is easier for us to shop in Lake Charles than to drive to Beaumont, a situation that will persist for many years. As explained below, I would like to see the unique, historic bridge preserved and improved in this project. The rehabilitation alternative would support the public's existing investment in this component of interstate infrastructure and maintain an iconic feature of the Lake Charles skyline.

I have briefly reviewed the Comprehensive Preliminary Alternatives Report (HNTB Corporation et al, May 2002, hereafter "HNTB 2002") as well as the project website. These preliminary comments assume that the project would be funded in large part by federal highway-aid monies and, thus, that the Federal Highway Administration (FHWA) will be a key decision maker in this proposal.

Purpose and Need for the Project

The purpose and need for the project is described as replacing the existing Calcasieu River Bridge (HNTB 2002, Summary, Section 1.2), and three of the four bridge alignment alternatives that were costed in Table 3-1 include demolition costs for the existing bridge. The absolute statement that this project is to replace the existing bridge is not an acceptable way to frame the project purpose and need under the National Environmental Policy Act (NEPA) and Section 106 of the National Historic Preservation Act (NHPA). The purpose and need should be stated, and backed up with

recent, quantitative data, with respect to legitimate purposes of the federal interstate highway system (e.g., enhance and improve mobility, increase safety, support local economic development initiatives). The array of alternatives that address these purported needs should then be developed, including preserving the existing bridge. Subsequent scoping and environmental review documentation presented to the public needs to properly frame the purpose and need as justified by the data.

The HNTB 2002 report does not present any traffic or safety data (that I could find)—current and horizon year—that would help me and other members of the public understand the current and projected demands for cross-river mobility. Additionally, the traffic data needs to distinguish between through-traffic and local traffic (by direction and peak-hour) since the split would be helpful in identifying and assessing alternatives. Significant new traffic generators need to be evaluated as well, particularly the proposed \$600 million Golden Nugget Casino and Resort planned next to L'Auberge du Lac Hotel and Casino at the near-west side of the existing bridge and river crossing.

Historic Calcasieu River Bridge

The existing bridge was declared eligible for the National Register of Historic Places in 2006 and placed on the List of Nationally and Exceptionally Significant Features of the Interstate Highway System that same year. Thus, compliance with both Section 106 of the NHPA and Section 4(f) (49 U.S. Code § 303) is required. Section 4(f) prohibits the destruction of historic bridges and other protected sites, unless it can be demonstrated that there is "no feasible and prudent alternative" to the demolition, and the project includes "all possible planning" to minimize harm. Before selecting an alternative that would destroy the existing historic bridge, the case law² and implementing regulations impose a very high legal hurdle upon the FHWA (and LaDOTD): they must find that preservation of the existing bridge poses "unique problems or unusual factors" or that the cost, social and economic impacts, or community disruption resulting from preserving the bridge would reach "an *extraordinary magnitude*." 23 C.F.R. § 774.17 (emphasis added). If a prudent and feasible alternative exists that involves using the historic bridge—as has been presented in the HNTB 2002 report (see below), the FHWA must select that alternative.

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¹See NEPA and Transportation Decisionmaking: The Importance of Purpose and Need in Environmental Documents, www.environmental.fhwa.dot.gov/projdec/tdmneed.asp at 1 ("FWHA Guidance"). The FHWA Guidance states that data should be provided to substantiate a variety of factors relating to purpose and need, including a reduction in vehicle hours of travel, improvements in travel speeds, reduction in travel accidents, savings in cost to the traveling public, enhanced economic development potential, increased tax base, improved access to public facilities, and the like. Id. at 4.

²See *Citizens to Preserve Overton Park Inc. v. Volpe*, 401 U.S. 402, 413 (1971).

Alternatives

Additional alternatives need to be identified and evaluated. In an email to the FHWA dated August 9, 2013, I submitted a completed example (including photos that I took) of an innovative capacity expansion of another truss bridge of the same era as the I-10 bridge. The project is the Waitemata Harbour Bridge (Auckland Harbour Bridge) in the center of Auckland, New Zealand (population 1.4 million). It is the longest bridge in the country and the expansion is known as the "clip on" bridge because of the addition of lanes in the late 1960s on either side of the original bridge. The link http://www.ipenz.org.nz/heritage/itemdetail.cfm?itemid=117 (from the website of Engineering Heritage New Zealand) provides an engineering overview of the "clip on" project and lessons learned. This option needs to be added to the alternatives that are analyzed for preserving and enhancing the public's existing investment in the I-10 historic bridge.

Of the alternatives that were reviewed in the HNTB 2002 report, only Alignment 3, Bridge Concept D (new six-lane upriver bridge, rehabilitation and continued use of the existing bridge for local access/frontage roads) preserves the historic bridge while providing increased capacity, especially for through-traffic. This alternative is recognized in the 2002 report as the "most desirable" from a constructability standpoint (p. 2-35). It would also improve access into the Lake Charles public beach, Lakeshore Drive, and the historic downtown by reconstructing the east exit off the existing bridge into a boulevard-type arterial that provides better connectivity for local traffic. This option would also provide the full capacity of the existing bridge for maintenance of traffic during construction of new lanes upriver. For all of these reasons, Alignment 3, Bridge Concept D should be advanced into an in-depth engineering review and consideration in the NEPA process.

Additionally, a variation on this option should be included, which would consist of using the existing historic bridge for through-traffic and constructing lower-elevation frontage roads on either side of the existing bridge for local traffic. As noted in the HNTB 2002 report, the navigational clearance for new crossings is 77.3 feet (73 ft. above a 4.3-ft. NGVD), thus providing a less-expensive new construction option to accommodate local traffic. On the south side, the new frontage crossing could tie into the existing at-grade frontage road for the beach. On the north side, there may be conflicts with pipe racks associated with the petrochemical plants, but these physical constraints likely need to be evaluated anyway.

In closing, I appreciate the opportunity to provide these preliminary comments on this

project proposal. Please ensure that these comments are included in the administrative record for this proposed action.

Sincerely,

Leslie Barras

912 W. Cypress Avenue

Lylin E. Barras

Orange, TX 77630

lebarras@gmail.com

409-768-0747

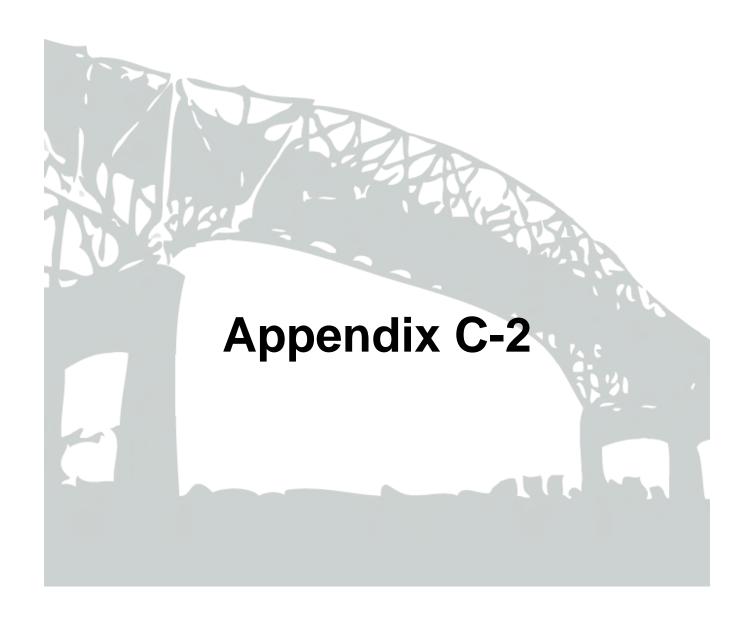
c: Noel A. Ardoin, P.E., LaDOTD

Robert Mahoney, FHWA, Louisiana Division

Kitty Henderson, Historic Bridge Foundation

Elizabeth Merritt, National Trust for Historic Preservation

Carol Legard, Advisory Council on Historic Preservation





I-10/I-210 West End to I-10/I-210 East End

Public Meeting Comment Form

Your insight and concerns are of key importance to the I-10 Calcasieu River Bridge Project Team. Please take the time to document your comments or questions below. After completion and before you leave today's meeting, please provide this questionnaire to the Project Team. You can also mail your written comments to the address at the bottom of this form. Comments received by **Monday, November 4, 2013** will be included as part of the official record.

PLEASE COMMENT ON THE FOLLOWING ITEMS	
Project Purpose & Need - What are the key reasons for th	is project? Additional reasons?
· Safety section - overpasse	s width/shoulder
· Geometrics/alignment - Speed · # of travel Lanes	
* + b+ +rquel hanes	TOTAL AND THE STATE OF THE STAT
Project Coordination Plan – Please let us know whatake place throughout the EIS process (listed below	t you think about the public involvement efforts that will). Do you have any additional comments/suggestions?
3 public meetings (including this one) and one public h process. You will have the opportunity to comment at	nearing which will take place at key stages throughout the EIS each of these meetings;
Agency Work Group meetings will be held throughout ficials;	the EIS process to gain the insight of agency, public and elected of-
\Diamond The project website will be updated regularly with mat	erials pertinent to the EIS process;
You will have the ability to provide feedback/submit qu	estions on the project website at any time throughout the EIS process;
Quarterly project newsletters will be distributed to inte	rested parties via email (or by mail upon request); and
Project meetings with local and community groups car	he held as needed
Do you have any other comments, questions or concerns of the Coordination with 210 ma	
Would you like to receive future updates on the I-10 Please include your contact information for the official project record. Anonymous comments	Name: John S. BRuck
cannot be verified.	Address:
Comments may be made online at www.i10lakecharles.com, submitted at the public meetings, or by U.S. Mail to I-10 Calcasieu River Bridge Project, c/o HNTB	Email: Sbruce@ Sulphyr.org
Corporation, 10000 Perkins Rowe, Suite 640, Baton Rouge, LA 70810.	Agency: City of Sulphur



Corporation, 10000 Perkins Rowe, Suite 640, Baton Rouge, LA 70810.

I-10 Calcasieu River Bridge Project (I-10/I-210 West End to I-10/I-210 East End)

I-10 Calcasieu River Bridge Project

I-10/I-210 West End to I-10/I-210 East End

Public Meeting Comment Form

Your insight and concerns are of key importance to the I-10 Calcasieu River Bridge Project Team. Please take the time to document your comments or questions below. After completion and before you leave today's meeting, please provide this questionnaire to the Project Team. You can also mail your written comments to the address at the bottom of this form. Comments received by **Monday, November 4, 2013** will be included as part of the official record.

PLEASE COMMENT ON THE FOLLOWING ITEMS: Project Purpose & Need - What are the key reasons for this project? Additional reasons? Project Coordination Plan - Please let us know what you think about the public involvement efforts that will take place throughout the EIS process (listed below). Do you have any additional comments/suggestions? 3 public meetings (including this one) and one public hearing which will take place at key stages throughout the EIS process. You will have the opportunity to comment at each of these meetings; Agency Work Group meetings will be held throughout the EIS process to gain the insight of agency, public and elected officials: The project website will be updated regularly with materials pertinent to the EIS process; You will have the ability to provide feedback/submit questions on the project website at any time throughout the EIS process; Quarterly project newsletters will be distributed to interested parties via email (or by mail upon request); and Project meetings with local and community groups can be held as needed. Do you have any other comments, questions or concerns (continued on back): Would you like to receive future updates on the I-10 Calcasieu River Bridge Project? ORI MerinoxICA Please include your contact information for the Name: official project record. Anonymous comments P.O.Bax 900, Lake Charles, 70602 Address: cannot be verified. lmarinovich @citaofle. US Comments may be made online at www.i10lakecharles.com, submitted at the Email: public meetings, or by U.S. Mail to I-10 Calcasieu River Bridge Project, c/o HNTB

Agency:

ity of Lake Charles

Agency Scoping Meeting Comment ID #	Name and Organization	Location
ID #3	Bill Shearman Chairman, DDA, City of Lake Charles	See Appendix A, ID #9
ID #4	Lynn F. Thibodeaux Clerk of the council, City of Lake Charles, Office of the City Council	See Appendix A, ID #10



I-10/I-210 West End to I-10/I-210 East End

Public Meeting Comment Form

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Project Coordination Plan – Please let us know what take place throughout the EIS process (listed below).	you think about the public involvement efforts that will Do you have any additional comments/suggestions?
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The project website will be updated regularly with mate	rials pertinent to the EIS process;
You will have the ability to provide feedback/submit que	stions on the project website at any time throughout the EIS process;
Quarterly project newsletters will be distributed to interest	ested parties via email (or by mail upon request); and
Project meetings with local and community groups can l	be held as needed.
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Do you have any other comments, questions or conce	erns (continued on back):
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AS THE TRINITY RIVER	BRIDGE NEAR ANUACH, T)
ON 1-10 (FOR FUTURE	DEVELOPMENT ON THE Coher
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Please include your contact information for the	Name: (TOE TOURS
official project record. Anonymous comments	000 111 111 1
cannot be verified.	Address: 908 MICHAEL LANE
Comments may be made online at www.i10lakecharles.com, submitted at the public meetings, or by U.S. Mail to I-10 Calcasieu River Bridge Project, c/o HNTB	Email: itoups@cityosc.us
Corporation, 10000 Perkins Rowe, Suite 640, Baton Rouge, LA 70810.	Agency:

Comments, questions or concerns (continued from front):
CALCASIEU RIVER. WE DO NOT
WANT TO LIMIT EXPANSION AND
DEVELOPMENT NORTH OF THE BRIDGE
AND LIMIT OUR CHILDREN'S FUTURES
(AZRAMY, PLANT EXPANSIONS WILL BE HAVUNG IN LARGE LOADS OF
EQUIPMENT & PLANT PAPTS TO AREAS
NORTH OF TITE BRIDGE (EX 5450L) CONOCO PHILIPS) JOE TOUPS jtoups ecityofle



Commandant United States Coast Guard Hale Boggs Federal Building 500 Poydras Street, Room 1313 New Orleans, LA 70130-3310 Staff Symbol: dpb Phone: (504) 671-2128 Fax: (504) 671-2133 D8DPBALL@uscg.mil

16591A 28 October 2013

MEMORANDUM

From: David M. Frank

CGD EIGHT (dpb)

To: Carl M. Highsmith, Program Operations Manager

Federal Highway Administration

Subj: Navigation Study Update Request

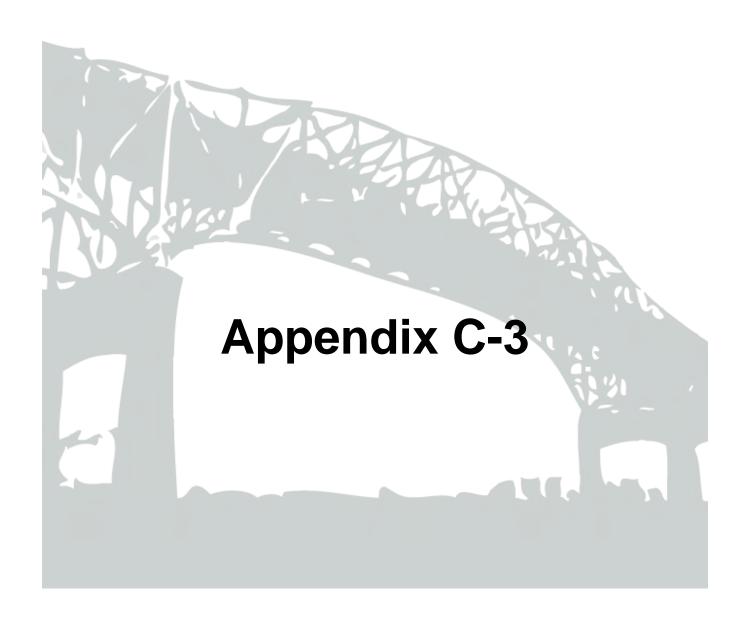
- 1) By letter from Louisiana Department of Transportation and Development (LDOTD), dated September 11, 2013, the Coast Guard was invited to concur on designation as a Participating and Cooperating agency for the purposes of satisfying requirements of the National Environmental Policy Act (NEPA) for the Interstate 10 (I-10) Calcasieu River Bridge project. The Interstate 10 (I-10) Calcasieu River Bridge is a controlled access free-flow roadway project that will cover nearly nine miles and will include rehab/ construction of a new I-10 fixed bridge over the Calcasieu River. This new bridge will require a Coast Guard Bridge Permit. The office of the Commander, Eighth Coast Guard District, Bridge Administration Branch agreed to be a Participating and Cooperating agency by letter dated September 20, 2013.
- 2) As a cooperating agency and per our conversation at the Scoping Meeting on October 23, 2013, the Coast Guard requests that an updated Navigation Study be completed in time to be included in the Draft EIS. This navigation study should update the Marine Use Study as published in May, 2001 as supplemented by the Port Planning Study as published in June, 2006. This update should include a detailed study of all navigation north of the bridge site. All property owners north of the site should be contacted and their use of the waterway evaluated. This should include, but not be limited to, the large vessels that are known to occupy berthing north of the bridge as well as any future plans of any of the property owners for marine usage.
- 3) The Coast Guard also requests that any issues that may require mitigation be mentioned specifically. Any mitigation should be agreed to, at least in principal, and specifically noted in the Final EIS, prior to the signature of the ROD
- 4) If you have any questions or concerns, please contact this office.

#

Copy: USCG, LCDR Xochitl Castaneda, Sector Huston Galveston, Waterways

USCG, Mr. Mike Measells, MSU Port Arthur, Waterways USCG, LT Daniel Gonzales, MSU Lake Charles Prevention USCG, MST1 Helton, MSU Lake Charles Waterways

FHWA, Mr. Bob Mahoney LDOTD, Ms. Noel Ardoin HNTB corp., Ms. April English





I-10/I-210 West End to I-10/I-210 East End

Public Meeting Comment Form

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PLEASE COMMENT ON THE FOLLOWING ITEMS	i:
Project Purpose & Need - What are the key reasons for the	his project? Additional reasons?
Make Sasol /VISTA/Conoco/PPG	clean up the EOC - semans the
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	at you think about the public involvement efforts that will v). Do you have any additional comments/suggestions?
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Quarterly project newsletters will be distributed to inte	erested parties via email (or by mail upon request); and
Build the Gridge North	onto good land.
Do you have any other comments, questions or cond INFORM the public about the	cerns (continued on back): EOC plume, and west vate
to find all other chronic.	leaks/spills - enforce the law; prote
Would you like to receive future updates on the I-10	Calcasieu River Bridge Project? Yes He peop
Please include your contact information for the official project record. Anonymous comments	Name:
cannot be verified.	Address:
Comments may be made online at www.i10lakecharles.com, submitted at the public meetings, or by U.S. Mail to I-10 Calcasieu River Bridge Project, c/o HNTB	Email:
Corporation, 10000 Perkins Rowe, Suite 640, Baton Rouge, LA 70810.	Agency:

Appendix C-3, Pg. 1

Comments, questions or concerns (continued from front):
What happens to the community when the water
is ruined?
Do you think a bridge is the primary concern
or the health of your citizens ?
or the health of your citizenry? What has been done to ensure industry doesn't
Why are my friend's FOIA documente redacted?

April English

From: April English

Sent: Friday, December 20, 2013 11:37 AM

To: April English

Subject: RE: I-10 Lake Charles Feedback

From: Administrator of lakecharles.cloudaccess.net [rdoshi@hntb.com]

Sent: Tuesday, November 05, 2013 02:39

To: inquiry@i10lakecharles.com Subject: I-10 Lake Charles Feedback

I-10 Lake Charles Feedback

I10 bridge comments

Charlie Atherton<charlieatherton@suddenlink.net>

Name: Charlie Atherton

Email: charlieatherton@suddenlink.net

Subject: I10 bridge comments

Message: The Calcasieu River Bridge should remain at it's current height of 135 feet. This current height is not by accident. This height is engineered to allow for the passage of ships that utilize the full carrying capacity of the Calcasieu River north where the water depth is naturally 60 to 80 feet deep. If the Titanic was afloat today the Calcasieu River woruld allow the passage of the Titanic under the existing 135 foot bridge as originally designed. After WWII the navy docked hundreds of ships for miles along the river upstream of the bridge, proving navigation subility. Shipping north of the bridge was originally hampered by the non-alignment of two railroad bridges until recent years when one of the bridges has now been removed allowing large ships to once again navigate upstream. The low level bridge concept was originally thought up and politically driven by ConocoPhillips with the hope that the EDC contamination under the bridge would not be found out. Loc al elected officials fast tracked the decision for a low level bridge over the objection of the public. Since everyone now knows how severe the EDC contamination by ConocoPhillips is and is now being addressed by the agencies, the bridge should remain t it's current height to allow future development of the miles of naturally deep water north of the bridge. Friend Ships discovered this secret long ago and utilizes the river along with others who want to bring in large ships. http://www.ars.usda.gov/is/graphics/photos/formosan/k8085-1.jpg

The LCHTD passed a resolution to keep the bridge at it's current height so they can fully utilize their public property north of the bridge.

Lake Charles Harbor and Terminal District
Board of Commissioners

Resolution 2004-032

A RESOLUTION expressing support to maintain the current height and width characteristics of the I-10 for any new replacement bridge planned for future construction.

WHEREAS, the Louisiana Department of Transportation & Development is currently studying replacing the Calcasieu River I-10 bridge; and WHEREAS, the District believes it is in the best interest of navigational interest and the general public that any new bridge maintain the height and width characteristics of the current bridge.

NOW, THEREFORE, BE IT RESOLVED BY THE BOARD OF COMMISSIONERS OF THE LAKE CHARLES HARBOR AND TERMINAL DISTRICT IN REGULAR SESSION CONVENED THAT:

SECTION 1: The Board of Commissioners of the Lake Charles Harbor & Terminal District does hereby express its support for maintaining, as to any new I-10 Calcasieu River bridge, the height and width characteristics of the current I-10 Calcasieu River bridge.

THUS PASSED AND ADOPTED at Lake Charles, Louisiana, on this 24th day of May, 2004.

FRED R. GODWIN, President

AMERICAN PRESS EDITORIAL
Jan 6, 2008 pE4
Advocates of lower bridge shortsighted

There has been plenty of talk in the last few months about the proposed height for a new Interstate 10 Calcasieu River Bridge.

We've heard from both sides about why they believe the bridge should be either 73 feet or 90 feet tall. Each gave good reasons for their position.

The state Department of Transportation and Development has recommended a 73-foot-tall bridge. The estimated cost for the new bridge will be about \$130 million.

A DOTD report states it would cost about \$15 million less to build than a 90-foot-tall bridge and be much safer for the 50,000 motorists that cross the bridge each day.

However, the nonprofit group Friend Ships and Lake Charles officials opposed that idea, saying a 73-foot bridge would prevent larger vessels from reaching the charity's facility and restrict development along the river north of the bridge.

The Lake Charles City Council voted 5-2 on Nov. 21, 2007 to support a 90-foot bridge. This is what Mayor Randy Roach is supporting.

A few weeks later, the Calcasieu Parish Police Jury voted by a 8-6 vote to endorse a 73-foot-tall bridge.

The Westlake City Council and Sulphur Mayor Ron LeLeux both support the Police Jury's decision.

On Dec. 19 that the Metropolitan Planning Organization voted 4-1 in support of the shorter bridge.

Roach has said the lower bridge will keep large vessels from sailing north of the bridge.

This in turn will affect any possible economic development for the hundreds of acres of undeveloped property lying along the river here, he said.

We wholeheartedly agree with the mayor on this one.

Local officials can't foresee what will happen in the next 50 year. Building a lower bridge will have major repercussions down the line.

If it's built at the lower footage, then the land along the river north of it will be unusable as waterfront industrial property.

The deep-water section of this part of the river has so much potential. Public officials who support the lower bridge, which would effectively cut off potential development north of it, lack vision.

Lower-bridge proponents argue that the land north of the bridge hasn't been developed since the current I-10 bridge was built in 1952. Thank goodness this thinking didn't prevail after the U.S. Air Force abandoned Chennault Air Base in the early 1960s, leaving its 10,000-foot runway dormant for more than 25 years.

We understand that the higher bridge will cost more money, but in the long run it will turn out to be good investment for this area's economy.

The 90-foot bridge is the way to go.

http://epaper.american press.com/Repository/ml.asp?Ref=QW1QLzIwMDgvMDEvMDYjQXIwNDQwMg==&Mode=HTML&Locale=english-skin-custom

It is against the law to restrict navigation or to block navagible waterways.

TITLE 33 > CHAPTER 11 > SUBCHAPTER I > § 494 Prev | Next § 494. Obstruction of navigation; alterations and removals; lights and signals; draws

ID #2

No bridge erected or maintained under the provisions of sections 491 to 498 of this title, shall at any time unreasonably obstruct the free navigation of the waters over which it is constructed, and if any bridge erected in accordance with the provisions of said sections, shall, in the opinion of the Secretary of Transportation at any time unreasonably obstruct such navigation, either on account of insufficient height, width of span, or otherwise, or if there be difficulty in passing the draw opening or the drawspan of such bridge by rafts, steamboats, or other water craft, it shall be the duty of the Secretary of Transportation after giving the parties interested reasonable opportunity to be heard, to notify the persons owning or controlling such bridge to so alter the same as to render navigation through or under it reasonably free, easy, and unobstructed, stating in such notice the changes required to be made, and prescribing in each case a reasonable time in which to make

such changes, and if at the end of the time so specified the changes so required have not been made, the persons owning or controlling such bridge shall be deemed guilty of a violation of said sections; and all such alterations shall be made and all such obstructions shall be removed at the expense of the persons owning or operating said bridge. The persons owning or operating any such bridge shall maintain, at their own expense, such lights and other signals thereon as the Commandant of the Coast Guard shall prescribe. If the bridge shall be constructed with a draw, then the draw shall be opened promptly by the persons owning or operating such bridge upon reasonable signal for the passage of boats and other water craft.

We are requesting that the official paper trail with all of the appropriate legal signatures that changes the bridge height from 135 feet to an illegl 73 foot height entered into the public record of this project.

We do not believe the all the agencies with legislative oversight have all legally followed the required public participation process or have actually signed off on the decision for an illegal low level bridge, especially the Bridge Administration of the Coast Guard. Office of Bridge Administration (CG-5411)

2100 Second Street, SW, Room 3500 Washington, DC 20593-0001 (202) 372-1511 fax (202) 372-1914 "Intermodal Mobility, Safety & Security"

Federal Maritime Law May Be Violated if MDOT Builds New Biloxi-Ocean Springs Bridge Without Drawspan by Keith Burton - GCN Filed 1/7/06

Updated 1/9/06 and 1/30/06 Since shortly after Hurricane Katrina, MDOT has said that it planned to rebuild the damaged Bay St. Louis and Biloxi-Ocean Springs bridges without a drawspan, which the former bridges had. But to do so will violate Federal Maritime Law.

Recently, both the Harrison County Development Commission and the Mississippi Development Authority went on record saying that MDOT's plan to build new bridges with nearly 100 feet of height and no drawspan would not be adequate.

Federal Maritime Law prohibits the building of bridges "...that shall at any time unreasonably obstruct the free navigation of the waters over which it is constructed..." The law further states that any impeding structure, if constructed, "...shall be removed at the expense of the persons owning or operating said bridge."

MDOT's proposed bridges at either end of Harrison County would restrict needed height requirements for shipbuilding in Harrison and Hancock Counties.

With the notice by the HCDC and the MDA, officials with MDOT are now aware that building bridges without drawspans would adversely effect the Coast's navigational requirements, triggering the federal law. As a result, it now appears likely that MDOT will have to reconsider its plans. Not to do so, could result in further delays in replacing the bridges and certainly impact the future of some key Coast industries, which are major employers that must have a clear access waterway. One example recently cited is that of Trinity Yachts in Gulfport. It is one of two shipyards bidding on a 300-foot mega-yacht that would require 110- to 120-feet clearance when it would be taken to open water by barge. It sees a future workforce of 700-750 people building larger yachts.

The fact that MDOT refuses to acknowledge that their bridge proposals do not meet the Coast's true needs now must be questioned by all public and governmental officials. MDOT's continuous lack of awareness can only impede the rebuilding of these bridges as it clear that a growing number of issues now cloud MDOT's plans.

In a time when Louisiana's transportation department has rebuilt the Katrina-damaged I-10 bridge over Lake Pontchartrain, MDOT's lack of performance over the Coast's two bridges is truly damming and already represents a major failure in the state's post-Katrina recovery effort. Even the best current estimates place the opening of MDOT's new bridges nearly two years away. At the current rate and in light of ever-increasing issues, this estimate is optimistic. Coast residents and businesses, including the casino industry need to be alarmed at MDOT's progress and have reason now to question MDOT's public statements on its efforts.

Meanwhile, GCN has learned that the Harrison County Board of Supervisors will likely add their voice to request MDOT to add drawspans in their proposed Biloxi-Ocean Spring and Bay St. Louis bridges. In an interview with GCN on Jan. 9, District 2 Supervisor Larry Benefield said that the county must have drawspans and that the board initially was under the impression from MDOT that drawspans would be included.

"I can't imagine that we build a bridge without drawspans, "Benefield said. "I think you will see us make a decision on the drawspans."

Benefield, who is also the board's vice-president, said it is in the best interests of the county that the new bridges have drawspans to allow for future economic development of the county.

MORE INFORMATION

Federal Law on Bridges

Federal Law on Bridges (viewable with a browser)

Federal Law on Bridges over Waters (opens a .pdf file) Bridge Battles: Drawbridge Would Lengthen Project - Sun Herald

Transportation committee decides I-10 bridge issue

12/19/07

By Theresa Schmidt

The State Department of Transportation now has direction from local government on how high is high enough for the I-10 Calcasieu River Bridge. Today's vote by the Transportation Policy Committee of Imcal allows bridge designers to move forward.

The debate on the height of the bridge has gone on for months all leading up to this day when Imcal's transportation policy committee would meet-- and take a stand. The vote here gets the ball rolling on design of a new bridge and Westlake interchange.

But first the committee heard from attorney Hunter Lundy, representing some who want a 90 foot bridge rather than 73 as recommended by the state. "When we'll spend \$12-14 million more on the bridge at Contraband Bayou to preserve recreational traffic, recreational boats for south Lake Charles, why won't we spend \$12-14 million more to compromise on a 90 foot bridge that cannot block off economic development."

ID #2

Concerned citizen Charlie Atherton says a 135 foot bridge is the way to go. "To build a new I-10 bridge less than 135 feet in height to kill shipping, economic development north of I-10 is against federal law, a disservice to the public, an abuse of power by decision makers, and a bad mistake that'll never be corrected."

As expected, committee members voted four to one in agreement with the state's recommendation of a clearance of 73 feet. Lake Charles Mayor Randy Roach was the lone no vote. He feels 90 feet would have been a reasonable compromise. "This decision is a hundred year decision. It's a decision that's going to affect this community for years to come. Here's a deep water area that's naturally deep water, and we won't be able to access it because the bridge will be too low."

With the committee's stand state highway officials will move forward with planning and designs so that if and when Congress provides money, Calcasieu will be ready to go.

Officials predict areas whose plans are ready to go are more apt to get funding, if it becomes available. http://www.kplctv.com/global/story.asp?s=7521477

The new I10 bridge must remain at 135 feet in height to allow for future development of the miles of natuaally deep 80 foot water north of the bridge that never needs to be dredged.

Charlie Atherton 122 Vine St. Sulphur, La., 70663



November 4, 2013

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe Baton Rouge, LA 70810 Steve Geiger Manager, Lake Charles Manufacturing Complex

Phillips 66 P. O. Box 37 2200 Old Spanish Trail Westlake, LA 70669 Phone 337-491-5222 Fax 337-491-5616

RECEIVED NOV 04 2013

HNTB CORPORATION

RE: I-10 Calcasieu River Bridge

(I-10/I-210 West End to I-10/I-210 East End)

State Project No. H.003931.2

Federal Aid Project No. BR-10-1(212)29

Calcasieu Parish, Louisiana

Dear Sir:

Phillips 66 is pleased to have the opportunity to submit comments to the Louisiana Department of Transportation and Development (DOTD) on the Interstate 10 (I-10) Calcasieu River Bridge design and construction project. We thank you for inviting comments on the project and for allowing us to provide for the public record clarifications and details on environmental impacts related to the bridge. There have been several news accounts recently regarding environmental contamination and the alleged impact of this contamination on the design and construction of the bridge. Our comments today are intended to present and clarify for both DOTD and the public, the documented facts regarding impacts to the soil and groundwater beneath the proposed I-10 Calcasieu River replacement bridge.

In 1994 (almost 20 years ago) a pipeline carrying 1,2 dichloroethane (EDC) spilled EDC into a ditch along Isle of Capri Boulevard, just south of the current I-10 Calcasieu River bridge. Phillips 66, through its predecessor companies, began an extensive emergency response effort at the time of discovery, and it has continued to implement long term cleanup work to remediate soil and groundwater impacted by EDC in the area. Phillips 66 has worked in close conjunction with the Louisiana Department of Environmental Quality (LDEQ) on the planning and execution of these cleanup efforts over the past 19 years. Work Plans and Progress Reports have been submitted to LDEQ on a frequent and regular basis, and these documents are available to the public at the LDEQ website. Through our long term remediation efforts, virtually all of the free phase EDC has been captured and removed from the subsurface.

When DOTD announced plans to replace the I-10 Calcasieu River Bridge, Phillips 66 worked cooperatively with the DOTD to evaluate the potential for EDC to impact future

bridge construction activities. Part of this evaluation included installation of ground water monitoring wells in a marsh area north of the existing I-10 bridge and in the path of the proposed replacement bridge. These wells identified the presence of very low concentrations of EDC in groundwater below the marsh at depths of approximately 55 feet below ground surface. These impacts are at depths below likely earth disturbance or piling installation depths for the new bridge. More importantly, these impacts are well above the depth of the Chicot aquifer, are separated from the aquifer by about 40 feet of clay, and are several hundred feet above the water intakes for the town. These impacts do not represent a threat to the drinking water for the town of Westlake.

Since installation of these monitoring wells, Phillips 66 has collected groundwater samples every three months to monitor for the presence of EDC in the area of the proposed new bridge. Concurrent with this effort, Phillips 66 worked with LDEQ to establish a site-specific cleanup standard for EDC in the marsh area following the LDEQ Risk Evaluation and Corrective Action Program (RECAP) process. A RECAP cleanup standard is a target concentration that when achieved, will be protective of human health and the environment. Through four years of monitoring the wells in the marsh area and in the path of the new bridge, all test results have been below the RECAP cleanup standard. Stated another way, the concentration of EDC in the path of the new bridge is below any concentration that would be a threat to the drinking water aquifer, to any workers on the bridge, or to any persons traveling over the bridge or in the area. No EDC cleanup is necessary in the path of the new I-10 bridge as currently proposed.

To illustrate this point, attached is Figure 1 that shows the areas of soil or groundwater that exceed the RECAP cleanup standard, along with a shaded overlay of the location of the proposed new I-10 Calcasieu River bridge. There are no impacts above the RECAP standard in the marsh area where the new bridge is proposed for construction. There is a small localized area beneath and to the south of the existing bridge where further EDC remediation is necessary (represented by yellow and orange lines). Phillips 66 has already begun a program of more aggressive remediation in these areas, with the intent of completing remediation years before any bridge construction begins.

The attached Figure 2 shows the location of the marsh monitoring points with detailed test results going back several years. These data are provided so that those with an environmental background can see the actual data in a clear and understandable form. Collection of samples for this testing is performed by an independent third party engineer, shipped under chain of custody, and analyzed by an independent third party laboratory. DOTD and LDEQ have also collected samples in this area to provide independent confirmation of our results. To summarize these data, the EDC concentrations are below a concentration that would be an impediment to construction of the I-10 bridge.

The LDEQ has reviewed and evaluated this data. On June 18, 2010, LDEQ wrote a letter to DOTD providing their conclusions. A copy of this letter is attached.

Phillips 66 fully supports the construction of a new I-10 Calcasieu River Bridge. For our part, Phillips 66 will continue to remediate any remaining impacts from the 1994 spill, and in future months, signs of this activity will be clearly visible south of Isle of Capri Boulevard.

We thank DOTD for the opportunity to submit these comments and to clarify for the public record that the soil and groundwater in the path of the new bridge is not a threat to public water supplies, that it is not a threat to bridge workers or the public, and that it is not an impediment to construction of the bridge. We sincerely offer to work cooperatively with DOTD and their consultants preparing the Environmental Impact Statement (EIS) for the bridge, and to answer any questions about the data or site conditions as they relate to bridge construction. It is our opinion that by working together we can attain the vision of constructing a new bridge sooner and at lower cost. Thank you again for allowing us to set the record straight.

Respectfully submitted,

Steve Geiger

Manager, Lake Charles Manufacturing Complex

Phillips 66

Steve Belia

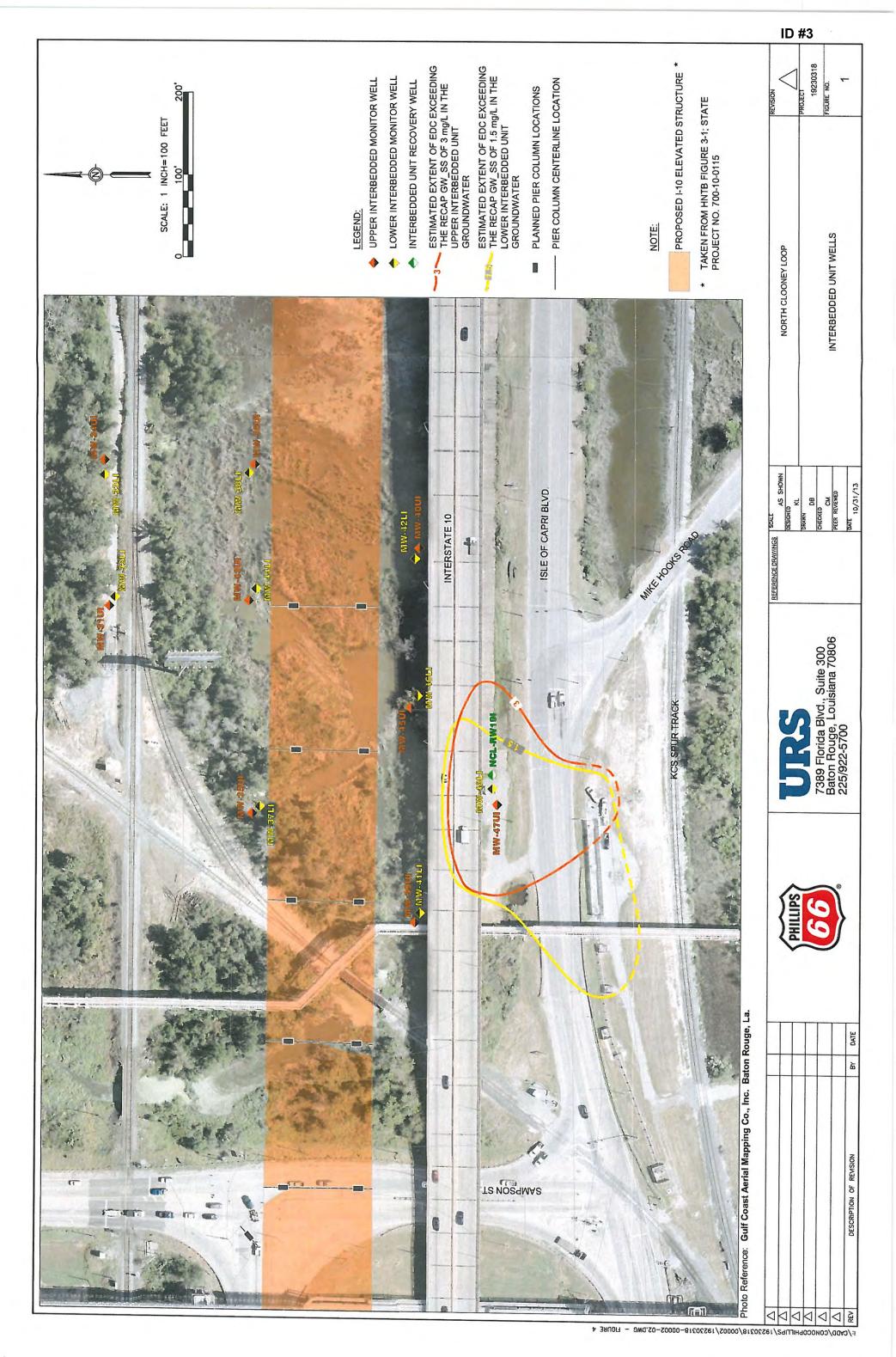
Manager, Eastern Region Remediation Management

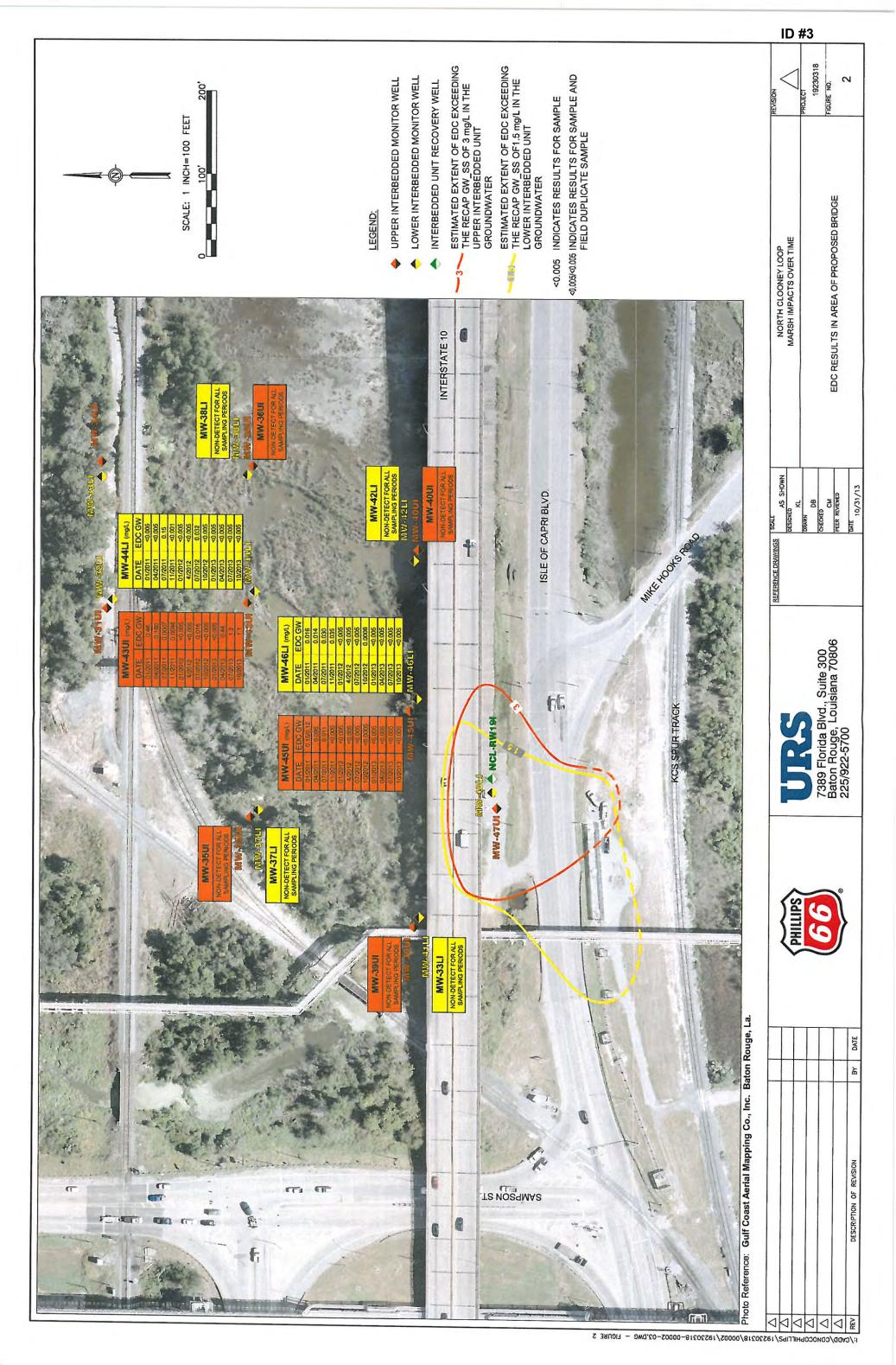
Phillips 66

Cc via email: Glenn Farnet

Christine Carbo Carol Collins Janet Grothe Jim Wallwork William Beck

Keith LeBlanc (URS Corp)





BOBBY JINDAL GOVERNOR



PEGGY M. HATCH SECRETARY

State of Louisiana

DEPARTMENT OF ENVIRONMENTAL QUALITY ENVIRONMENTAL SERVICES

June 18, 2010

CERTIFIED - RETURN RECEIPT REQUESTED (7001 0320 0002 6646 3495)

Ms. Cheryl Duvieilh

Executive Counsel

Louisiana Department of Transportation and Development
P.O. Box 94245

Baton Rouge, Louisiana 70804-9245

RE: ConocoPhillips, Inc. – Proposed Construction for the I-10 Bridge Replacement and Sampson Street Overpass
Clooney Loop Marine Docks Area

Dear Ms. Duvieilh:

This letter revises and clarifies the previous LDEQ correspondences of May 7, 2008 and November 19, 2009, and March 22, 2010.

The location of these planned construction projects is in close proximity to a 1994 discovered release of 1,2-dichloroethane (EDC) at the ConocoPhillips, Inc. (CP) Marine Docks, the remediation of which LDEQ provides regulatory oversight. Representatives of CP, LDEQ, LDOTD, URS, and HNTB (consultant for LDOTD) met regularly throughout 2009 to discuss the analytical results and other findings from CPT's located in the swamp north of I-10. These activities undertaken by CP and included three mobilizations. Analytical data from all 2009 mobilizations was consolidated into a Site Investigation Report, submitted to LDEQ on April 14, 2010. Additionally, ConocoPhillips has proposed an Interim Measures Work Plan (submitted January 15, 2010) and a Monitoring Well Network Installation Plan (submitted April 23, 2010). Both plans have been reviewed by LDEQ and LDOTD, with comments forwarded to CP as necessary.

As previously discussed with the LDOTD, the Federal Highway Administration (FHWA), and ConocoPhillips, there is shared concern about any construction in this area that may exacerbate the current subsurface conditions and concern for the potential for impact to the Chicot Aquifer system (Chicot). Specifically the concern is installation of foundation pilings for the aforementioned highway construction through subsurface zones known to be contaminated primarily with EDC.

Ms. Cheryl Duvieilh June 18, 2010 Page 2

Preliminary piling locations were transmitted to LDEQ from HNTB on October 19, 2009 via e-mail (figure attached for reference). LDEQ understands that this information is still somewhat tentative. Additional pilings may be required at locations undetermined at this time, and piling locations and depths may change. As previously noted, in the absence of final determinations of piling locations and LDOTD-proposed depths, it is impossible to provide any more specificity in recommending depths which would be protective of the subsurface environment, i.e., avoiding exacerbation of current groundwater conditions. As such the Department views these depths of piling generalizations as guidance and subject to change. However, based on the figure provided as well as all available field data, LDEQ is able to provide the following:

- As per the correspondence from LDEQ to LDOTD dated November 19, 2009, LDEQ would have no objection to piling depths of 75' or less north of the current I-10 footprint. While pilings may be placed within contamination zones, this depth would not penetrate zones currently uncontaminated by EDC.
- South of the current I-10 footprint, no piling should exceed a depth of 40' with the
 exception of the following: Using a line drawn from CPT18, CPT7, and a point 50' due
 east of I8 as a reference, there would be no depth restrictions to the east of this line.

It should also be noted that the maximum recommended piling depths given by DEQ would include any and all subsurface-penetrating activities, including those undertaken during geotechnical testing.

As always, we remain committed to working with all parties and agencies in facilitating these projects while protecting the subsurface environment in the Lake Charles/Westlake areas. If you have any questions, please contact Dutch Donlon at (225)-219-3188 or Laura LeBouef at (225)-219-3540.

Sincerely,

Cheryl Sonnier Nolan, Assistant Secretary

Office of Environmental Services

LAD/LQL

c: Imaging Operations - GW

April English

From: Kate Prejean

Sent: Friday, October 25, 2013 11:02 AM

To: April English

Subject: FW: I-10 Lake Charles Feedback

A comment from the website.

Kate Brady Prejean, P.E. Transportation Section Manager Direct (225) 368-2818

----Original Message----

From: Administrator of lakecharles.cloudaccess.net [mailto:rdoshi@hntb.com]

Sent: Friday, October 25, 2013 10:36 AM

To: <u>inquiry@i10lakecharles.com</u> Subject: I-10 Lake Charles Feedback

I-10 Lake Charles Feedback

assist with 106 review

Adley Cormier<aipcormier@gmail.com>

Name: Adley Cormier

Email: ajpcormier@gmail.com Subject: assist with 106 review

Message: As a member of the HPC for City and longtime historian, I am concerned with any changes to the major vehicle corridor through central and north Lake Charles. Because of our unique geography, the route of I-10 displaced

quite a number of historic structures. I would like to help mitigate any adverse change in the future

From: Ben Garber Sr. [mailto:bengarber@bellsouth.net]

Sent: Friday, October 25, 2013 10:12 AM

To: _DOTD-CustomerService

Subject: I-10 bridge over Calcasieu River

Ms.Diedre Druilhet,

Pls. pass my comments to the appropriate party-I am a Civil Engineer, registered professional in Louisiana since 1956, a Fellow in ASCE, and a retired Construction Manager, World wide.

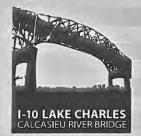
My concern is the Union Pacific RR bridge bottleneck. The RR bridge is 107 yrs. Old and needs to go, for several reasons, obviously. It needs to be part of the new

highway bridge, and incorporated into any new design. No development North on the Calcasieu River can occur without this being done. Please draw attention to this problem before it is too late.

Ben Garber, PE, Fellow ASCE

3636 pickrel lane ,Lake Charles ,La.70615

Tel.337-439-3493



I-10/I-210 West End to I-10/I-210 East End

Public Meeting Comment Form

Your insight and concerns are of key importance to the I-10 Calcasieu River Bridge Project Team. Please take the time to document your comments or questions below. After completion and before you leave today's meeting, please provide this questionnaire to the Project Team. You can also mail your written comments to the address at the bottom of this form. Comments received by **Monday, November 4, 2013** will be included as part of the official record.

PLEASE COMMENT ON THE FOLLOWING ITEMS:	
Project Purpose & Need - What are the key reasons for this project? Additional reasons?	
	nt you think about the public involvement efforts that will y). Do you have any additional comments/suggestions?
3 public meetings (including this one) and one public process. You will have the opportunity to comment at	hearing which will take place at key stages throughout the EIS each of these meetings;
Agency Work Group meetings will be held throughout ficials;	the EIS process to gain the insight of agency, public and elected of-
The project website will be updated regularly with mat	terials pertinent to the EIS process;
You will have the ability to provide feedback/submit qu	uestions on the project website at any time throughout the EIS process,
Quarterly project newsletters will be distributed to inte	erested parties via email (or by mail upon request); and
Project meetings with local and community groups can	n be held as needed.
Do you have any other comments, questions or con-	cerns (continued on back):
Would you like to receive future updates on the I-10	Calcasieu River Bridge Project? Yes X No
Please include your contact information for the	Name: Iqbal Mohammad
official project record. Anonymous comments cannot be verified.	Address: 816 North Lakeshore Drive
Comments may be made online at www.i10lakecharles.com, submitted at the public meetings, or by U.S. Mail to I-10 Calcasieu River Bridge Project, c/o HNTB	Email: soniapetroleum@yahoo.com
Corporation, 10000 Perkins Rowe, Suite 640, Baton Rouge, LA 70810.	Agency:
	(if applicable) Appendix C-3, Pg.



I-10/I-210 West End to I-10/I-210 East End

Public Meeting Comment Form

OCTOBER 24, 2013

Your insight and concerns are of key importance to the I-10 Calcasieu River Bridge Project Team. Please take the time to document your comments or questions below. After completion and before you leave today's meeting, please provide this questionnaire to the Project Team. You can also mail your written comments to the address at the bottom of this form. Comments received by **Monday, November 4, 2013** will be included as part of the official record.

PLEASE COMMENT ON THE FOLLOWING ITEMS: Project Purpose & Need - What are the key reasons for th	
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2. Grade is too steep -	To our circula ya mees or constraint repair
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Quarterly project newsletters will be distributed to inte	rested parties via email (or by mail upon request); and
Project meetings with local and community groups car	n be held as needed.
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	V-
Do you have any other comments, questions or conc	erns (continued on back):
My input on this project	is the design of
the new bridge. For ou	TEV 30 113., THE EXISTING
Would you like to receive future updates on the I-10	Calcasieu River Bridge Project? Yes No
Please include your contact information for the	Name: CORNELIUS MOON
official project record. Anonymous comments cannot be verified.	Address: 1805 CARRY DRIVE
Comments may be made online at www.i10lakecharles.com, submitted at the	Email: _ LAKE CHARLES, LA 70605
public meetings, or by U.S. Mail to I-10 Calcasieu River Bridge Project, c/o HNTB Corporation, 10000 Perkins Rowe, Suite 640, Baton Rouge, LA 70810.	<u></u>
	Agency: CORNIEMOUN 3654@ GMAIL, COM
	Annondiy C 2 Da

Comments, questions or concerns (continued from front):

bridge has been an icon (Signature) for
Lake Charles. The replacement bridge
presents on opportunity for on even more
beautiful bridge. The bridge site is visible
from not only travelers on I-10, but also
from our new larke front.
My recommendation is for a new
CABLE- STAYED Spain. The new Auduton Bridge
in West Felicioni Porish is a beautiful bridge
and is designed for a 1583 ft span. From the
lake, a fan design could give the "swiling ship"
affirmation.
The Houston Ship Channel and the bridge
in Buston are of faw I have visited and exad
wonders to the driving experience.
Planse toxke advantage of this agartunity
for a beautiful new "Signature Bridge"
Thomas for collowing me to provide injut.
Parneli, Moon

Friend Ships Unlimited (a.k.q. Park West Childen

1019 N. First Ave., Lake Charles, LA 70601 USA + Ph: (337) 433-5022 + Fax: (337) 433-3433 + Email: inf 1019 N.

November 1, 2013

NOV -3

To Whom It May Concern:

Don and Sandra Tipton and Teri Shields, the Executive Board of Directors of Friend Ships, a maritime corporation located in North Lake Charles, north of the I-10 Calcasieu River Bridge, would like to state the case for continued access to the natural deep water navigable waters of north Lake Charles that we believe is imperative. On behalf of our corporation and the low-income, primarily African-American community of North Lake Charles, we are certain that continued access to these waters by vessels requiring a vertical clearance as high as 135' is in the best interests of our waterfront land owners, the community as a whole and the greater Lake Charles work force.

Our corporation owns four ships and two large classic wooden boats that transit under the current bridge. One of our ships requires 111' vertical clearance. One of our classic boats requires a similar vertical clearance. Our plans for the future include obtaining three additional vessels and the creation of a marina, job training center and repair yard for large, wooden, classic boats.

Two of our neighbors are currently utilizing their waterfront to berth ships. The Port of Lake Charles and the City of Westlake own deep water frontage north of the bridge. In light of the fact that 1/6th of the economy of Louisiana is related to ports and maritime concerns that create 1/8th of the State's jobs, we believe lowering the bridge is short sighted and driven by an agenda to minimize the cleanup required by chemical companies that contaminated the water, a spill that threatens the health of the community. In addition, the desire for an onramp to the bridge over the railroad tracks in Westlake along with the political ambition of some who seek to please a few powerful interests at the expense of the greater population, has obscured the true facts of this situation.

A new bridge can be built with all the positive qualities of a lower bridge while maintaining the Coast Guard mandated height of 135'. Another option would be to install a drawbridge that opened on a schedule of once a day, when there is vessel traffic and at a time that road traffic is minimal.

It has been asserted that there is not sufficient maritime traffic in our area of up to 70' of deep water and that in the past decade, the need has not grown. It must be realized that DOTD has planned to lower the bridge during this entire period. Commercial maritime entities would not risk an investment in property north of the I-10 Bridge while this issue is pending so it is not a valid evaluation to assume that a significant amount of interest in the deep water aspects of this property will not resume once the bridge is maintained at its current height or a drawbridge is installed. One must look back in history, prior to the construction of an obstruction to navigation that was built - a second railroad bridge near the existing railroad bridge that blocked access to the channel by ships. This bridge no longer exists and at present, navigation is only limited by a 135' Calcasieu River Bridge vertical clearance and a 90+' opening in the railroad swing bridge. Prior to the construction of the second railroad bridge, up to 500 blue-water, deep sea, ocean going ships as large as 18,000 tons, used our area to moor as part of the navy reserve fleet after World War II. Once the second railroad bridge was constructed, ship traffic was halted. Once the second railroad bridge was taken out, the deep water aspects of the channel were forgotten until the Friend Ships once again established them upon our arrival in 2003. If the Titanic were around today, she could sail up to North Lake Charles - her height, width and depth are all within the range of what this channel can accommodate.

Our channel is a protected safe harbor in times of storm and it is utilizes regularly for that purpose. In addition, reducing waterfront property from deep water to no access for use of that resource will greatly diminish the value and future economic development of this commercially zoned property in our low income community.

The deep waters of Louisiana are vitally important to the maritime activity for much of the country. The Coast Guard required the current height of the bridge at 135' because it is their job to protect access to the navigable waters of the US and the previous bridge, a drawbridge, opened an average of over 400 times a month because of frequent traffic. We are located in a strategic position and this should not be sacrificed for 50 to 100 years because of short-sightedness, political agendas and financial concerns for private companies. Maintain access to the natural

deep water river channel north of the I-10 Bridge. While other ship channels on the Calcasieu have to be continually dredged at a cost of multi-millions, this is a channel that never has needed to be and never will need to be dredged. It is naturally deep with no moving silt, an incredible, natural, protected port-of-call with tremendous potential for development.

The American Press agrees with us. The Mayor and City Council of Lake Charles agree with us. The decision makers in Sulphur agree with us. The Port of Lake Charles agrees with us. The recreational boaters of the Lake Charles Power Squadron agree with us.

The case for this issue is not limited to these arguments and we would like to be given a chance to expound on them in an open forum.

Sincerely,

Don Tipton

Sandra Tipton

Teri Shields

ce: Lundy, Lundy, Soileau and South; USCG Office of Bridge Programs



I-10/I-210 West End to I-10/I-210 East End

Public Meeting Comment Form

Your insight and concerns are of key importance to the I-10 Calcasieu River Bridge Project Team. Please take the time to document your comments or questions below. After completion and before you leave today's meeting, please provide this questionnaire to the Project Team. You can also mail your written comments to the address at the bottom of this form. Comments received by **Monday, November 4, 2013** will be included as part of the official record.

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	uestions on the project website at any time throughout the EIS process;
Quarterly project newsletters will be distributed to inte	
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Jee attached letter.	cerns (continued on back):
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lease include your contact information for the	Name: Michael Tritico
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	Email: michaettritica a yahoo, com
	Agency: LEGIORE
	Appendix C-3, Pg

RESTORE

P.O. BOX 233 LONGVILLE, LA 70652

10/25/2013

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Road Suite 640 Baton Rouge, LA 70810

Dear sirs:

I attended the Public Meeting held last night at the Lake Charles Civic Center. The materials presented were very informative. The people who were there to answer questions about the exhibits were quite well-informed and very helpful. Thank you all for giving the people of this area that opportunity to get up-to-date on the bridge and highway plans.

I needed to get back home so I did not fill out the Comment Form while I was at the Civic Center, and I have many more comments than could fit onto the form so I am instead sending you this letter. I will first address the items for which the form specifically requests comments:

"Project Purpose & Need" There is no doubt that traffic congestion, safety, and bridge design issues need to be carefully studied and changes made so that existing problems can be removed and long-term options will be increased.

"Project Coordination Plan" Last night's meeting was, in a way, discouraging, since some of us have attended such meetings for years yet always see the situation stuck at roughly the same place, planning, discussion, changes of plans, more discussions... No practical change ever happens in the real world out there on the highway or bridge. I saw an estimated time of 3 years on one poster, for some sequence of events, and my first reaction was to just add a zero to the 3 since 30 years seems to be more realistic if we continue at the existing pace.

However, the ideas of, "the ability to provide feedback/submit questions on the project website at any time throughout the EIS process," quarterly project newsletters, and the other things that seem to be enhancements of the public's opportunities for participation are welcome improvements. Certainly the stagnation that seems to have become characteristic of the bridge corridor projects could hardly be any worse than it is already, but I would hope that the extra mechanisms for public participation do not somehow contribute to the negative inertia but might instead be managed as accelerators.

I would like to receive future updates on the I-10 Calcasieu River Bridge Project. Although I could receive short notifications at my e-mail address, michaeltritico@yahoo.com, I would like the newsletter or any lengthy correspondence to come to me by real mail.

"Other comments, questions, or concerns" I will now begin with those things.

Looking at the situation holistically it seems obvious that the existing bridge must be dismantled and that there is no way to build a safe and sufficient bridge in the "existing right of way" or anywhere near that location (because the substrate has been severely damaged by EDC contamination. I will discuss that in more detail shortly.)

Since there is no practical way to keep the river crossing where it now exists, the logical thing to do is to re-route Interstate 10 to high and solid ground north of the present location.

In one of the previous meetings I suggested a corridor up at the latitude of Joe Miller Road. There are other latitudes that could also provide locations for crossing the main fork and the West Fork of the Calcasieu River with no need for a bridge with ship height clearance. There is one corridor between Moss Bluff and Gillis now occupied by high voltage power lines. Maybe that one could become a dual use right of way. If not, going farther north to get out of the highly-developed north Moss Bluff zone, a corridor between Gillis and Ragley should have land less expensive to acquire than having to expropriate the new and planned subdivisions in the Moss Bluff area. Another advantage to going up to the South Ragley area would be that the new Interstate would then be out of the area shown by the National Hurricane Center's SLOSH model to be vulnerable to tropical storm surges.

What though, of the concern that a north route would deprive the City of Lake Charles of impulsive tourist stops or other commerce? Look at the French Quarter of New Orleans, or Fisherman's Wharf in San Francisco, or many other famous tourist stops nationwide. Most of those are not beneath Interstate highways. People who want to go to some attraction figure out what exit to take and they do it. Proper publicity would offset any tendency of lazy drivers to bypass anything worth seeing. As it stands right now downtown Lake Charles and the north shore of the Lake do not seem to be strongly magnetic for passing out-of-staters.

A sensible planning effort on the parts of people developing new attractions combined with sensible highway planning could synergistically turbocharge Lake Charles as a family-oriented travel destination. The ongoing uncertainties about traffic could be removed finally. Having to exit the Interstate and come south a few miles to get to something special would not be any problem for anyone truly interested in a good experience. When I was a Ranger-Naturalist in Yosemite Park, all the park visitors had driven over 75 miles from the nearest Interstate Highway. They will come if it is worth visiting.

Another concern I have heard expressed repeatedly through the years, the impediments to navigation presented by low bridges at the latitude of Lake Charles, would also be alleviated by moving the I-10 Corridor north. The ships that do need to move under the existing bridge are few and do not often make the trip, but they are important ships, especially the Friendships that do very critical humanitarian work in times of disaster. Certainly it would not be right, even if it could be accomplished, to put in a low-level bridge unless it were a drawbridge. Opening a drawbridge for 20 minutes once or twice a year, at times of low road traffic, such as at 3 A.M., would be a small price to pay for Americans to be able to help fellow Americans in some disaster zone or people in other countries who might be suffering in some kind of crisis. Traffic tie-ups of far longer duration happen every week on the existing bridge.

(I have to wonder how long it will be before the railroad right-of-way also has to be moved north because of loss of load-bearing capacity beneath the tracks. We have already seen what the chlorinated hydrocarbons can do to track support in Fisherville. It is likely just a matter of time until a train wreck occurs in eastern Westlake adjacent to the I-10 Bridge. Maybe the railroad and highway relocation planning efforts can be combined in order to increase the senses of urgency and stakeholder cooperation.)

Let me go more into detail about my firm conviction that the soil beneath and parallel to the existing I-10 Bridge has been severely damaged and can no longer safely support that bridge or any new bridge that might be contemplated.

For a couple of years I have had public records requests in to the State and Federal Highway agencies. I asked for the inspection reports for the bridge, the chemical analyses for soils there, and documents that might have included discussions about the possible impacts of the EDC leak upon conditions at the bridge. My concern has been that the bridge has become increasingly-unstable not just because of its age but also because the ground it is anchored within has been changing, has been softened by the EDC to the point where a catastrophic failure could occur.

The only way I can describe the agencies' earlier responses to my Sunshine Law and Freedom of Information Requests is "stonewalling." I persisted, however, and just this week, in response to my appeal at the Federal level, I received a new CD with 54 pages of material, much of which was redacted, as had been an earlier, ~3,000 page CD. However, I did see, (despite many unredacted pages which were poorly-copied, washed-out looking), a few pages with quite pertinent information. That information further convinced me that my concerns are valid, concerns about the threat to the public from a possibly sudden shift and partial collapse of the bridge.

I know that you have not only the material I have seen but undoubtedly material much more recent than the most recent (year 2009) material in the CD I just received. What I saw in the 2009 material made complete sense to me when I thought back to what I have seen from my reviews of materials during situations at hazardous waste disposal sites as well as the chlorocarbon train wrecks at Livingston, Eunice, and Fisherville.

For example, there have been two studies commissioned by industrial companies, to determine what effect EDC has on local clays. Both studies showed that our regional clay is quickly and severely degraded by EDC. It loses its ability to bear weight and it loses its ability to retard movement of fluids. One study was done by Capozzoli and Associates for BFI at Willow Springs and the other was done by Kirk Brown and Associates for CWMI at Carlyss. The Louisiana Department of Environmental Quality has copies of each of those studies in the files of the two companies.

From the train wrecks we also saw that chlorinated hydrocarbons move quickly through soils and degrade the soils as they move through. In the case of Fisherville, the situation became something ironically spoken about as "the annual Fisherville train wreck" when rail cars would again occasionally derail at the same location because the track foundation experienced ongoing degradation despite the constant collection of material through recovery wells. Once the chlorinated hydrocarbons are released they cannot be fully-retrieved and they never stop turning the clay into mush.

Where within the I-10 project area is the EDC plume? How significant is it? What is its eventual fate? How should answers to those things affect any decisions made about the project?

The material I just received shows the location in more detail than it appeared on two of the posters you had on display last night at the Civic Center. The plume crosses beneath the bridge a couple of hundred yards or so east of the Sampson Street longitude and it extends in a northeasterly direction almost to the railroad tracks. Since almost five years have elapsed

since the data I have seen was collected I would imagine that the plume may have reached the tracks already. That means that the effect on the substrate underlies any possible new location to either the north or south of the existing bridge.

The concentration of the contamination is astounding, some 90,000 times the RECAP (Risk Evaluation Corrective Action Program) trigger level of 5 ppb. That great exceedance of a threshold meant to be a conservative guideline for protection of groundwater surely attests to the probablity that the crystalline structure of the medium through which the EDC is migrating has been compromised and compromised significantly. I did not see in the new CD any tests of boring material for load-bearing capacity but such tests likely exist. I think that it would be wise for the highway departments to make public the results of the tests so that the realities of the situation can become understood by everyone. Unless there is complete transparency there will not likely be public support for decisions that *must* be made.

The fact that contamination has been found at various depths sampled, even to within 40 feet of the top of the Chicot Aquifer, that is, as of 2009, down 2/3 of the way from ground surface to the Sole Source aquifer, means that it is inevitable that the EDC will enter that aquifer. What is quite disturbing is that the plume is going in a direction contrary to the usual direction of groundwater flow in this region. Usually the flow is downdip, toward the Gulf, with localized variations because of the usual interconnections with surface streams and their historical scour trenches and old buried courses. Although the stratigraphy pages I got in the new CD were illegible, what seems likely from that unusual direction of flow is that the nearby Westlake Municipal Water Supply well with its heavy draft is pulling the plume downward and northeastward.

That is bad news for the people of Westlake, even though they do have another big well north of the town, but to lose the one that has served them all these years would be a shame. That may be unavoidable. At best the loss could be delayed for awhile, maybe, if an extensive grid of recovery wells were to be installed between the most concentrated part of the plume and the municipal well. That grid might well have to be located right in the existing I-10 corridor and railroad right of ways.

The material that was legible and unredacted in the new CD from the Federal Highway Administration does show that the Louisiana Department of Environmental Quality is quite concerned about the possibility that any new bridge pilings driven might hasten the arrival of the EDC into the Chicot Aquifer by dragging the EDC even more rapidly downward.

From my experiences looking at other groundwater contamination problems in Southwest Louisiana I have come to believe that recovery wells are only sparingly-efficient and cannot really "remediate" a problem once the contaminants pervade the subsurface to any significant extent. Recovery wells (and any kind of thermal or bioremediation measures) do buy some time for people to adjust and that is about all they do. If Westlake needs time to figure out another alternative public water supply, (perhaps a pipeline from Toledo Bend Lake or another large well somewhere in northern Calcasieu Parish far away from existing contamination), then Westlake may need the grid of recovery wells between the I-10 Bridge and the railroad bridge, adding another reason for not wasting time thinking about putting a new Interstate bridge in the existing corridor.

The fact that there are 16 other public water supply wells within two miles of the study area makes even more imperative the consideration of utilization for time-buying recovery wells within the area within the bridges' rights of way. In addition to those public water supply

wells there are 41 domestic wells, 43 industrial water supply wells, and an irrigation well. Too many people depend upon the groundwater now being threatened by the EDC leak to sacrifice the option of a recovery well field location exactly where it would do the most good.

I have sent a request to the Federal Highway Administration asking that I be given more recent and more extensive information. I hope that I will get the latest boring and analytical data and maybe some idea of how the discussions are going (discussions that surely must be addressing the kinds of things I have talked about in these comments.) I see no reason that any information should be kept from the people. So what if the companies involved in the release of the EDC face liabilities? That is their problem; it is not the public's fault and should be no excuse for keeping facts from the people who drive over the bridge and drink the water. No company should have more rights than any human being, especially human beings whose safety and health might be in any remote way threatened by what some company has done. That whole litigation delay/need for secrecy situation should just be dissolved immediately. Maybe the State Attorney General can get together with the U.S. Attorney General and show some backbone on behalf of the people.

I would again like to thank you all for the nice presentation, but I hope that at the next stage in this process there will be introduction of the concept of relocation northward of the I-10 crossing of the Calcasieu River to high and solid ground with fully transparent disclosure of the reasons for that proposal.

Sincerely.

Michael Tritico, Biologist and President of RESTORE

Restore Explicit Symmetry To Our Ravaged Earth

April English

From:

Noel Ardoin < Noel.Ardoin@LA.GOV>

Sent:

Friday, October 25, 2013 3:03 PM

To:

April English

Cc:

Joubert Harris; Patrick B. McIntire FW: FOIA records re EDC plume - part 1

Subject: Attachments:

FOIA MT p1.pdf; FOIA MT p2.pdf; FOIA MT p3.pdf; FOIA MT p4.pdf; FOIA MT p5.pdf; FOIA MT p6.pdf; FOIA MT p7.pdf; FOIA MT p8.pdf; FOIA MT p9.pdf; FOIA MT p10.pdf; FOIA MT p11.pdf; FOIA MT p12.pdf; FOIA MT p13.pdf; FOIA MT p14.pdf; FOIA MT p15.pdf; FOIA MT p15.pdf; FOIA MT p16.pdf; FOIA MT p17.pdf; FOIA MT p18.pdf; FOIA MT p19.pdf; FOIA MT p20.pdf; FOIA MT p21.pdf; FOIA MT p22.pdf; FOIA MT p23.pdf; FOIA MT p24.pdf;

FOIA MT p25.pdf; FOIA MT p26.pdf

From: Laura Cox [mailto:lauraycox@gmail.com]
Sent: Friday, October 25, 2013 2:59 PM

To: Noel Ardoin

Subject: Fwd: FOIA records re EDC plume - part 1

From Mike Tritico.

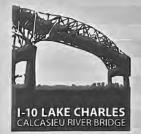
The plume testing from '09 shows 90,000 times the RECAP threshold requiring remediation.

It was 40 ft from the top of the Chicot aquifer sands in '09 and Mike thinks it is being drawn toward Westlake's municipal well.

Mike FOIA'ed records re testing / measurement of the plume from feds and state 2.5 years ago; just months ago he received 3,000 pages of mostly redacted info. he appealed. just this week he received 54 pages, with lots redacted, but enough info to identify the consulting firm that has performed the measurement/testing and to state the above.

We should have unredacted, clear copies.

Bert



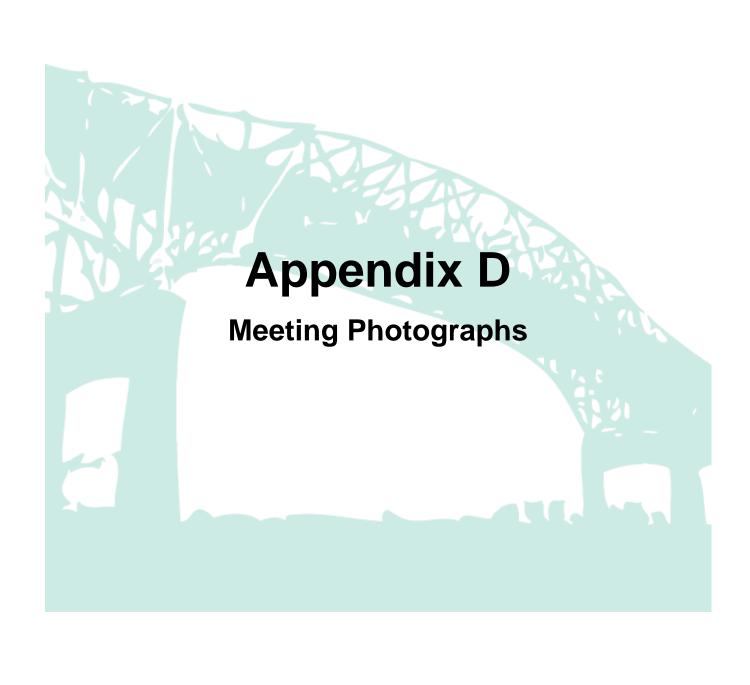
I-10 Calcasieu River Bridge Project

I-10/I-210 West End to I-10/I-210 East End

Public Meeting Comment Form

Your insight and concerns are of key importance to the I-10 Calcasieu River Bridge Project Team. Please take the time to document your comments or questions below. After completion and before you leave today's meeting, please provide this questionnaire to the Project Team. You can also mail your written comments to the address at the bottom of this form. Comments received by Monday, November 4,

13 will be included as part of the official record.	
PLEASE COMMENT ON THE FOLLOWING ITEMS:	
Project Purpose & Need - What are the key reasons for th	is project? Additional reasons?
150 03 Bridge and Capac	14 of the I-10-210-210-5Treach
Hinders mouth a- L	ahe Charles
Hillaste year of years	
	t you think about the public involvement efforts that will). Do you have any additional comments/suggestions?
3 public meetings (including this one) and one public h process. You will have the opportunity to comment at	nearing which will take place at key stages throughout the EIS each of these meetings;
Agency Work Group meetings will be held throughout ficials;	the EIS process to gain the insight of agency, public and elected of-
The project website will be updated regularly with mat	erials pertinent to the EIS process;
You will have the ability to provide feedback/submit qu	estions on the project website at any time throughout the EIS process;
Quarterly project newsletters will be distributed to inte	rested parties via email (or by mail upon request); and
Project meetings with local and community groups can	be held as needed.
~ 1	1 - 10. 1 211 .
Ihrs 15 guar 1 m	ahe sure This 19 fullic12-el
Throughtest the Melia on	on sping basis, Public serve
	Announts + Bill Boom
Do you have any other comments, questions or conc	terns (continued on back): and print
Would you like to receive future updates on the I-10	Calcasieu River Bridge Project? Yes No
Please include your contact information for the	Name: Leva Allanget
official project record. Anonymous comments	feld V. Vincta
cannot be verified.	Address: 70/ MLH HWY, LC, La 70GO
Comments may be made online at www.i10lakecharles.com, submitted at the	Email: pdv aLRC Wireless Com
public meetings, or by U.S. Mail to I-10 Calcasieu River Bridge Project, c/o HNTB Corporation, 10000 Perkins Rowe, Suite 640, Baton Rouge, LA 70810.	Agency: La Radio Comm, Inc.
	Appendix C-3, Pg. 30
10 Caleacion Piner Pridge Project (L10/L210 Wort End to L10/L210 Fact End)	www i10lakecharles.com



Public Scoping Meeting Photographs















Agency / Public Meeting #2 Summary Report August 2017

I-10 CALCASIEU RIVER BRIDGE I-10/I-210 West End - I-10/I-210 East End



AGENCY & PUBLIC MEETING COMMENTS & RESPONSES



Agency Meeting held August 3, 2017 from 2:00 PM to 3:00 PM

Public Meeting held August 3, 2017 from 5:00 PM to 8:00 PM

State Project Number: H.003931 Calcasieu Parish, Louisiana







1.0 INTRODUCTION

The second agency meeting and second public meeting were held on Thursday August 3, 2017 at the Lake Charles Civic Center, Contraband Room at 900 Lakeshore Drive Lake Charles, LA 70601.

The agency meeting was held from 2:00 PM to 3:00 PM, followed by the public meeting from 5:00 PM to 8:00 PM.

The comment period opened on August 3, 2017 and ended August 14, 2017. Attendees at the agency and public meetings could provide comments through a variety of methods, including the following:

- Submitting a written or verbal comment at the Agency/Public Meeting;
- Mailing a written comment to I-10 Calcasieu River Bridge Project c/o HNTB Corporation, 2021 Lakeshore Drive, Suite 230, New Orleans, LA 70122; or
- Logging on to the project website (www.i10lakecharles.com) and selecting Contact Us.

Section 2.0 presents comments received from agencies and responses to those comments. Similarly, **Section 3.0** presents comments received from the public and responses to those comments.

2.0 AGENCY MEETING COMMENTS

Commenters are identified in **Table 1**, summarized below the table, and responses are provided accordingly. Copies of the comments are included in **Attachment A.**

Table 1. Agency Comments Based on Material Presented at Agency Meeting

ID# *	Name	Organization	Title
1	Beck, Robert	FAA	Manager, Operations Support Group
2	Sanders, Kristin	Louisiana Office of Cultural Development, Department of Culture	Deputy State Historic Preservation Officer
3	Luckett-Snyder, Casey	EPA, Superfund Division	Remedial Project Manager

Note: * Copies of the comments are found in Attachment E and are referenced by ID #. Acronvms:

FAA = Federal Aviation Administration

EPA = United States Environmental Protection Agency

ID #1: Beck, Robert with FAA

- **Comment 1:** Encourage coordination with FAA's Obstruction Evaluation/Airport Airspace Analysis (OE/AAA) office so that they can review the alternatives to provide possible impacts. If any part of the project exceeds notification criteria under FAR Part 77, notice to the FAA is required at least 30 days prior to the proposed construction date.
- **Response 1:** Comment noted. FAA's OE/AAA office will be added to the Agency Work Group for future coordination.

ID #2: Sanders, Kristen with Louisiana Office of Cultural Development

- **Comment 1:** The interchanges proposed in the Preliminary Build Alternatives 2 4 have the potential to adversely affect historic standing structures.
- Response 1: Comment noted. An assessment of potential impacts to historic standing structures will be completed for the Reasonable Alternatives. If adverse impacts are identified, the Project Team will work to avoid and/or minimize such impacts to the extent practicable.
- Comment 2: In order to comment per Section 106 regulations (36 CFR 800), need Areas
 of Potential Effects (APE) for the Preliminary Build Alternatives and an assessment on the
 National Register of Historic Places (NRHP) eligibility made on all the standing structures
 within the APE.
- Response 2: Comment noted. The APEs will be established for the Reasonable Alternatives. The Project Team will coordinate with SHPO on the APEs. Once approved by SHPO, the NRHP eligibility of all standing structures within the APEs will be evaluated as part of the EIS.

ID #3: Luckett-Snyder, Casey with EPA

- **Comment 1:** PBAs that involve the extension of Sulphur Ave. to Enterprise Blvd. have the potential to impact the Gulf State Utilities North Ryan Street Superfund Site (Site). Although remediation has occurred, low level threat contamination remains in the subsoil of former exposed tar area (see graphic in comment). Request coordination prior to design and construction should a Sulphur Ave. extension to Enterprise Blvd. be selected as a Reasonable Alternative.
- Response 1: Comment noted. The Project Team will coordinate with EPA as the EIS progresses and the Reasonable Alternatives are further refined.
- **Comment 2:** EPA must review and approve a Soil and Stormwater Management Plan that includes (1) sampling and analysis plan for contaminants; (2) plan to deal with and disposal of contaminated soil; (3) plan that ensures legal disposal of contaminated soil; and (4) plan to minimize stormwater contact with contaminated soil.
- Response 2: Comment noted. The Project Team will coordinate with EPA and complete
 the necessary plans and analyses should they be warranted and/or document as a
 commitment that will be followed up in future phases of the project, if needed.
- **Comment 3:** Any future property owners of all or a portion of the property must comply with land use restrictions to control and limit exposure to Site contamination.
- Response 3: Comment noted.
- **Comment 4:** Project should follow requirements of 29 CFR 1910.120 concerning HAZWOPER training requirements for construction workers who may be working with contaminated subsurface soils.
- Response 4: Comment noted.

3.0 PUBLIC MEETING COMMENTS

Table 2 provides a listing of all comments received. For reporting purposes, comments are summarized into major points. Also included in **Table 2** are the corresponding response codes for each comment, which can be directly accessed by clicking on the hyperlink in the response code column. The response code key is presented in **Table 3**. Copies of all comments received are included in **Attachment B**.

Table 2. Public Comments Received and Response Codes

Name (Last/First)	Date	Comment(s)	
Abdalla, A	8/13/17	Make existing I-210 Loop the designated I-10 route and make the present I-10 route I-210	
Alejandro, L	8/4/17	Suggest an underground tunnel. Would be easy and cheaper to build.	<u>D-1</u>
Ashworth Emily	8/3/17	1. Do not support PBA 1.	<u>C-1</u>
Ashworth, Emily	0/3/17	2. Support PBA 3, Support Sub-Alt B.	<u>B-3</u> , <u>B-6</u>
		Support bridge remaining at current height of 135 feet to maximize the future navigational use and development of the naturally deep and protected waters.	<u>A-1</u>
Atherton, Charlie	8/14/17	Requesting the official paper trail with all the appropriate legal signatures that changes the bridge height from 135 feet to 73 feet be entered into the public record of this project.	<u>E-2</u>
		3. Do not believe all the agencies with legal authority and legislative oversight have legally followed the required public participation process or signed off on the decision for the 73-foot bridge, especially absent is the Bridge Administration of the Coast Guard, Office of Bridge Administration.	<u>E-2</u>
		1. Support Sub-Alt. E	<u>B-9</u>
Bates, Betty	8/3/17	2. Do not support Enterprise Sub-Alts	<u>C-6, C-8</u>
,		Please construct infrastructure from I-10 to Westlake first – takes 30-40 min to get out of Westlake Support replacement of Calcasieu River Bridge	<u>A-1, G-1</u> <u>B-11</u>
Bates, Don	8/3/17	Stay away from contaminated area at all cost.	
Bonvillian, Betty	8/12/17	 Do not disturb the aquifer. Request two bridges with cross overs for emergency personnel. Heavy trucks should have their own reinforced lane, paid for by the industries with heavy trucks that 	E-6 D-1
,		tear up the road and release pollutants.	<u>A-1</u>

Name (Last/First)	Date	Comment(s)	
		Need for new I-10 bridge is imminent	<u>B-11</u>
		2. Grade of future bridge should be studied. Grade of the current bridge has caused many accidents.	<u>D-2</u>
		3. Do not dig around the EDC spill area.	<u>C-11</u>
Borel, Mattie	8/3/17	4. Concerned about potential impacts to the aquifer.	<u>E-6</u>
		5. The height of the current bridge was determined to allow large ships to go under the bridge and hide from the enemy in times of past warfare. This should be considered for times of future warfare before lowering the height of the bridge.	<u>A-1, E-2</u>
		1. Support PBAs 2 and 3, Sub-Alt. C	<u>B-2</u> , <u>B-3</u> , <u>B-7</u>
Capdebosco, Pam	8/3/17	2. Want direct access to both east and west and downtown Lake Charles at Ryan St. because of the location of community events and restaurants.	<u>D-2</u>
		3. Take flooding issues into consideration as well as the contamination of water and air.	<u>E-1, D-2, H-2</u>
Carloton Mika	8/3/17	1. Do not support PBA-4	<u>C-4</u>
Carleton, Mike	8/3/17	2. Get the EDC contamination site cleaned up	<u>H-1</u>
		1. Please reaffirm the general right-of-way is not expected to impact the historic sites of Corporation Cemetery (at Moss and Church) and Cantonment Atkinson/Bilbo Cemetery.	<u>E-1</u>
	8/5/17	2. Support widespan option (assume means long span bridge option).	<u>B-3</u>
Cormier, Adley		3. Support moveable bridge to direct traffic to Enterprise Blvd.	<u>B-13</u>
Connier, Adiey		4. Support additional connections to Lakeshore Dr. and Ryan St (assume means west of Ryan St.)	B-5, B-7, B-9
		5. Moveable bridge at the site of the Old Spanish Trail bridge along with an alignment of Sampson St. (which would run with no connection to I-10 at this site) to Mike Hooks to Marine St. to Nelson would be useful to move traffic from West to East Calcasieu.	<u>A-1, D-1</u>
		1. Support PBA 2 and Support Sub-Alt. B.	<u>B-2</u> , <u>B-6</u>
Council, Walter	8/3/17	2. DOTD requires Complete Streets consideration for all new projects. Sub-Alternative B offers opportunity to implement pedestrian, bike and regional transit opportunities.	<u>E-4</u>
Crawford, Craig *	8/3/17	Please do not cut straight across Lake Charles. It would ruin the beauty of the lake.	<u>C-4</u>
<u> </u>		Do not cut across Lake Charles, keep bridge in existing location.	C-4
Crawford, Craig *	8/3/17	2. Would like an architecturally pleasant bridge that incorporates the crossed gun logo.	<u>A-1</u>
		3. Reuse or sell the guardrail from the current bridge.	<u>A-1</u>
Diamond, R.	8/3/17	1 Add at least 1 or 2 tall langs to bridge to holp with financing	
Patrick	8/3/17	2. Use suspension bridge option to span the EDC area and railroad tracks on I-10 and Sampson St.	<u>B-3</u>

Name (Last/First)	Date	Comment(s)	Response Code(s)
Fritzenschaft, Peter	8/5/17	 Suggest the following design instead of those presented at the public meeting: Build a north loop of I-210 from west I-10 interchange around the backside (west) of Sasol and Nelson Power station up to Hwy. 171 north of Moss Bluff. Continue the North I-210 Loop from Hwy. 171 to the east I-10 interchange - do not stop at Hwy. 171 Would require bridges over West Fork and Calcasieu River, but smaller scale than a new I-10 bridge. With this north loop and the I-210 loop open to traffic, conduct demolition of the I-10 bridge, leaving the concrete piers in place for future use. Build a lower profile I-10 bridge on the existing concrete piers. Benefits of the above design include (1) there would be 3 avenues of interstate roadway for east-west traffic (2) heavy industry traffic would be re-routed around the outside of Westlake greatly lowering the amount of traffic going through town and minimizing the problems the train causes when it goes through Westlake to a more acceptable level, (3) would provide a means to remove the current I-10 bridge from use during refurbishment without greatly impeding traffic flow, (4) would provide a more efficient means of egress for south Lake Charles in the case of hurricane evacuation, and (5) would provide the area with an adequate infrastructure that will allow growth well into the future. 	
Gibson, Angela	8/3/17	Anything that can be done to alleviate the extra traffic that will go to I-10 during the I-210 bridge project should be done before the I-210 bridge project starts.	<u>A-1</u>
		 The EIS process that DOTD is now continuing after a four-year hiatus does not meet federal regulatory standards. DOTD has skipped critically important steps that it said it would take in the October 2013 Public Scoping meeting. 	<u>F-2</u> <u>F-3</u>
Harbison,		Both the feasibility study and the scoping process that form the foundation of the proposed EIS are outdated and inconsistent with the process described to the public four years ago.	<u>F-4</u>
Richard with Phillips 66 Company	8/14/17	DOTD has apparently relied on outdated and incorrect data to eliminate from consideration the most cost-effective alternative, PBA 1-F.	F-5, B-1, B-10
Company		 PBA 1-F was eliminated from consideration in a secret process in which the public and stakeholders were not given the opportunity to provide meaningful input. 	<u>F-6</u>
		 Many of the specific safety and congestion issues that were identified as a problem at Sampson St. in the draft 2013 purpose and need statement were removed from the purpose and need presented at the August 3, 2017 meeting. 	<u>F-7</u>
		Please explain how far the bridge will be from my home on Church Street. Is the bridge going in a circle from right to left still passing next my property?	<u>l-1</u>
Hersey, Elizabeth	8/11/17	2. Will I be offered to sell and move or stay?	<u>E-5</u>
		 It appears it is coming to I-10 east toward Railroad Avenue and Hersey street. Explanation needed. Request more police in the area to deal with panhandlers. 	<u>l-2</u> A-1

Name (Last/First)	Date	Comment(s)	
		Streamline and compress the environmental process.	<u>F-1</u>
		2. Bridge is obsolete and needs to be replaced now.	<u>B-11</u>
Knapp, Leonard	8/10/17	Need discussion of the impact of the contamination now under the bridge and its impact on cost and problems which might result.	<u>H-1</u>
		4. Look at an alternative location north of the present site that might avoid the issue of contamination, going through Moss Bluff.	<u>D-1</u>
Lake Charles	8/9/17	Support renovation/replacement of the I-10 bridge.	<u>B-11</u>
Yacht Club	0/9/17	2. Do not support new bridge across the middle of Lake Charles	<u>C-4</u>
		1. Support Sub-Alts. A and B.	<u>B-5, B-6</u>
Leger, Randy	8/3/17	2. Must keep I-10 bridge open while new bridge is built. Own a business on the east side and we need to be able to deliver our product on the west side of the bridge in a timely manner.	<u>D-4</u>
Magallon, Benjamin	8/3/17	1. PBAs 2 and 3 have the most desired balance of mitigation impacts. While PBA 2 is less costly than PBA 3, the potential for added benefits to travel and tourism with a long-span bridge could be one way to show long term off-sets to the difference in cost.	<u>B-2</u> , <u>B-3</u>
-		2. For the north/south connectivity of surface level roads, address bike/pedestrian connectivity.	<u>E-4</u>
Mansell, E.	8/3/17	Looking forward to job being done.	<u>A-1</u>
Marcon, John	8/3/17	1. Recommend solidifying the EDC site by grout, etc. If made a solid site it eliminates the EDC from moving. In 1991 Olin build a large retaining wall to build a new wastewater treatment plant using a large auger to build the wall. The wall is still standing. I think a similar system could be used to solidify the contaminated site.	<u>A-1</u> , <u>H-1</u>
		2. The option of the bridge through the Olin site (PBA 4) goes through difficult/expensive terrain.	<u>E-3</u>
McDonald, Marc	8/4/17	Will the graphics/information presented at the public meeting be posted on the website or elsewhere?	
Poppell, Brittney	7/27/17	Will the project require right-of-way acquisition?	<u>E-5</u>
Powell, A.	8/3/17	Support PBA 3.	<u>B-3</u>
Reilly, Patrick	8/4/17	Is it possible to receive and review the slide presentation shown at the August 3 rd meeting?	<u>A-2</u>
		The bridge is part of an interstate system that is regulated in part by the Federal government and should be built with Federal funds or with grants for most of the project.	<u>G-1</u>
		2. The new bridge should be built along the I-10 corridor to keep the main flow of traffic on the interstate system. It also serves as an alternate route when south Lake Charles and I-210 are too congested.	<u>B-12</u>
Robinson, Jeff	8/5/17	Make the new bridge no less than three lanes in each direction rather than two lanes due to ever growing traffic congestion.	<u>D-2</u>
		Lake Charles could have built a new bridge a few years ago when the cost would have been cheaper. The cost will continue to rise – get it done now.	<u>A-1</u>
		5. Do not over plan and spend a fortune on planners and over-priced project plans and studies. Use one of the many studies that have already been done.	<u>F-1</u>

Name (Last/First)	Date	Comment(s)		
		ACV is in favor of the bridge replacement project.	<u>B-11</u>	
		2. Need sufficient and adequate alternate routes with traffic controls during bridge down time, especially during hurricane season to ensure safety of all commuters.	<u>D-4</u>	
Roy, Lanny with A Community	8/2/17	Due to the EDC contamination, safety precautions must be put in place with safe work practices employed and adequately trained workers. Constant monitoring practices must be established.	<u>H-1</u>	
Voice (ACV)	0/2/11	4. Support implementation of Lead Safe Work Practices for the removal and disposal of the existing bridge, as most bridges build around the time of the Calcasieu River Bridge contain lead based paint.	<u>E-3</u>	
		5. Support using Disadvantaged Business Enterprise contractors and local residents for jobs, as the majority of large scale construction projects across the state employee out of state contractors that fail to return investment into the communities in which they work.	<u>D-6</u>	
		Concerned about the potential impacts to drinking water.	<u>E-6</u>	
Searcy, Carly	8/3/17	2. The option that avoids the major contaminants is probably very costly.	<u>A-1, G-2</u>	
		3. I want to know more about the different truss systems to keep from drilling below the aquifer.	<u>l-3</u>	
Spain, Mike	8/1/17	What is the estimate on construction cost for the project?	<u>G-2</u>	
	8/3/17	Without knowing the cost my recommendation is either PBA 2 or PBA 3.	<u>B-2, B-3, G-2</u>	
Stewart, Charles		2. Support either Sub-Alternatives A, C or E.	<u>B-5, B-7, B-9</u>	
		3. Do not support options that extend to Enterprise Blvd. (B or D).	<u>C-6, C-8</u>	
Still, Mary	8/10/17	Supports a new bridge soon – needed for safety.	<u>B-11</u>	
Still, Maly		2. Prefabricate as much of the bridge as possible to make construction go faster.	<u>A-1</u>	
Tipton, D. with Friend Ships	8/12/17	 2. Prefabricate as much of the bridge as possible to make construction go faster. Commenter expressed the following concerns: Lowering the bridge will permanently destroy the potential maritime economic development and reduce property values. Lowering the bridge could limit vessels from utilizing any future potential marinas included in the North Lake Charles Riverfront Parkway and Redevelopment Plan. The American Press published an editorial in 2008 that details reasons that the advocates of a lower bridge are shortsighted. 		

Name (Last/First)	Date	Comment(s)			
Tipton, D. with Friend Ships (continued)		 Per USCG regulations and settled law, the USCG cannot allow a structure to be built over navigable waters that does not provide for the reasonable needs of current and future navigation. Friend Ships' area is a natural safe harbor from storms. Park West Children's Fund/Friend Ships is authorized by the USCG as a TWIC security dock for the moorage of US and foreign vessels of different sizes and heights. The long-term future of Friend Ships and land owners who will be affected by the lowering of the bridge cannot adequately be predicted. 			
		Support a bypass north of Lake Charles at a latitude that would not require a massive bridge.	<u>D-1</u>		
		2. Error on PBA 4: The two new bridge crossings are not over Bayou Contraband, but are on the Clooney Island Loop of the Calcasieu River.	<u>A-4</u>		
		3. Do not support PBA 4 – it would be an eyesore and hazard to boaters.	<u>C-4</u>		
		4. EDC contamination must be studied in the EIS and presented to the public.	<u>H-1</u>		
		5. RESTORE was told that the rules for the Interstate specifically prohibit drawbridges or turnstile bridges (and tunnels). What has changed?	<u>D-5</u>		
		6. Westlake municipal water wells just north of the railroad seem to be drawing EDC upgradient toward themselves. The EDC would destroy the soil beneath the proposed Sub-Alts. A-E and PBAs 2 and 3.	<u>H-2</u>		
Tritico, Michael with RESTORE	8/10/17	7. There should immediately be a test well drilled somewhere north of the last set of monitoring wells which showed the presence of EDC. All previous zones should be sampled for all chlorinated hydrocarbons as the well is being drilled.	<u>H-3</u>		
WILLINESTORE		8. EDC causes a collapse of the crystalline structure of local clays – it would be best to plan for future problems rather than planning to put alternatives into places where problems will eventually occur.	<u>H-4</u>		
		9. Water levels in the nearshore wells rise/fall with the tidal pulses of the river. This constant movement of fluid should be factored into the projects of arrival time of the EDC at the Westlake municipal wells before going through the trouble of building the Sulphur Avenue extension alternatives.	<u>H-5</u>		
		10. The impact of railroad vibrations on clay and sand in the project area should be studied – the pulses of a train caused a sudden collapse of the substrate and the train and tracks in India years ago.	<u>A-1</u>		
		11. Annual sampling of the EDC contamination should be occurring and that information made available online for the public.	<u>H-6</u>		
		12. Given the damage to the bridge, it would be better to do a planned removal of the bridge before a cataclysmic collapse, whether or not a replacement bridge is ready.	<u>A-1</u>		

Name (Last/First)	Date	Comment(s)			
		 Regarding the <i>Draft Purpose and Need</i>, the commenter noted that traffic congestion, safety and bridge design issues need to be carefully studied and changes made so that existing problems can be removed. 	<u>A-1</u>		
		2. Regarding the <i>Draft Project Coordination Plan</i> , the commenter stated that this project has been stuck in the planning phase with no practical forward movement. However, the commenter explained that the public involvement opportunities (e.g., website, newsletters, etc.) are welcome improvements.	<u>A-1, F-1</u>		
		 Commenter would like to receive future updates on the proposed project; short notifications via email and lengthy correspondence via mail. 	<u>A-3</u>		
Tritico, Michael with RESTORE Note: These comments are dated 10/25/13 and were based on materials presented at the 8/24/13 Public Scoping Meeting. Commenter resubmitted the	8/10/17	 4. Commenter provided statements related to constructing the new I-10 Calcasieu River Bridge north of its present location: Construct the bridge near Joe Miller Road, in the corridor between Moss Bluff and Gillis, or in the corridor between Gillis and Ragley. Vertical bridge height not a problem in these corridors ROW would likely be less expensive to acquire If constructed south of Ragley, interstate would be out of the area shown by the National Hurricane Center's SLOSH model to be vulnerable to tropical storm surges Would remove impediments to navigation; important to leave the existing navigational clearance so that organizations like Friend Ships can complete their humanitarian work With planning and proper advertisement, a more northern interstate route would not adversely affect development and the economy 	<u>D-1, E-2</u>		
comments in response to the August 3, 2017 Public Meeting.		 5. Commenter provided statements related to the EDC contamination: Bridge increasingly unstable due to age and the EDC contamination. Some FOIA material requests has been redacted. Studies show the effect of EDC on local clays; regional clay is quickly and severely degraded by EDC, losing its ability to bear weight and slow down the movement of fluids. EDC plume may have reached the railroad tracks north of I-10 Concentration of EDC is 90,000 times the LDEQ's RECAP trigger level of 5 ppb. If boring tests for load bearing capacity have been completed, they need to be made public. As of 2009, EDC was within 40 feet of the top of the Chicot Aquifer. Is inevitable that the EDC will enter that aquifer. FOIA materials show that the LDEQ is concerned about new bridge pilings hastening EDC contamination into the Chicot Aquifer. 	<u>A-1, H-1, H-4,</u> <u>H-6, H-7</u>		

Name (Last/First)	Date	Comment(s)	Response Code(s)
Tritico, Michael with RESTORE (continued – 10/25/13 comments)		 EDC is moving in a direction contrary of groundwater flow in the region, caused by the heavy draft of the Westlake Municipal Water Supply well pulling the plume downward and northeastward. Recovery wells are sparingly-efficient and cannot remediate a problem once the contaminants pervade the subsurface. Given the number of people depending on the groundwater, a recovery well field location within the bridge ROW would do the most good, as opposed to the construction of a new bridge. Commenter requests FHWA send more recent and extensive information (e.g., boring data, litigation discussions/status), emphasizing the need for full disclosure related to the EDC. 	<u>A-1, H-1, H-4,</u> <u>H-6, H-7</u>
Unknown 1	8/4/17	When will materials be available showing the alternatives from the public meeting?	<u>A-2</u>
Unknown 2	8/4/17	I am in favor of the project to construct a new I-10 bridge with three lanes in each direction and a shoulder on each side; and new bridge should be just north of the existing bridge.	<u>B-12, D-2</u>
		2. Reject the South alternative, as it will destroy the scenic beauty of our Lake Charles.	<u>C-4</u>
Unknown 3	8/8/17	More cost effective to reduce the proposed bridge that would not touch the EDC spill by adding a two-lane exit ramp high enough to go over the railroad track on Sampson St. and also have an entrance ramp coming back onto I-10 and looping under the interstate to continue east.	<u>A-1, B-1, B-10</u>
OTIKITOWIT 3		2. Think of future infrastructure needs and increase the number of lanes to eight instead of six.	<u>A-1, D-2</u>
		3. Do not support compensated foundation.	<u>C-2</u>
		4. Do not tamper with water source.	<u>E-6</u>
		Want to see a committee of local public officials meet with all parties involved with the pollution issue to get it settled as soon as possible.	<u>A-1</u> , <u>H-1</u>
	8/8/17	2. The design of the bridge could mimic the same I-10 bridge over the Sabine River where large tug boats could continue to service the port property just north of it.	<u>A-1</u> , <u>D-1</u>
Unknown 4		Have three lanes each side and the outside west lane could veer over the railroad tracks and tie into the Westlake entrance road.	<u>A-1, D-1</u>
		4. Support new bridge immediately north of the existing bridge.	<u>B-12</u>
		5. As a small business owner in Sulphur we are seeing the impact of slow traffic with both bridges open. If I-10 is shut down, it would be a disaster for the economy.	<u>A-1, D-4</u>
		6. Neighboring states will help get funding because I-10 is a major pipeline for the Gulf Coast economy.	<u>G-1</u>
		1. Support PBA 3	<u>B-3</u>
Whelan, Wendy	dy 8/3/17	2. Strongly oppose PBA 1, PBA 2, and Sub-Alternative F	C-1, C-2, C-10
vviiciaii, vveilay		3. Do not support Sub-Alts. with Sulphur Ave. extension to Enterprise Blvd.	<u>C-6, C-8</u>
		4. Support Sub-Alternatives A, C and E.	<u>B-5, B-7, B-9</u>
Wranosky, Linda	8/7/17	Support PBA 3.	<u>B-3</u>

Note: * Crawford, Craig submitted one comment by email and one comment on the public meeting comment form. Both included a Do Not Support comment for PBA 4. Accordingly, and as tallied in **Table 7**, Mr. Crawford's Do Not Support comment for PBA 4 was only considered once.

Table 3. Responses to Comments (Response Codes A-I)

A Genera		
Response Code	General Topic Addressed	Response
A-1	General comment or suggestion	Comment noted.
A-2	Public meeting materials	All materials presented at the public meeting, including handouts, the repeating presentation, and exhibit boards can be found on the project website www.i10lakecharles.com under the public involvement tab. Commenters requesting the location of public meeting materials were contacted by the Project Team.
A-3	Request to receive future updates on the project	Commenter has been added to the project mailing list and will receive updates as they are available.
A-4	Comment requiring revision by Project Team	Comment noted and the applicable exhibits will be revised.

B Support	B Support specified PBA, Sub Alternative or project feature			
Response Code	General Topic Addressed	Response		
B-1	Support PBA 1	Comment noted and incorporated into alternatives screening analysis.		
B-2	Support PBA 2	Comment noted and incorporated into alternatives screening analysis.		
B-3	Support PBA 3	Comment noted and incorporated into alternatives screening analysis.		
B-4	Support PBA 4	Comment noted and incorporated into alternatives screening analysis.		
B-5	Support Sub-Alt. A	Comment noted and incorporated into alternatives screening analysis.		
B-6	Support Sub-Alt. B	Comment noted and incorporated into alternatives screening analysis.		
B-7	Support Sub-Alt. C	Comment noted and incorporated into alternatives screening analysis.		
B-8	Support Sub-Alt. D	Comment noted and incorporated into alternatives screening analysis.		
B-9	Support Sub-Alt. E	Comment noted and incorporated into alternatives screening analysis.		
B-10	Support Sub-Alt. F	Comment noted and incorporated into alternatives screening analysis.		
B-11	General support for new bridge (but no specific alternative identified)	Comment noted and incorporated into alternatives screening analysis.		
B-12	Support new bridge along existing I-10 corridor	Comment noted and incorporated into alternatives screening analysis.		
B-13	Support Sulphur Ave. extension to Enterprise Blvd.	Comment noted and incorporated into alternatives screening analysis.		

C Do not s	C Do not support specified PBA, Sub Alternative or project feature		
Response Code	General Topic Addressed	Response	
C-1	Do not support PBA 1	Comment noted and incorporated into alternatives screening analysis.	
C-2	Do not support PBA 2	Comment noted and incorporated into alternatives screening analysis.	
C-3	Do not support PBA 3	Comment noted and incorporated into alternatives screening analysis.	
C-4	Do not support PBA 4	Comment noted and incorporated into alternatives screening analysis.	
C-5	Do not support Sub-Alt. A	Comment noted and incorporated into alternatives screening analysis.	
C-6	Do not support Sub-Alt. B	Comment noted and incorporated into alternatives screening analysis.	
C-7	Do not support Sub-Alt. C	Comment noted and incorporated into alternatives screening analysis.	
C-8	Do not support Sub-Alt. D	Comment noted and incorporated into alternatives screening analysis.	
C-9	Do not support Sub-Alt. E	Comment noted and incorporated into alternatives screening analysis.	
C-10	Do not support Sub-Alt. F	Comment noted and incorporated into alternatives screening analysis.	
C-11	Do not support general construction in EDC contamination area	Comment noted and incorporated into alternatives screening analysis.	

D Questio	ons/comments on project design a	and construction
Response Code	General Topic Addressed	Response
D-1	Suggest new alternative for evaluation	Comment noted. The PBAs were identified after various stages of alternatives development and refinement, including a Feasibility Study, multiple bridge height and engineering studies, and public and agency coordination, thus encompassing the range of alternatives for the proposal. NEPA requires an EIS to examine all reasonable alternatives. In accordance with NEPA, a reasonable range of alternatives representative of the full spectrum of Reasonable Alternatives was explored and objectively evaluated for the project.
D-2	Project design features	All of the proposed PBAs include the following improvements along I-10 between the I-210 interchanges: Proposed widening of I-10 b/w the I-210 interchanges to six, 12-ft lanes (3 in each direction) with 12-foot shoulders Proposed replacement of I-10 EB to I-210 SB ramp bridge Proposed 6 lane overpass at PPG Dr. Proposed replacement/improvement of US 90 overpass to allow I-10 to be widened Proposed access improvements to Sampson St. to/from I-10 Proposed 6-lane overpasses to improve vertical clearance and new U-Turns under the overpasses at the following locations: Veterans Memorial Blvd, Ryan St., Bilbo St., Krikman St., Enterprise Blvd., Shattuck St., Railroad Crossing, and Opelousas St. Proposed improvements to US 171 overpass to allow I-10 to be widened and improve vertical clearance Replacement of the existing Calcasieu River Bridge Required drainage improvements Regarding the proposed number of I-10 main lanes: The current proposal for I-10 is three lanes in each direction. The traffic analysis to be completed for the Reasonable Alternatives as part of the EIS would confirm if the proposed three lanes in each direction are anticipated to meet the needs of future traffic or if additional lanes would be needed. Regarding the bridge grade: The existing steep grades slow traffic on the up-slope and make it more difficult to stop on the downslope. The existing bridge grade is 5% on the east approach. That exceeds the recommended 3% maximum grade of DOTD design guidelines. The grade of the new Calcasieu River Bridge will be 3%, which is anticipated to improve driver safety.
D-3	Compensated foundation	A compensated foundation consists of excavating a volume of the ground below grade, reducing the weight and partly or wholly compensating for the loads imposed by the new bridge. If constructed, the compensated foundation would be constructed above any known EDC contamination. Should PBA 2 (compensated foundation) be identified as a Reasonable Alternative, the design and impacts of a compensated foundation would be evaluated in detail within the EIS.
D-4	Maintenance of traffic	Travel on I-10 would be maintained during the project's construction. This includes maintaining traffic on I-10 while the new Calcasieu River Bridge is constructed and while I-10 between the I-210 interchanges is under construction.
D-5	Moveable bridge	Regarding the location of the moveable bridge: There is no movable bridge structure proposed along the I-10 corridor in the project area.

		The new I-10 Calcasieu River Bridge is proposed as a fixed, non-moveable structure. There is a movable bridge proposed as part of the Sulphur Avenue extension; however, this will not be part of the interstate
D-6	Use Disadvantage Business Enterprise (DBE) firm and local contractors.	Comment noted. Use of DBE firms and/or local contractors would be per the DOTD policy in effect at the time of contract advertisement.

E Questio	E Questions/comments on environmental impacts/issues		
Response Code	General Topic Addressed	Response	
E-1 a a	Potential social, economic and environmental impacts and/or request for protection of environmental resources in the study area.	Social, economic, and environmental resources were considered during the development, evaluation and screening of Preliminary Alternatives in an effort to avoid and/or minimize any potential future negative impacts on these resources. Once the Reasonable Alternatives are finalized, the alternative designs will be further refined and evaluated as part of the EIS. These refined designs will be specifically evaluated for their potential direct, indirect and cumulative impacts on the study area resources. Efforts would be made to avoid, minimize, or mitigate potential environmental impacts associated with the proposed Reasonable Alternative(s) for the project.	
		Regarding potential impacts to Corporation Cemetery and Cantonment Atkinson/Bilbo Cemetery: ROW impacts are not anticipated to Corporation Cemetery and Cantonment Atkinson/Bilbo Cemetery for PBAs 1, 2 and 3. PBA 4 potentially could require ROW from Cantonment Atkinson/Bilbo Cemetery; however DOTD would work to refine the alignment to either avoid or minimize, to the extent practicable, ROW needs.	
E-2	USCG coordination and navigational clearance	Per the 2014 Navigational Study for the I-10 Calcasieu River Bridge and Approaches, a 73-ft. vertical clearance for the new Calcasieu River Bridge (as recommended by DOTD) blocks navigation for five existing vessels and three reasonably foreseen future vessels, all owned (or will be owned) by Friend Ships. In accordance with the USCG White Paper (USCG Bridge Program, Reasonable Needs of Navigation White Paper, 2012) and as part of the EIS, DOTD will evaluate if the vessels can be modified to pass under the proposed bridge (if economically feasible) and determine if there are alternative routes available for passage. DOTD is currently working with Friend Ships to identify potential locations south of the new bridge where their vessels could be relocated. DOTD is also coordinating with the USCG Bridge Administrator on the navigational clearance determination and following USCG guidance.	
		Regarding the request that the navigational clearance determination be included as part of the public record: Documents associated with the navigational clearance determination will be included as part of the public record per the discretion of the USCG.	
E-3	Hazardous materials (non-EDC)	Lead safe work practices would be utilized if lead is encountered at any stage of the proposed project. Regarding the Olin remediated landfill, the proposed preliminary alignment for PBA 4 could potentially impact the remediated landfill. However, as the proposed alignment is preliminary, DOTD would work to avoid or minimize impacts to the remediated landfill, as practicable. Should the proposed alignment be selected as the Preferred Alternative and impact the remediated landfill, DOTD would follow the appropriate procedures to mitigate and	

		monitor the impacts as regulated by the EPA.
E-4	Bicycle and pedestrian facilities	DOTD and FHWA are committed to the incorporation of Context Sensitive Solutions (CSS) and the complete streets policy into the proposed project design. CSS is when interdisciplinary teams work with public and agency stakeholders to tailor solutions to the setting; preserve scenic, aesthetic, historic, and environmental resources; and maintain safety and mobility. The intent of the DOTD complete streets policy is to create a comprehensive, integrated, connected transportation network for Louisiana that balances access, mobility, and safety needs of motorists, transit users, bicyclists, and pedestrians of all ages and abilities.
E-5	Right-of-Way (ROW)	Regarding ROW acquisition: Efforts would be made to avoid, minimize, or mitigate potential environmental impacts associated with the proposed alternative(s) to ROW and structures. Real property would be acquired in accordance with the Uniform Relocation Assistance and Real Property Acquisition Policies Act which provides important protections and assistance for people affected by Federally funded projects. It ensures that people whose real property is acquired, or who move as a result of projects receiving Federal funds, will be treated fairly and equitably and will receive assistance in moving from the property they occupy. Regarding if ROW will be required for the project: It is anticipated that the proposed project would require ROW at various locations along the project corridor. ROW needs differ amongst the PBAs and Sub-Alternatives. Minimize ROW impacts is one of the 11 project objectives used to screen the PBAs. Potential ROW impacts associated with each PBA and their associated Sub-Alts. can be found in the Objectives Screening Matrix presented in Attachment C . Once the Reasonable Alternatives are identified, the design schematics will be advanced and potential ROW impacts refined as part of the EIS. It is at that time that specific ROW impacts will be determined. Regarding the residence on Church St.: It is not anticipated that ROW would be needed from the property on Church St. given its distance from the I-10 corridor and that the improvements to I-10 in that area are proposed to remain along the same corridor as existing I-10.
E-6	Chicot Aquifer	The EDC release is located above the Chicot Aquifer, which supplies the drinking water for Lake Charles and surrounding communities. The traditional construction of an elevated bridge structure would require driving piles in the EDC area, which in turn could aggravate the downward migration of the contaminant towards the aquifer. DOTD developed technical solutions to avoid or minimize the risk of construction in the EDC area. Those technical solutions include constructing the I-10 bridge west approach span using a compensated foundation (PBA 2), spanning the EDC area with a long span bridge (PBA 3), or completely avoiding the EDC area by constructing a new bridge across Lake Charles south of the existing I-10 corridor (PBA 4). In addition, elevating Sampson St. above the railroad lines would require driving piles in the EDC area. To avoid or minimize risk of construction in the EDC area, DOTD developed options for circumventing the at-grade railroad crossings. These options involve the extension of Sulphur Avenue west across the Calcasieu River, with various options for tying into I-10 (Sub-Alternatives A-E). It is DOTD's goal to avoid or minimize any risk associated with construction in the EDC area.

F Question	ns/comments on the EIS process	
Response Code	General Topic Addressed	Response
F-1	Streamline the environmental process	The environmental process will occur in accordance with NEPA, the federal regulations for implementing NEPA (40 CFR Parts 1500-1508; 23 CFR 771), and other federal legislation further refining the environmental process (e.g., SAFETEA-LU, MAP-21, Fast Act). It is the goal of the Project Team to complete the environmental process in a timely manner; and efforts to streamline that process may be considered if determined practicable and in accordance with federal regulations and legislation.
F-2	EIS has been on hold for four years, which does not meet federal regulatory standard.	DOTD has and will continue to follow the EIS process in accordance with the Council on Environmental Quality (CEQ) issued Regulations for Implementing the Procedural Provisions of the National Environmental Policy Act (40 CFR §§ 1500–1508) and the Federal Highway Administration (FHWA) issued regulations (23 CFR § 771), Environmental Impact and Related Procedures.
F-3	DOTD has skipped steps in the EIS process that were presented at the 2013 Scoping Meeting	Project timelines are necessarily updated based on project needs, changing circumstances, and/or complexity of issues. While the project timeline was modified from the version presented at the October 2013 scoping meeting, none of the steps mandated by NEPA and governing the EIS process, nor evaluations/materials resulting from those steps have been eliminated. The public and agencies were given the opportunity as part of the August 3, 2017 public and agency meetings to review and comment on all aspects of the proposed project, and that input will be considered and incorporated as practicable.
F-4	Feasibility and scoping process are outdated	The DOTD recommended PBAs available for comment at the August 3, 2017 public and agency meetings were identified after various stages of development and refinement and were not solely based on the previously completed and approved Feasibility Study. Other studies and input subsequent to the Feasibility Study shaped the refinement of the PBAs including but not limited to several marine use/bridge height evaluations, previous public and agency input, and other engineering and environmental factors such as the discovery of EDC contamination in the project area. Please also see response codes F-2 and F-3.
F-5	Screening process used outdated EDC data	Data used at the August 3, 2017 public meeting was based on EDC Isoconcentration Maps from First Quarter 2016. Per your comments, "Data collected by Phillips 66 over the last 1 ½ years indicates the EDC plume is no longer present north of I-10." EDC has been regularly detected in the area north of I-10, and was detected in MW-34UI and in the northern-most perimeter wells as recently as the Third Quarter of 2017. Monitoring well data has historically shown that EDC is present north of I-10, spanning through the DOTD right-of-way and migrating towards, and now past, the original northern perimeter wells. Given that unknowns remain about the full extent, depth and migration of the EDC, First Quarter 2016 data were used because the data better correspond with the dynamics of the contamination spill over a broader period of time.
F-6	The screening process was completed without input from the public and stakeholders.	The purpose of the August 3, 2017 meetings was to provide the public and agencies an opportunity to review and comment on all aspects of the proposed project, including the Preliminary Alternatives, alternatives screening process, criteria/measures used to screen the alternatives, screening results, and DOTD recommended Reasonable

		,
		Alternatives, all of which were in draft/preliminary form awaiting public and agency comment. The detailed screening matrices showing how each screening objective was evaluated and rated for each Preliminary Build Alternative were also available for review at the public meeting. To encourage further transparency and public input, all meeting materials have been made available on the project website. In accordance with the NEPA process, public and agency input solicited will be considered and materials modified based on this input as determined practicable. Note that the Reasonable Alternatives presented at the public meeting are recommendations only, and the finalization of those recommendations will not occur until after public and agency input obtained from the August 3, 2017 meetings are incorporated into the screening analysis. Ultimately, the final identification of Reasonable Alternatives will be made by DOTD in coordination with FHWA based on professional judgement with consideration given to all project objectives, including environmental issues, cost, engineering issues, and public and agency input.
F-7	The purpose and need presented at the public meeting is different than what was presented at the 2013 Scoping Meeting	The purpose and need of the project has not changed from that presented at the 2013 scoping meeting. Congestion and safety issues at Sampson St. are included under the project needs of Increased Traffic Congestion and Roadway and Bridge Safety Concerns, respectively. Accordingly, the congestion and safety concerns at Sampson St. were included as part of the Tier 1 Purpose and Need Screening. The purpose and need of the project is to improve the lack of system connectivity, reduce traffic congestion, improve structural and functional roadway and bridge deficiencies, and improve safety.

G Questic	G Questions/comments about project financing and cost		
Response Code	General Topic Addressed	Response	
G-1	Project funding	Construction of the proposed project will be dependent on funding availability. The project could be funded from multiple potential sources including but not limited to Federal aid, state funding, private contributions, and tolling. It is unlikely that the entire project would be funded at one time. A key activity within the NEPA process is to further evaluate the Reasonable Alternatives, identify segments of independent utility and develop an implementation schedule for those improvements based on priorities tied to purpose and need and project goals. As the design schematics of the Reasonable Alternatives are advanced, and cost estimates become more refined, DOTD will identify the set of "most likely improvements", which could form the basis for the first construction phase.	
G-2	Cost of project	Optimize cost is one of 11 project objectives used to screen the PBAs. The estimated construction cost (2017) for the project ranges from approximately \$600 Million for PBA 1; approximately \$770 to \$800 Million for PBA 2 – compensated foundation (depending on the Sub-Alt.); approximately \$820 - \$850 Million for PBA 3 – long span bridge (depending on the Sub-Alt.); and approximately \$990 Million to 1 billion for PBA 4 – South Corridor (depending on the Sub-Alt.). The preliminary construction costs associated with each PBA and associated Sub-Alt. can be found in the Objectives Screening Matrix presented in Attachment C . Cost estimates will be refined as the design schematics of the Reasonable Alternatives are advanced through the NEPA process.	

	ents related to EDC contamination	
Response Code	General Topic Addressed	Response
H-1	EDC contamination	The EDC contamination in relation to the proposed project will be evaluated in the EIS based on available information and to the extent practicable in coordination with Louisiana Department of Environmental Quality (LDEQ). Assessment and remediation of the EDC spill is the responsibility of the entity responsible for its release, Phillips66. LDEQ is working with Phillips66 on the monitoring and remediation of the EDC contamination. Well monitoring findings are available to the public through the LDEQ Electronic Document Management System (EDMS). If a proposed alternative with the potential to encounter EDC is selected as the Preferred Alternative, DOTD would follow the proper procedures to ensure the safety of its employees, contractors, and the public.
H-2	Westlake municipal water wells just north of the railroad seem to be drawing the EDC upgradient toward themselves. The EDC would destroy the soil beneath Sub-Alts A-E and PBAs 2 and 3.	The potential impact of EDC on the integrity of soils, along with other mitigating factors, will be considered during the evaluation process of the Reasonable Alternatives and beyond, regardless to which Alternative is selected as "Preferred".
H-3	There should immediately be a test well drilled somewhere north of the last set of monitoring wells which showed the presence of EDC. All previous zones should be sampled for all chlorinated hydrocarbons as the well is being drilled.	DOTD concurs that additional wells, both north and northwest of the current northern most wells, would be beneficial to the assessment and remediation of the EDC release. This has been conveyed to Phillips66 and the LDEQ.
H-4	EDC causes a collapse of the crystalline structure of local clays – it would be best to plan for future problems rather than planning to put alternatives into places where problems will eventually occur.	DOTD recognizes the behavior of EDC in soil and agrees with the reference to its structural impact on clays. Such behavior and other challenges associated with EDC are considered in the various alternatives evaluated, which is a required component of the NEPA process. Ultimately, EDC impacted alternatives could be screened out via the NEPA process.
H-5	The water levels in the nearshore wells rise and fall with the tidal pulses of the river. This constant movement of fluid should be factored into the projects of arrival time of the EDC at the Westlake municipal wells before going through the trouble of building and of the alternatives that involve a Sulphur Avenue extension.	Additional hydrogeological data from the referenced area would certainly be supportive to pending decisions with respect to the dynamics and resulting impacts from the rise and fall of tides and movement of EDC. Fate and Transport Modeling of EDC in this area could be an effective tool to acquire additional information in this instance. As referenced in response H-2, DOTD agrees that additional test data would be beneficial to the decision-making process. DOTD is hopeful Phillips66, as the responsible party for the EDC release, recognizes the value of this needed information and will consider such a study.
H-6	Annual sampling of the EDC contamination should be occurring and that information made available online for the public.	Monitor wells associated with the EDC release are actually required to be sampled and tested on a quarterly basis in the North Clooney Loop Area, which includes the area north of I-10. The results from this testing are compiled and reported semi-annually and subsequently made available to the public via LDEQ's Electronic Document Management System (EDMS). All other test data reported to the LDEQ for this site (as with all other sites) are also recorded in this database. As the owner of the database,

		LDEQ may have exceptions to their publication routine where it may be necessary to withhold or redact sensitive data or information.
H-7	The EDC plume is moving in a direction contrary to the usual direction of groundwater flow in this region, caused by the heavy draft of the Westlake Municipal Water Supply well pulling the plume down and northeastward. Westlake may need to find a new public water supply.	Further study would be needed to determine if the cause for contaminants detected in the northern perimeter wells can be attributed to public and/or industrial supply pumping wells in deeper zones and/or if there is an alternate source. Detections of petroleum-related volatile hydrocarbons in the northern perimeter wells, which differs from constituents detected to the south of I-10 related to the EDC pipeline and tank releases, indicates a potential contributing source unrelated to the EDC releases.

I Unclear Comment		
Response Code	General Topic Addressed	Response
I-1	Church St. property	The westbound approach of the Calcasieu River Bridge would be over 1.5 miles from the specified residence. Unclear about the "bridge going in a circle from right to left". Assuming commenter is referencing the I-10 overpass going over the railroad. The project proposes the replacement of the I-10 railroad overpass, but the new overpass would remain in the same location as the existing overpass.
I-2	Railroad Ave. and Hersey St.	Assuming commenter is referencing I-10 as it moves east toward Railroad Ave. and Hersey St. Improvements to I-10 in the area near Railroad Ave. and Hersey St. will remain in the same general footprint as existing I-10.
I-3	Truss systems	Assuming commenter is referencing the compensated foundation alternative. If accurate, see response code D-3.





Air Traffic Organization Central Service Center 10001 Hillwood Parkway. Fort Worth, TX 76177

Federal Aviation Administration

AUG 0 1 2017

Noel Ardoin Environmental Engineer Administrator Louisiana Department of Transportation and Development P.O. Box 94245 Baton Rouge, LA 70804-9245

Dear Mr. Ardoin:

Thank you for your letter dated July 18, 2017, addressed to Mr. Michael O'Harra regarding the Environmental Impact Statement on proposed project 1-10 Calcasieu River Bridge, Calcasieu Parish, LA., State Project No. H.003931 F.A.P. No. BR-10-1(212)29. We normally participate in, and comment on, other federal agency environmental documents only from the perspective of the Federal Aviation Administration's (FAA) areas of responsibility; that is, whether the proposal will have effects on aviation and the National Airspace System. We generally do not provide comments from an environmental standpoint.

From a regularity perspective, you may need to consider if notice to FAA is required for the effect of the proposed actions on airspace. We encourage you to coordinate with the FAA's Obstruction Evaluation/Airport Airspace Analysis (OE/AAA) office so that we can review the alternatives to provide you with the possible impacts. For information on the requirements for notifying the FAA, instructions for completing the forms, or other information regarding the airspace notification process, please visit the OE/AAA web site at: https://oeaaa.faa.gov/oeaaaEXT/portal.isp.

You will need to determine if formal notice to the FAA is required for the effect of the proposal on airspace. The requirements for this notice may be found in Title 14 of the Code of Federal Regulations, Part 77, Objects Affecting the Navigable Airspace. If any part of the project exceeds notification criteria under FAR Part 77, notice to the FAA is required at least 30 days prior to the proposed construction date. More information may be obtained at the OE/AAA web site.

Sincerely,

Robert W. Beck

Manager, Operations Support Group

ATO Central Service Center



DEPARTMENT OF CULTURE, RECREATION & TOURISM
OFFICE OF CULTURAL DEVELOPMENT

BILLY NUNGESSER LIEUTENANT GOVERNOR

State of Louisiana Office of the Lieutenant Governor

RENNIE S. BURAS, II
DEPUTY SECRETARY

June 7, 2017

April English
Environmental Planner
HNTB Corporation
Attn: LDOTD Environmental Section
P.O. Box 94245
Baton Rouge, LA 70804-9245

Re: Section 106 Review and Compliance State Project No. H.003931 Proposed I-10 Calcasieu River Bride Project Built Environment Comments on Interchange Preliminary Build Alternatives Calcasieu Parish, LA

Dear Ms. English:

Thank you for your letter of May 8, 2016, concerning the above-referenced undertaking. We are of the opinion that the interchanges proposed in Preliminary Build Alternatives numbers two through four have the potential to adversely affect historic standing structures. In order to comment per the Section 106 Regulations (36CFR800), we would need Areas of Potential Effects established for those Preliminary Build Alternatives and an assessment on the National Register of Historic Places-eligibility made on all of the standing structures located within the Areas of Potential Effects.

If you have any questions, please contact Mike Varnado in the Division of Historic Preservation at (225) 219-4596.

Sincerely,

Kristin Sanders

Katin P. Sanders

Deputy State Historic Preservation Officer

KS:MV:s



UNITED STATES ENVIRONMENTAL PROTECTION AGENCY **REGION 6** 1445 ROSS AVENUE, SUITE 1200

DALLAS TX 75202-2733

August 14, 2017

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive, Suite 230 New Orleans, LA 70122

RE: EPA Comments on the Reasonable Alternatives involving Sulphur Ave. Extension to Enterprise Blvd. and Potential Impacts Due to Proximity to the Gulf State Utilities North Ryan Street Superfund Site

Dear Sir or Madam,

The United States Environmental Protection Agency participated in the agency and officials meeting for the I-10 Calcasieu River Bridge Project on August 3, 2017. The consultant team provided an overview of the project and requested comments. During the meeting, several reasonable alternatives and sub-alternatives were identified for the project. After reviewing the presentation materials at the agency meeting, EPA has determined that the reasonable alternatives which involve the extension of Sulphur Ave. to Enterprise Blvd. have the potential to impact the Gulf State Utilities North Ryan Street Superfund Site (Site).

The Site is located at 303 North Ryan St., Lake Charles, Louisiana. A manufactured gas plant (MGP) operated at the Site between 1916 and 1932. Site activities contaminated soil with polychlorinated biphenyls (PCBs), volatile organic compounds (VOCs) and polycyclic aromatic hydrocarbons (PAHs). Site activities contaminated sediment and groundwater with VOCs and PAHs. The United States Environmental Protection Agency proposed the Site for inclusion on the Superfund program's National Priorities List (NPL) in February 1995.

In light of the known contamination remaining in the subsurface soils near the proposed road construction areas depicted in the reasonable alternatives which involve the extension of Sulphur Ave. to Enterprise Blvd., the EPA believes that it is appropriate to provide comments for consideration that describe preliminary measures to prevent exposure to such contamination.

A Site removal action was performed between 2000 and 2003 and included dredging, dewatering and disposal of sediment from the Calcasieu River, and excavation of storm sewer piping, bedding soil, and sediments from the west drainage ditch. The removal action also included excavation, consolidation and capping of the exposed tar area.

In 2004, EPA issued a Record of Decision (ROD) addressing soil and sediment contamination at the Site, defined as Operable Unit 2 (OU2). This OU2 ROD stated that the site removal action addressed contaminated soil, sediment, and source material and eliminated the need to conduct further remedial action, and therefore the selected remedial action was "No Further Action Necessary." It is important to note that the remedial action for OU2 (identified as the former exposed tar area on the attached Figure 2 map) left low-level threat contamination in the subsoil. As a result, ongoing Site operations and maintenance are required to ensure that no unacceptable exposure risks posed by the Site occur in the future. Those ongoing Site operations and maintenance include but are not limited to Site maintenance, monitoring and compliance with the restrictions of the conveyance notice filed with the Entergy property deed that describes the land use restrictions necessary to control site worker and environmental exposure to low-level threat contamination remaining onsite. These restrictions prohibit any unauthorized excavations or use of contaminated soil and limit the future use of the property to commercial or industrial purposes. The attached Figure 1 from EPA's 2015 Five Year Review for the Site shows the extent of the conveyance notice.

If any Reasonable Alternative which involve the extension of Sulphur Ave. to Enterprise Blvd. is selected to be the "Recommended Preferred Alternative" in the project's Environmental Impact Statement process, then EPA requests coordination prior to design and construction activities.

- Prior to the start of any construction or excavation activities, EPA must review and approve a Soil and Stormwater Management Plan that will prevent exposure to contamination remaining in the subsurface soil. The plan should include but not be limited too, sampling and analysis plan of subsurface soils for PCBs, VOCs, and PAHs; dealing with and disposal of contaminated soil encountered during excavation; ensuring that disposal of the contaminated soil is in accordance with State and Federal law; and stormwater measures utilized to minimize stormwater contact with contaminated soil.
- Any future property owner(s) of all or a portion of the property within the boundaries of the conveyance notice describing necessary land use restrictions to control and limit exposure to Site contamination must comply with those land use restrictions.
- EPA recommends that the project follows the requirements of 29 CFR 1910.120 concerning HAZWOPER training requirements for construction workers who may be working with potentially contaminated subsurface soils.

If you have any questions concerning this letter or the comments provided herein, please do not hesitate to contact me at (214) 665-7393.

Sincerely,

Casey Luckett Snyder Remedial Project Manager

Superfund Division

LA/NM/OK Section (6SF-RL)

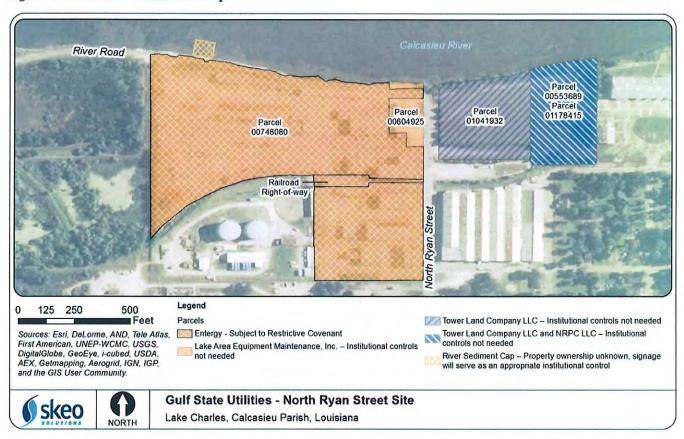
asey Luckett Snyler

Cc via electronic copy:

Robert Harris, LDEQ

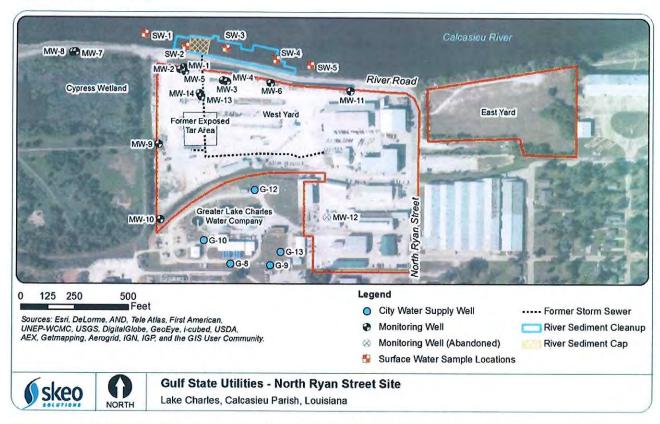
Toby Chu, Entergy

Figure 1: Institutional Control Base Map



Disclaimer: This map and any boundary lines within the map are approximate and subject to change. The map is not a survey. The map is for informational purposes only regarding EPA's response actions at the Site.

Figure 2: Detailed Site Map



Disclaimer: This map and any boundary lines within the map are approximate and subject to change. The map is not a survey. The map is for informational purposes only regarding EPA's response actions at the Sit



sunday August 13, 2017

I-10 Calcasieu River Bridge Project
 c/o HNTB Corporation
 2021 Lakeshore Drive , Suite 230
 New Orleans , LA 70122

Ref: 1-10 Calcasieu River Bridge Project Comments

I would like to submit the following comments for consideration in reference to the above project.

To get things started — lets consider making a swap; make the present 1-210 Loop the designated 1-10 Interdtate Highway route across Lake charles and then make the present 1-10 Interstate route the I-210 Loop around Lake Charles

The problems with support foundations due the environmental condition of the soil will never go away. If the North (or West) approach side of the I-210 Loop is environmental free from sub-surface contamination, then the swap could be possible.

If we can get past this problem, then lets consider the following:

- A. The original Interstate Highway System was surveyed and the routes designated many years ago (probably in the 1950's).
 This present route of I-10 crossing Lake Charles on the North Side of the City should be reevaluated and consideration be given to the possibility of a Southern route across Lake Charles.
 - (a) SAFETY ——— the heavy refining industry expansion in the Westlake Area (big change from processing sweet crudes vs processing heavy sour crudes) plus newer units with maybe higher pressures and tempetures.
 - (b) SAFETY ---- close proxicimity to railroad serving the Westlake Area industry.
 - c (?) SAFETY —— the Industrial Complex on the Sulphur side near the present I-210 Loop route becomes part of the equation at this point.

SUMMARY --- potential for Interstate Highway closure due to explosions, fires, chemical releases, etc. does exist

- (d) DEMOGRAPHY -- significant changes since the 1950's --- an overall movement Southward
 - malls, retail stores, auto dealers, gaming, education, medical, LNG, subdivisions, office buildings, air port, air craft industry, etc.

- (e) POLITICAL ---lakefront development concerns, Riverboat gambling, North Lake Charles business development,
- B. If all concerns could work with this proposal of designation exchange then my suggestion would be:

----- erect the proposed six lane I-10 replacement bridge adjacent to the present I-210 Loop bridge If the three year time frame and the \$500 million budget is workable in this case and the other two existing bridges can be made safe for three more years,

next the existing I-10 bridge could be replaced with the option already in place of erecting a new lower level, 4 lane instead of 6 lanes, and being designated the new I-210 Loop bridge.

demolition of the two bridges replaced would be necessary for safety and ship traffic.

Thanking you for your efforts to this MAJOR Project,

Abraham Abdalla P.O. Box 59

De Quincy, LA. 70633-2059

From: <u>I-10 Lake Charles Bridge</u>

To: April English
Subject: Suggestion

Date: Friday, August 04, 2017 8:22:34 PM

You received a message from Alejandroluiss@gmail.com

I think a underwater tunnel would easy and cheaper to build



Please Print

Name: Did & Emily Agroaph

I-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End)
State Project No. H.003931

Public Meeting Comment Form

Please provide your comments on the following items:

- Preliminary Alternatives
- · Alternatives Screening Methodology and Results
- Recommended Reasonable Alternatives for further evaluation in the Environmental Impact Statement (EIS)

The Recommended Reasonable Alternatives are as follows:

- Preliminary Build Alternative 2, Sub-Alternatives A-E
- Preliminary Build Alternative 3, Sub-Alternatives A-E

Please return this completed form at the comment table or to a Project Team member.

You can also submit comments online at www.i10lakecharles.com or by U.S. mail to the following address:

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

Address: 2609 Rhelf John, Calve Cherres, UA 70611
Email: ethogsh 13 @ outlook. com
Agency (if applicable): (85 dents
Would you like to receive future updates on the project? Yes or No (circle one)
Comments:
first and twemost, then you for taking the time to present
first and foremost, then hyon for taking the time to present this information to our community. We are excited about the
south that is rating bloom but out that course come
Prin. The biggest pain in my opinion is transportation.
Jain. The biggest pain in my opinion is transportation. Speaking with the experts @ even Startion In home three (Continued on Back)
(Continued on Back)

Sollowing Suggestins:
1. PBA 1 Should NOT be an alternative
- Though it is the Cheapest alterative it could potentially
_ cost more in the long run. If the water is cartaminated
some of the projected projects could upull their business
Lansing one City to so from boom to bust. We could bot also have increased illness at worst a public health
Latalso have increased illness at worst a public health
Cr515.
2. I agree with the recommended PBA 2 or 3 favoring PBA
3 with SubAHB, Aviding J-10 by placing ahaltwratine
from Sampson Street to Enterprise 3/Vd would-accomplish a bew things. I would help get those from westlake to
Enterprise allowing direct access to lake Chenes of toward
171/Moss Bluff orea. Huy 171 is very cargested in the am
+ pm. Mony Motarist cut through wash lake Chures using
Simmons Street, Docoos Blud, and Fit Zenreity load to set
to I-10 due to the increased traffic. This option will help to improve traffic flow of reduce the time of James to get from
Lake Cheres to Westlahe.
Think you again for taking the time to consider my low
thoughts of opinion.
Sincerely,
Emily S. Ashwarh
337-764-6386

From: <u>I-10 Lake Charles Bridge</u>

To: April English

Subject: I10 Calcasieu River Bridge Project No. H.003931 / No. BR-10-1(212)29

Date: Monday, August 14, 2017 4:02:51 PM

You received a message from charlieatherton@suddenlink.net

II0 Calcasieu River Bridge Project State Project No. H.003931 / Federal Aid Project No. BR-10-1(212)29 Charlie Atherton Public Comments 8/14/17

To Decision Makers,

The Calcasieu River Bridge should remain at its current height of 135 feet. This current height is not by accident. This height is engineered to allow for the passage of ships that utilize the full carrying capacity of the Calcasieu River north where the water depth is naturally up to 61feet deep. If the Titanic was afloat today the Calcasieu River would allow the passage of the Titanic under the existing 135 foot bridge as originally designed. After WWII the navy docked hundreds of ships for miles along the river upstream of the bridge, proving navigation suitability. Shipping north of the bridge was originally hampered by the non-alignment of two railroad bridges until recent years when one of the bridges has now been removed allowing large ships to once again navigate upstream. The low level bridge concept was originally thought up and politically driven by ConocoPhillips with the hope that the EDC contamination under the bridge would not be found out. Local elected officials fast tracked the decision for a low level bridge over the objection of the public. Since everyone now knows how severe the EDC contamination by ConocoPhillips is and is now being addressed by the agencies, the bridge should remain at its current height to allow future development of the miles of naturally deep water north of the bridge. Friend Ships discovered this secret long ago and utilizes the river along with others who want to bring in large ships. The Lake Charles Harbor and Terminal District passed a resolution to keep the bridge at its current height so they can fully utilize public and port property north of the bridge.

We are requesting that the official paper trail with all of the appropriate legal signatures that changes the bridge height from 135 feet to an illegal 73 foot height, be entered into the public record of this project.

We do not believe the all the agencies with legal authority and legislative oversight have all legally followed the required public participation process or have actually signed off on the decision for an illegal low level bridge to be built across the Calcasieu River on I10, especially absent is the Bridge Administration of the Coast Guard, Office of Bridge Administration.

We ask that the I10 Bridge remain at the 135' current correct height over the Calcasieu River to maximize the future navigational use and development of the naturally deep and protected waters. This is the only remaining land readily available on the Calcasieu River for ship berthing, docks, and economic marine development.

Charlie Atherton

122 Vine St.

Sulphur, La. 70663

NOTE; PICTURES ARE INCLUDED BUT DO NOT SHOW UP. I WILL ALSO SEND BY EMAIL TO Joachim.Umeozulu@LA.GOV

Excerpt from KPLC TV 12/19/07;

Concerned citizen Charlie Atherton says a 135 foot bridge is the way to go. "To build a new I-10 bridge less than 135 feet in height to kill shipping, economic development north of I-10 is against federal law, a disservice to the public, an abuse of power by decision makers, and a bad mistake that'll never be corrected."

As expected, committee members voted four to one in agreement with the state's recommendation of a clearance of 73 feet. Lake Charles Mayor Randy Roach was the lone no vote. He feels 90 feet would have been a reasonable compromise. "This decision is a hundred year decision. It's a decision that's going to affect this community for years to come. Here's a deep water area that's naturally deep water, and we won't be able to access it because the bridge will be too low."

http://www.charts.noaa.gov/OnLineViewer/11347.shtml shows naturally deep water that has never been dredged

that is as deep as 61 feet.

TITLE 33 > CHAPTER 11 > SUBCHAPTER I > § 494

Prev | Next

§ 494. Obstruction of navigation; alterations and removals; lights and signals; draws How Current is This?

No bridge erected or maintained under the provisions of sections 491 to 498 of this title, shall at any time unreasonably obstruct the free navigation of the waters over which it is constructed, and if any bridge erected in accordance with the provisions of said sections, shall, in the opinion of the Secretary of Transportation at any time unreasonably obstruct such navigation, either on account of insufficient height, width of span, or otherwise, or if there be difficulty in passing the draw opening or the drawspan of such bridge by rafts, steamboats, or other water craft, it shall be the duty of the Secretary of Transportation after giving the parties interested reasonable opportunity to be heard, to notify the persons owning or controlling such bridge to so alter the same as to render navigation through or under it reasonably free, easy, and unobstructed, stating in such notice the changes required to be made,

and prescribing in each case a reasonable time in which to make such changes, and if at the end of the time so specified the changes so required have not been made, the persons owning or controlling such bridge shall be deemed guilty of a violation of said sections; and all such alterations shall be made and all such obstructions shall be removed at the expense of the persons owning or operating said bridge. The persons owning or operating any such bridge shall maintain, at their own expense, such lights and other signals thereon as the Commandant of the Coast Guard shall prescribe. If the bridge shall be constructed with a draw, then the draw shall be opened promptly by the persons owning or operating such bridge upon reasonable signal for the passage of boats and other water craft.

Lake Charles Harbor and Terminal District Board of Commissioners

Resolution 2004-032

A RESOLUTION expressing support to maintain the current height and width characteristics of the I-10 for any new replacement bridge planned for future construction.

WHEREAS, the Louisiana Department of Transportation & Development is currently studying replacing the Calcasieu River I-10 bridge; and

WHEREAS, the District believes it is in the best interest of navigational interest and the general public that any new bridge maintain the height and width characteristics of the current bridge.

NOW, THEREFORE, BE IT RESOLVED BY THE BOARD OF COMMISSIONERS OF THE LAKE CHARLES HARBOR AND TERMINAL DISTRICT IN REGULAR SESSION CONVENED THAT:

SECTION 1: The Board of Commissioners of the Lake Charles Harbor & Terminal District does hereby express its support for maintaining, as to any new I-10 Calcasieu River bridge, the height and width characteristics of the current I-10 Calcasieu River bridge.

THUS PASSED AND ADOPTED at Lake Charles, Louisiana, on this 24th day of May, 2004.

FRED R. GODWIN, President

I HEREBY CERTIFY that the above and foregoing is a true and correct copy of a Resolution adopted by the Board of Commissioners of the Lake Charles Harbor & Terminal District in regular session convened on the 24th day of May, 2004.

MARSHALL J. SIMIEN, JR., Secretary/Treasurer Federal Maritime Law May Be Violated if MDOT Builds New Biloxi-Ocean Springs Bridge Without Drawspan by Keith Burton - GCN Filed 1/7/06

Updated 1/9/06 and 1/30/06

Since shortly after Hurricane Katrina, MDOT has said that it planned to rebuild the damaged Bay St. Louis and Biloxi-Ocean Springs bridges without a drawspan, which the former bridges had. But to do so will violate Federal Maritime Law.

Recently, both the Harrison County Development Commission and the Mississippi Development Authority went on record saying that MDOT's plan to build new bridges with nearly 100 feet of height and no drawspan would not be adequate.

Federal Maritime Law prohibits the building of bridges "...that shall at any time unreasonably obstruct the free navigation of the waters over which it is constructed..." The law further states that any impeding structure, if constructed, "...shall be removed at the expense of the persons owning or operating said bridge."

MDOT's proposed bridges at either end of Harrison County would restrict needed height requirements for shipbuilding in Harrison and Hancock Counties.

With the notice by the HCDC and the MDA, officials with MDOT are now aware that building bridges without drawspans would adversely effect the Coast's navigational requirements, triggering the federal law. As a result, it now appears likely that MDOT will have to reconsider its plans. Not to do so, could result in further delays in replacing the bridges and certainly impact the future of some key Coast industries, which are major employers that must have a clear access waterway. One example recently cited is that of Trinity Yachts in Gulfport. It is one of two shipyards bidding on a 300-foot mega-yacht that would require 110- to 120-feet clearance when it would be taken to open water by barge. It sees a future workforce of 700-750 people building larger yachts.

The fact that MDOT refuses to acknowledge that their bridge proposals do not meet the Coast's true needs now must be questioned by all public and governmental officials. MDOT's continuous lack of awareness can only impede the rebuilding of these bridges as it clear that a growing number of issues now cloud MDOT's plans.

In a time when Louisiana's transportation department has rebuilt the Katrina-damaged I-10 bridge over Lake Pontchartrain, MDOT's lack of performance over the Coast's two bridges is truly damming and already represents a major failure in the state's post-Katrina recovery effort. Even the best current estimates place the opening of MDOT's new bridges nearly two years away. At the current rate and in light of ever-increasing issues, this estimate is optimistic.

Coast residents and businesses, including the casino industry need to be alarmed at MDOT's progress and have reason now to question MDOT's public statements on its efforts.

Meanwhile, GCN has learned that the Harrison County Board of Supervisors will likely add their voice to request MDOT to add drawspans in their proposed Biloxi-Ocean Spring and Bay St. Louis bridges. In an interview with GCN on Jan. 9, District 2 Supervisor Larry Benefield said that the county must have drawspans and that the board initially was under the impression from MDOT that drawspans would be included.

"I can't imagine that we build a bridge without drawspans, "Benefield said. "I think you will see us make a decision on the drawspans."

Benefield, who is also the board's vice-president, said it is in the best interests of the county that the new bridges have drawspans to allow for future economic development of the county.

MORE INFORMATION

Federal Law on Bridges

Federal Law on Bridges (viewable with a browser)

Federal Law on Bridges over Waters (opens a .pdf file) Bridge Battles: Drawbridge Would Lengthen Project - Sun Herald

AMERICAN PRESS EDITORIAL

Jan 6, 2008 pE4

Advocates of lower bridge shortsighted

There has been plenty of talk in the last few months about the proposed height for a new Interstate 10 Calcasieu River Bridge.

We've heard from both sides about why they believe the bridge should be either 73 feet or 90 feet tall. Each gave good reasons for their position.

The state Department of Transportation and Development has recommended a 73-foot-tall bridge. The estimated cost for the new bridge will be about \$130 million.

A DOTD report states it would cost about \$15 million less to build than a 90-foot-tall bridge and be much safer for the 50,000 motorists that cross the bridge each day.

However, the nonprofit group Friend Ships and Lake Charles officials opposed that idea, saying a 73-foot bridge would prevent larger vessels from reaching the charity's facility and restrict development along the river north of the bridge

The Lake Charles City Council voted 5-2 on Nov. 21, 2007 to support a 90-foot bridge. This is what Mayor Randy Roach is supporting.

A few weeks later, the Calcasieu Parish Police Jury voted by a 8-6 vote to endorse a 73-foot-tall bridge.

The Westlake City Council and Sulphur Mayor Ron LeLeux both support the Police Jury's decision.

On Dec. 19 that the Metropolitan Planning Organization voted 4-1 in support of the shorter bridge.

Roach has said the lower bridge will keep large vessels from sailing north of the bridge.

This in turn will affect any possible economic development for the hundreds of acres of undeveloped property lying along the river here, he said.

We wholeheartedly agree with the mayor on this one.

Local officials can't foresee what will happen in the next 50 year. Building a lower bridge will have major repercussions down the line.

If it's built at the lower footage, then the land along the river north of it will be unusable as waterfront industrial property.

The deep-water section of this part of the river has so much potential. Public officials who support the lower bridge, which would effectively cut off potential development north of it, lack vision.

Lower-bridge proponents argue that the land north of the bridge hasn't been developed since the current I-10 bridge was built in 1952. Thank goodness this thinking didn't prevail after the U.S. Air Force abandoned Chennault Air Base in the early 1960s, leaving its 10,000-foot runway dormant for more than 25 years.

We understand that the higher bridge will cost more money, but in the long run it will turn out to be good investment for this area's economy.

The 90-foot bridge is the way to go.

Betty Bates, City Council West Lake Louisiana:

What I would like to ask all of you is please do the infrastructure from getting from interstate 10 to West Lake before you start the other large bypass. It's desperate over here in West Lake. We have line from Interstate 10 all the way to Sulphur, getting out of West Lake takes us 30-40 minutes (its line to line to line). It is terrible. We need help. And the bridge, of course, please do it as soon as possible. I have been traveling over it for 57 years and I have seen it go down, go down, go down and I am very frightened that one of these days it might collapse. Those 18 wheelers, you will have 10 and 12 on one side and maybe 10 and 12 on the other and its terrible. It's unsafe. Please tend to this.

Betty Bates, West Lake Louisiana City Council Person:

Looking over all the plans, we have decided that E is probably our best bet. The Enterprise Blvd exit is a very bad decision. If you are going to St Patrick's Hospital you would have to go all the way Enterprise Blvd come all the way go all the way across to St. Patrick's. If you come down off E, you gonna come down and come down and go to St Patrick's Hospital, you will be able to get off. Other- wise it's not, it's not a good decision. Enterprise Blvd is a disaster.



I-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End)
State Project No. H.003931

Public Meeting Comment Form

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- Recommended Reasonable Alternatives for further evaluation in the Environmental Impact Statement (EIS)

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- Preliminary Build Alternative 2, Sub-Alternatives A-E
- Preliminary Build Alternative 3, Sub-Alternatives A-E

Please return this completed form at the comment table or to a Project Team member.

You can also submit comments online at www.i10lakecharles.com or by U.S. mail to the following address:

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

Please Print	$\mathcal{I} \mathcal{A}_{r}$
Name:	on DRIVE
Address: <u>39/5</u>	13 AYOU WOOD IN. L.C. 70605
Email:	
Agency (if applicable):	
Would you like to rece	eive future updates on the project? Yes or No (circle one)
Comments:	y away from Contaminated
area 1	RT Tell 0251, !!!
1	
(Continued on Back)	

BA-BONVIllIAN 833 PARISH RD.



Please Print

I-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End)
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I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

Name: MAHIE DORE
Address: 11065. Perkins Fenny Rd-LC 10611
Email: Mhoz.34@ YAhoo.com
Agency (if applicable):
Would you like to receive future updates on the project? Yes or No (circle one)
Comments:
The need for a new I-10 bridge is imminerat in my opinion -
The grade oxefuture bridge Should be Studied as the grade of
the Current tridge has heen the Course of many accidents.
Under No Circumstances should the mistake he made, to
neg around the ESC Spill area - We Cannot make additions
(Continued on Back) aguifer for water (at least I don't think we can

and water in the # 1 se ason we are alive today.
If the height of our Current fridge was determined to
allow larger ships to go under The bridge of Ride
from the energy in themes of future wars as exemi
Ophips were sighted in the Helf during wwith
but I do think we Stoold think about this
Refore lowering the height of the bridge
3.



I-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End)
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I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

Please Print
Name: PAM CAPDEBOCCO
Address: 931 SHADY LANK WESTLAKE LA 70609
Email: pam fcgpdeboscg@gagiL, com
Agency (if applicable):
Would you like to receive future updates on the project? Yes or No (circle one)
Comments: AS A WESTLAKE RESIDENT & WOULD LIKE TO
THANK EVERYONE, INVOLVED IN THIS PROJECT
ENVIRONMENTAL CONCRENS ARE FIRST AND
FOREMOST. J HAVE GRANDCHILDREN THAT ARE
NOT YET BORN WHO WILL BE ALLECTED BY
(Continued on Back)

THE DECISIONS BEIND MADE BY YOU.
AS OF NOW I AM CEANING TOWARD
SUB- AUT C PBA OX 3 I WILL CONTINUE
TO BE INVOLVED IN THIS PRUCESS. I WOULD
LIKE DIRECT ACCESS TO BOTH EAST Y WEST
AND DOWNTOWN LAKE CHARLES (RYAN)
BECAUSE OF THE LOCATION OF COMMUNITY
EVENTS & RESTURANTS, WHEN ENTER TAINME.
SUCH AS MUSEUMSBLAYS CHURCH ETC IS
RASILY ACCESSABLE IT HELAS OUR DAILY
LIVES AND THE WHOLE SOUTHWEST
EXPERIECE OF THOSE WHO VISIT. THIS
INCREASES REVENUE TO PAY FOR SUCH A
PROTECT. PLEASE TAKE FLOODING ISSUES
ENVIRONMENTAL ISSUES SUCH AS THE
CONTAMINATION OF WATER AND AIR
CONTINUENTON OF WITHER THOS ITM.

From: <u>I-10 Lake Charles Bridge</u>

To: April English
Subject: 8/3/17 public forum

Date: Friday, August 04, 2017 11:21:38 AM

You received a message from michaeldcarleton@gmail.com

I did not attend the forum so my comments are based on the written materials provided to me. 1. The main reason for me commenting is to encourage the DOTD to eliminate from future consideration entirely PBA 4 and eliminate it from all future materials. I'm frankly disappointed the DOTD would even spend the time marking such a route on paper. This would be a ridiculous eyesore to build a new bridge through the heart of Lake Charles, not to mention all the impact on lake recreation and lakeside properties. I don't care if there's a nuclear waste dump underneath the current bridge this still makes no sense. Get the area cleaned up and move on. 2. I thought Conoco had already cleaned up some of the EDC waste. If the remainder needs to be cleared to satisfy EPA that should be a priority getting it done while further planning and funding for the bridge is in the works. Thank you. Mike Carleton

From: <u>I-10 Lake Charles Bridge</u>

To: April English
Subject: public comments

Date: Saturday, August 05, 2017 12:03:23 PM

You received a message from ajpcormier@gmail.com

first, I would like future updates on this project

My name and address are: Adley Cormier, 631 Sixth Street, Lake Charles, LA 70601. I serve on the City of Lake Charles Historic Preservation Commission.

I wished to reaffirm that the general right of way for work on this corridor is not expected to be wider/greater than the current boundaries so that expected work will not impact on the two principle adjacent historic sites of Corporation Cemetery (at Moss and Church) and the Cantonment Atkinson/Bilbo Cemetery site at the south bend of Lakeshore Drive. Should changes to design be contemplated, please advise of the possible impacts.

As to general comments, the widespan option rather than the compensated foundation seems to me to be the wiser move with the built elements chosen to avoid the known sites of contamination.

As to the Sampson street issue, the possibility of a moveable bridge to direct traffic to Enterprise Boulevard seems the most useful. I would urge that additional connections to Lakeshore Drive and Ryan Street would be great options as well. Frankly, a movable bridge at the site of the Old Spanish Trail bridge along with an alignment of Sampson (which would run with no connection to I-10 at this site) to Mike Hooks to Marine St to Nelson would be a useful component to moving traffic from West Calcasieu to East Calcasieu.



I-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End)
State Project No. H.003931

Public Meeting Comment Form

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Preliminary Alternatives

Please Print

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The Recommended Reasonable Alternatives are as follows:

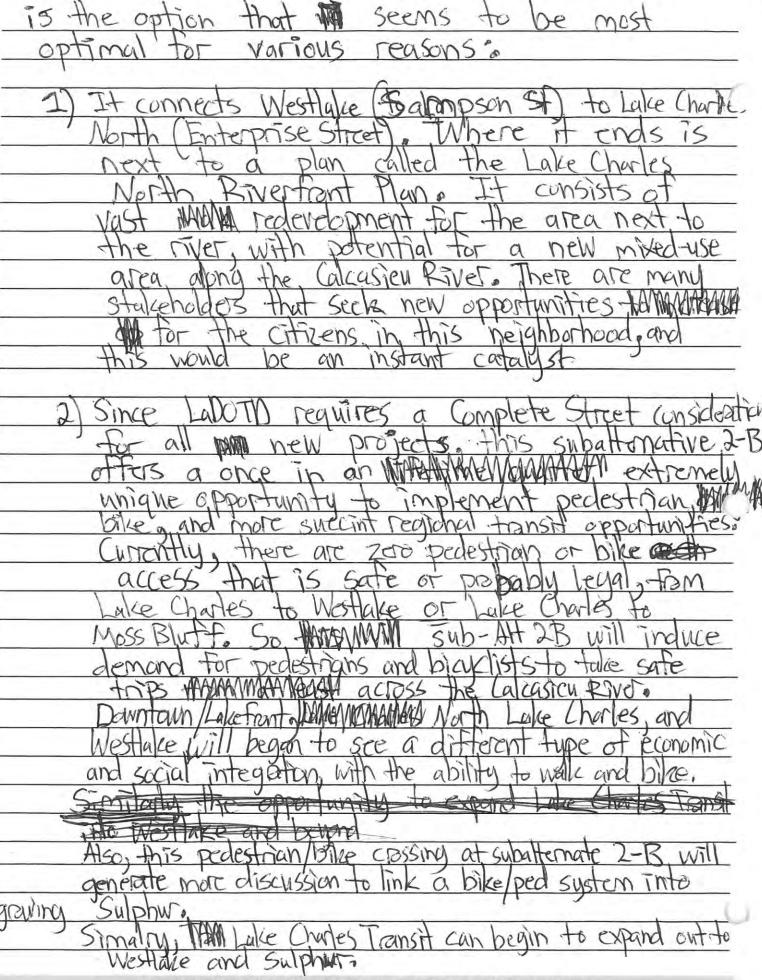
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- Preliminary Build Alternative 3, Sub-Alternatives A-E

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> I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

Name: Walter Council
Address:
Email: Waltera) in cal. la
Agency (if applicable):IMCAL
Would you like to receive future updates on the project? Yes or No (circle one)
Comments:
The sub-alternatives should detinitely be part
The sub-alternatives should definitely be part of one of the finalists, which it appears they
will ben
The subalternative 2-B, with the compensated foundation
(Continued on Back)



From: <u>I-10 Lake Charles Bridge</u>

To: April English
Subject: I-10 Bridge Calcasieu

Date: Thursday, August 03, 2017 5:12:48 PM

You received a message from craighcrawford@ygmail.com

I just saw some possible alternate plans for the I-10 bridge that propose that it cut straight across Lake Charles. Please do not do this unless your plan is to ruin the entire lake and beauty of the lake.

Figure B and Figure C of the plan are criminal and should treated as such. Please do not ruin our lake.



Please Print

I-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End)
State Project No. H.003931

Public Meeting Comment Form

Please provide your comments on the following items:

- Preliminary Alternatives
- · Alternatives Screening Methodology and Results
- Recommended Reasonable Alternatives for further evaluation in the Environmental Impact Statement (EIS)

The Recommended Reasonable Alternatives are as follows:

- Preliminary Build Alternative 2, Sub-Alternatives A-E
- Preliminary Build Alternative 3, Sub-Alternatives A-E

Please return this completed form at the comment table or to a Project Team member.

You can also submit comments online at www.i10lakecharles.com or by U.S. mail to the following address:

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

Name: raig (vacorord
Address: 317/ Wilson Street LC, LA 70601
Email: Craigh crawford @ gmail.com
Agency (if applicable):
Would you like to receive future updates on the project? (es or No (circle one)
Comments: Cutting the bridge Occross Lake Charles
Take would be devastating. It's an awful ide
Keep the bridge location in a similar area as it
Currently is and recreate some character
as the current bridge, Look at the Avidubor
(Continued on Back)

Dridge that cost \$409M for 2,900 cars/day. I-10 over Calcasien has 72,000 cars/day. We should have an architectually pleasant bridge that incorporates the unique crossed gun logo and stays in the same vicinity of the current pridge. Also reuse or sell the guardrail from the current
I-10 over Calcasien has 72,000 cars/day.
We should have an architectually pleasant
bridge that incorporates the unique
crossed gun 1090 and stays in the same
vicinity of the current pridge, Also
reuse or sell the guardrail from the current
bridge.
(



Please Print

I-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End)
State Project No. H.003931

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I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

Name: RPATRICK DIAMOND
Address: 5479 MOULIN ROUGE DIR, LAKE CHARLES, LA 70605
Email: r- Patrick diamond O remax, Net
Agency (if applicable):
Would you like to receive future updates on the project? (res) or No (circle one)
Comments: D Add at least 1 or 2 toll lawes to bridge to help financing D Use Signergion bridge option to span both ED Carea and Tailroad tracks on I-po and Sampson St.
@ Use Sispension bridge option to span both ED Carea
and railroad tracks on I-p and Sampson ST.
/Osinting and a Double
(Continued on Back)

I-10 Calcasieu River Bridge Project
C/O HNTB Corporation
2021 Lakeshore Drive, Suite 230
New Orleans, LA. 70122

To whom it may concern,

I was not able to attend the recent presentation given at the Lake Charles Civic Center on Thursday evening August 3rd reviewing the I-10 Calcasieu River new bridge options. However, as noted in the Friday edition of the Lake Charles American Press, I am writing this letter to send in my comments for consideration as I am an area resident that uses this bridge and surrounding roadways daily. First, let me state that I realize the challenges and constraints that this project presents with regards to the environmental issues, compressed time frame needed to address the current bridges condition, and overall cost limitations that need to be taken into account. All of this being said, there are no easy answers to the present bridge's dilemma or it would have already been done. However, I would like to present a few thoughts that may, or may not have, been considered. In either case, I believe they have a strong basis of justification (see below) that will not only address the bridge problem but also help provide an answer to the infrastructure needs for the growth in the area which is being stymied due to the volume of vehicle traffic. They are as follows:

Proposed Option

- First build a North loop of I-210 from west I-10 interchange around the backside (west) of Sasol and Nelson Power station up to Hwy. 171 north of Moss Bluff. Continue the North I-210 Loop from Hwy. 171 to the east I-10 interchange (NOTE: <u>Do not stop at Hwy. 171</u> as this will cause heavier traffic from I-10 north to Moss Bluff which is already overloaded at times). This will require both the West Fork and Calcasieu to be bridged but both these bridges should be much smaller scale than either I-10 or South I-210 loop.
- With both the North & South I-210 loops open to traffic flow, shutdown access to the I-10 bridge (temporary turnarounds will be needed just before the base of each side of the bridge since the current ones go under the bridge). Conduct demolition of the I-10 decking and structural steel leaving the concrete piers in place for future use (Note: Testing will need to be done to determine the concrete piers' condition but there are contractors & technologies available that can help revitalize the concrete if needed. This probably needs to be determined upfront now to see if this idea is viable)
- Build a lower profile I-10 bridge on the existing concrete piers (new structure design may allow some widening of the bridge but 4 lanes with some shouldering should be sufficient since overall traffic load should be lowered due to north I-210 loop access).

Benefits/Justification

- First, there would be 3 avenues of interstate roadway for east/west traffic to take when
 traversing the Calcasieu river instead of the current 2. This would help relieve clogged main
 arteries of travel as well as surrounding "backroads".
- Second, heavy industry traffic would be re-routed around the outside of Westlake greatly lowering the amount going through the town. This is not only true for the Sasol, Phillips 66, & Entergy traffic that could hit the North I-210 loop directly through feeder lanes but also for all area industry traffic heading north (I believe there would be strong support for this from industry). This will also minimize the problems the train causes when it goes through Westlake to a more acceptable level.
- Third, it will provide a means to remove the current I-10 bridge from use during refurbishment without greatly impeding traffic flow. This will also allow all existing approach ramps and roadways to I-10 bridge to be left as is thus lowering costs (except for temporary turnaround at base of bridge but this should be minimal).
- Fourth, this would provide a more efficient & effective means of egress for South Lake Charles in the case of hurricane evacuation.
- Fifth, and most important from a long term strategy, it will provide the area with an
 adequate infrastructure that will allow growth well into the future. It is true that in 3-7 years
 the traffic load due to out of state construction workers may wane but we will also have
 more people to operate and maintain the newly built facilities as well as the extra resources
 required to supply support services (i.e. contractors, material suppliers, retailers, food
 outlets, etc.).

We need the infrastructure with the extra capacity and not just a new bridge. Don't get me wrong, we direly need a new bridge but let's do it in an effective manner (short & long term) where we get both. I want to thank you in advance for considering my input and would be more than happy to discuss this further if you'd like. I may be contacted at the information below. Again, thank you for your consideration.

Sincerely,

Peter Fritzenschaft

1765 Deerfield Drive

Lake Charles, LA. 70611

Cell # (337)-263-1309



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I-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End)
State Project No. H.003931

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I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

Name: Angela Gibson
Address: 7786 Hwy 90E L.C., La 70615
Email: kalkirkaragnail, com
Agency (if applicable):NA
Would you like to receive future updates on the project? Yes or No (circle one)
Comments:
Anything that can be done to alleviate
the extra traffic that will go to IIO
during the 210 bridge project should be
done before the 210 bridge project
Starts, At this point, I don't care
(Continued on Back)

which option is ch new IID bridge, I something done.	iosen	for 4	ne
new IIO bridge, I	inst	want .	to see
something done	3		
serie Tring acres			
(

Richard Harbison Manager Lake Charles Refinery

PHILLIPS 66 2200 Old Spanish Trail Westlake, LA 70669



Via U.S. Mail and Email to AEnglish@HNTB.com

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive, Suite 230 New Orleans, LA 70122

Re: Written Comments for the Record of the August 3,

2017 Public Meeting

State Project No. H.003931 Calcasieu River Bridge

Project

To Whom It May Concern:

I am writing on behalf of Phillips 66 Company. Phillips 66 had several representatives at the August 3, 2017 public meeting for the I-10 Calcasieu River Bridge Project. Consistent with the information provided during that meeting, I ask that this letter be included in the official public meeting record.

Phillips 66 is providing these summary comments and the attached initial technical comments for consideration and response from DOTD. Phillips 66 also intends to provide more detailed comments and technical information before the end of the 45-day public comment period that is underway. Phillips 66's purpose in raising these concerns is to protect the integrity of the federal Environmental Impact Statement (EIS) process, to give the public and government agencies access to accurate and up-to-date scientific data, and to remedy critical deficiencies in the current EIS that bear directly on the proper scope of study and reasonable range of alternatives.

Summary of Concerns

As you know, Phillips 66 owns and operates facilities that will be directly affected by potential changes to the I-10 Calcasieu River Bridge. The company fully supports thoughtful improvements to the Calcasieu River crossing, which should greatly benefit the public. However, DOTD ceased cooperative and collaborative discussions with Phillips 66 about the project in 2012.

Unfortunately, the DOTD's current approach to the project EIS raises numerous serious questions about the NEPA review and planning process. DOTD is not following its own federally-mandated project coordination plan. DOTD has skipped an important Public Meeting that was supposed to be held before DOTD proceeded to evaluate various proposed preliminary alternatives. According to the draft Project Coordination Plan posted on the DOTD project website in October, 2013, DOTD said it would hold a public meeting (identified as Public Meeting #2) to accomplish three things: (1) to show the public and stakeholders the proposed preliminary alternatives it was considering; (2) to solicit input from the public and stakeholders on those proposed preliminary alternatives before further evaluation of them; and (3) to draft preliminary design evaluation criteria that would be used in the subsequent evaluation of the proposed preliminary alternatives. According to the draft Project Coordination Plan, DOTD was not going to choose or present recommended reasonable alternatives until Public Meeting #3. However, at the August 3, 2017 public meeting -- which was Public Meeting #2 -- the public was informed that DOTD had already defined the evaluation criteria, selected the proposed preliminary build alternatives (PBAs), screened the PBAs, and recommended the designs for the bridge and Sampson Street access that it will fully evaluate in the EIS.

The public and stakeholders, including Phillips 66, were not given any opportunity to review or provide comment on preliminary design alternatives, the PBAs developed by DOTD (which were substantially different than those presented in the past), or the evaluation criteria. DOTD is not following its draft Project Coordination Plan which, by its own terms, is a structured plan designed to "promote early and continuous involvement from stakeholders, agencies, and the public" in the environmental review process which is "invaluable in order to achieve informed consent and collaboration."

A few of the most troubling questions are discussed in more detail below, and are based on the limited information provided to the public during the August 3 meeting.

Timing and Process Questions

The EIS timeline presented during the August 3 public meeting misleadingly begins with an "Alternatives Screening and Development Process" in spring 2017. Under CEQ NEPA regulations, the EIS process must start with a notice of intent

to prepare an EIS and progresses first to a public scoping process. It appears that DOTD intends to continue the project's NEPA process with a limited opportunity for public comment following a notice of intent published four years ago in 2013. Inevitably, much of the information collected during the initial scoping process is now stale, and many circumstances have since changed. Moreover, public awareness of the ongoing NEPA process has faded given the amount of time that has passed. In order to fulfill all its legal obligations to public participation in this major project, DOTD should comply with the public scoping process.

When DOTD began its EIS process in 2013, it explained that there would be three public meetings—one that served as an initial scoping meeting, a second to "gather public input on the final project coordination plan, purpose and need statement and preliminary alternatives," and a third to "gather public input on the reasonable design alternatives." Setting aside the four year gap between the first public meeting and today, DOTD has inexplicably abandoned this process.

DOTD has abandoned its own draft plan and is not adhering to the descriptions of the public meetings that appear on its project website. The meeting held on August 3, 2017, was described in handouts as "Public Meeting #2." It is inexcusable to allow so much time to elapse between planned public outreach sessions and simply label the most recent meeting as "Public Meeting #2" in the false hope that interested stakeholders will remember the content and purpose of the initial scoping meeting in 2013. Moreover, DOTD apparently has not finalized the project coordination plan which was in draft form four years ago. The August 3 meeting's purpose does not appear to have been to "gather information about the final project coordination plan, purpose and need statement, and preliminary alternatives." Instead, the meeting handouts say that DOTD is gathering information on "preliminary alternatives," "alternatives screening methods," "screening results," and "reasonable alternatives recommended for further analysis" in the EIS.

This unreasonably elongated and muddled process does not serve the purposes of NEPA. DOTD should hold the public meetings it promised when the process was initially announced, or explain why it thinks a different process is appropriate.

Alternatives Questions

In May 2002, over 15 years ago, HNTB finalized what was called a "Comprehensive Preliminary Alternatives Report." DOTD's website is now

billing that report as a "Feasibility Study." No matter how this analysis is titled, relying on a report that old is not appropriate. It appears that the agency has relied on this outdated study to perform preliminary screening of feasible alternatives. Nothing presented at the public meeting explained how or if the 2002 report was reassessed to determine if those earlier recommendations are still valid a decade and a half later. At a minimum, DOTD should revisit and thoroughly reevaluate the May 2002 report before it makes crucial NEPA decisions concerning the feasibility of proposed alternatives. Further, that reevaluation process should be open and transparent to the public.

The principle of reevaluation of potentially stale analyses is engrained in FHWA's NEPA regulations. There, the guiding standard calls for agencies to reassess important project conclusions after three years of "major steps to advance the action." It is inconceivable that analysis so central to the EIS, the consideration of alternatives to be evaluated in detail, would not be reevaluated after a period five times as long as the accepted standard for such analysis.

The most dramatic change announced at the August 3 public meetings is the existence of "reasonable alternatives recommended for further analysis in the EIS." And while the purpose of that meeting purported to include public input on those alternatives, it is obvious that DOTD has already conducted a screening of the alternatives without input from the public and contrary to what DOTD said it would do in its 2013 draft Project Coordination Plan.

Confusingly, the handouts from the August 3 public meeting say that the preliminary alternatives for the project "will undergo a two-tiered screening process." The presentations at the meeting make it clear that this process *has already* occurred. In fact, DOTD has already selected—without any public input—two "recommended reasonable alternatives" for further review in the EIS. The only explanation for this decision is an arbitrarily color-coded matrix that claims to evaluate alternatives on the basis of a series of "screening objectives" that are significantly narrower than the statement of purpose and need. There is no way to tell, among other things, what methodology was followed in creating this matrix, where the information in the matrix came from, and who decided which effects qualified as "low," "medium," or "high." All of this information ought to have been made available to the public long before DOTD created a matrix or conducted its screening process.

The result of this secretive screening process is that DOTD has eliminated from further consideration the most cost-effective alternative, PBA 1-F, on the grounds that it "is a potential risk for downward migration of EDC towards [the] aquifer" – even though PBA 1-F meets the project's purpose and need criteria. Indeed, "potential risks" like these are precisely the sort of things that need thorough, scientific evaluation as part of an open and transparent EIS process. DOTD has not adequately explained how it reached this conclusion. The information presented at the August 3 meeting demonstrates DOTD did not rely upon accurate or up-to-date scientific data concerning the current EDC contamination when it chose to eliminate this alternative from further consideration.

Also significant is that many of the specific safety and congestion issues that were identified as a problem at Sampson Street in the draft 2013 Need and Purpose statement were removed from the Need and Purpose presented at the August 3 meeting, without any explanation. A particularly significant example is that the 2013 draft Need and Purpose emphasized the importance of proposed improvements to elevate the Sampson Street interchange facility over the UP and KCS railroad tracks to address long-standing public safety concerns and traffic congestion problems associated with the existing at-grade railroad crossings, and to improve response time during industry emergency evacuations. However, at the August 3 public meeting, a different Purpose and Need statement was presented, and the elevated Sampson Street interchange design, sub-alternative F, was not recommended to move forward for environmental evaluation in the EIS.

Thus, after it takes the other steps described above, DOTD should include PBA 1-F as a recommended reasonable alternative in the EIS.

Conclusion

The EIS process that DOTD is now continuing after a four-year hiatus does not meet federal regulatory standards. Both the "feasibility study" and the scoping process that form the foundation of the proposed EIS are outdated and inconsistent with the process described to the public four years ago. DOTD has skipped critically important steps that it said it would take in the October, 2013 Public Scoping meeting. DOTD has apparently relied on outdated and incorrect data to eliminate from consideration the most cost-effective alternative, PBA 1-F, and has done so in a secret process in which the public and stakeholders were not given the opportunity to provide meaningful input. The right thing for DOTD to do is to

hold a new public scoping meeting. Whatever EIS is prepared for the I-10 Calcasieu River Bridge needs to be prepared as part of an open and transparent process that includes all reasonable alternatives and includes a process by which objective and up-to-date scientific data can be presented and fairly considered.

Very truly yours,

fleth.

PHILLIPS 66 INITIAL TECHNICAL COMMENTS TO LDOTD ON INFORMATION PROVIDED AT PUBLIC MEETING #2 ON AUGUSTS 3, 2017

I-10 Lake Charles Calcasieu River Bridge State Project No. H.003931 Calcasieu River Bridge Project

Phillips 66 offers LDOTD these initial abbreviated comments to the information published at the public meeting for the Calcasieu Bridge Project on August 3, 2017. Phillips 66 has been working with the Louisiana Department of Environmental Quality (LDEQ) to address the impact of the 1994 EDC release since it was discovered. A groundwater recovery and treatment system has been in operation since shortly after the incident and has been highly successful at recovering EDC and cleaning up the area.

Since it was first made aware of the proposed bridge and interchange projects, Phillips 66 cooperated with LDEQ and LDOTD to ensure that the environmental impact of the 1994 incident is fully understood. However, in February of 2012, LDOTD advised that it would no longer hold Joint Task Force meetings with the remediation team, and ceased cooperative and collaborative discussions about the project. Phillips 66 is committed to being an active participant in the process and will continue these efforts to ensure that the most up-to-date information is available for the appropriate agencies and the public to evaluate.

1. LDOTD published incorrect EDC data

Outdated information was presented by LDOTD regarding the location of EDC contamination north of I-10 and construction risks in that area. Data collected by Phillips 66 over the last 1 ½ years indicates the EDC plume is no longer present north of I-10. Accordingly, the potential risk for downward migration of EDC towards the aquifer should not be a construction risk since EDC is no longer present above any regulatory screening standard.

Figures at Station 4 of the public meeting purport to show "constraints" in the bridge building footprint, including an EDC contamination "plume" north of the existing I-10 bridge. LDOTD did not provide any current monitoring data, environmental site assessments, or similar reports to support its depiction of this EDC plume. The data that the LDOTD used to depict a plume in the path of the proposed bridge is several years old. LDOTD is relying on out-of-date and inaccurate information that does not represent the true environmental site conditions in the proposed construction zone for the Calcasieu River Bridge under PBA 1.

Phillips 66 has installed 12 groundwater monitoring wells in the marsh area between the existing I-10 bridge and the Union Pacific Railroad tracks located north of the marsh, and collected quarterly samples since 2011. Through a combination of Phillips 66's recovery efforts south of I-10 and natural attenuation over the last 6 years, the EDC concentrations beneath the marsh area have decreased. In 6 years of groundwater monitoring in the marsh area, there has never been a detection of EDC above the cleanup standard established by the LDEQ, and since the spring of 2016, there has not been a detection of EDC above any regulatory screening standard. The data collected by Phillips 66 since 2011 has been shared with LDOTD, and the LDOTD has routinely collected their own samples in conjunction with Phillips 66. To our knowledge, LDOTD has never questioned the veracity of this data, or presented any data or facts to refute or contradict this information.

The "constraints" map shown by LDOTD at the public meeting inaccurately depicts an EDC contamination plume north of the existing I-10 bridge, where data from the last 1 ½ years show that the EDC is no longer present.

2. Phillips 66 objects to screening out design alternative PBA 1-F.

The pile foundation preliminary bridge alternative (PBA 1) and the Sampson Street Interchange (sub-alternative F) is a reasonable alternative design for replacement of the Calcasieu River Bridge and access to the Westlake Area, and should be recommended for evaluation in the EIS. It is the most cost-effective and technically feasible preliminary design, and it meets the purpose, need, and objectives of the project.

The stated reason for the decision to screen out PBA 1-F from further consideration was "Driving piles is a potential risk for downward migration of EDC towards aquifer." No information was provided to the public to support this statement. There is not a plume of EDC in the marsh area north of the existing I-10 bridge where the new bridge is proposed for construction. Should new EDC impacts be discovered at a later date, Phillips 66 would be responsible for any future cleanup costs.

Even under the environmental site conditions as of 2010, the LDEQ, the Louisiana agency tasked with protecting the ground waters of the state, did not prohibit construction in the EDC area. The LDEQ provided written guidance to LDOTD in 2010, that states it is acceptable for piles to be advanced into EDC impacted areas, provided certain depth limitations are not exceeded. These depth limitations are much deeper than the depth of pilings on the current I-10 bridge, and according to bridge experts consulted by Phillips 66, these depth limitations should not be an impediment to constructing a new bridge with a standard design. There is no credible basis for PBA 1-F being screened out at this stage of the design review.

Consistent with NEPA regulations, all of the data, studies, structural designs, or similar reports that DOTD relied on to establish screening objectives for and to evaluate the PBAs, including evaluation of Construction Risk in EDC Contamination Area" should be made available to the public.

3. Other construction options should be considered for the Sampson Street interchange sub-alternative F

At the public meeting, no information was provided as to whether alternative designs for the Sampson Street interchange design (sub-alternative F) were studied, including whether any mitigation options were considered, before screening out that design. The concept of compensated foundations to support the new I-10 main-lane bridge through the depicted EDC contamination zone appears to be a viable alternative, as recommended in PBA 2. It is likely that these foundations would also work for the Sampson Street interchange design (sub-alternative F). Also, short driven piles could be designed to avoid underground contamination for the Sampson Street interchange. Unlike compensated foundations, driven piles present little or no risk to workers as no material is removed from the ground.

Phillips 66 and the public were not shown the PBAs until the public meeting. Thus, no opportunity was provided to suggest or evaluate an alternative design for the proposed Sampson Street interchange (sub-alternative F) using a compensated foundation or short driven piles to avoid the EDC contamination. These other construction options could provide a more cost-effective interchange design that meets the purpose, need, and objectives of the project without introducing longer travel times for emergency vehicle access and the potential risks of a moveable bridge as with the sub-alternatives that are recommended to be carried forward in the EIS process.



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I-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End) State Project No. H.003931

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Comments on the project will be accepted for 45 days following this public meeting.

Name: Elizaboth M.S. Hursey
Address: 2202 Chulch St
Email:
Agency (if applicable):
Would you like to receive future updates on the project? Yes or No (circle one)
Comments: Would Like to Keep United the Program
and Know tither my finisher tands inthat
je passing almost outop of my home.
(Continued on Back)

To: April English

Subject: I-10 Calcasieu River Bridge Project

Date: Thursday, August 10, 2017 10:38:00 PM

You received a message from knapplk@bellsouth.net

I was unable to attend the public hearing in Lake Charles last week, but wish to be included for further notice and discussion regarding one of the most important decisions regarding I-10 in the upcoming future. First, I would suggest that the process as outlined needs to be streamlined and compressed. The bridge is now obsolete and needs replacement underway now. I understand the importance of the EIS and do not wish to undermine a thorough review, but suggest it needs to be done more expediently. Second, as part of the assessment, there needs to be a through discussion of the impact of the contamination now under the bridge and its impact on cost and problems which might result. Third, I would suggest a thorough look at an alternative location, north of the present site that might avoid the issue of the contamination, going through Moss Bluff.

Thank you for the opportunity to comment.

Leonard Knapp, Jr.; 3320 Country Club Drive, Lake Charles, LA 70605; phone: 337-304-9300

To: April English

Subject: I-10 Calcasieu River Bridge Project

Date: Thursday, August 10, 2017 10:38:00 PM

You received a message from knapplk@bellsouth.net

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Thank you for the opportunity to comment.

Leonard Knapp, Jr.; 3320 Country Club Drive, Lake Charles, LA 70605; phone: 337-304-9300

To: April English

Subject: Feedback from the LC Yacht Club

Date: Wednesday, August 09, 2017 10:28:47 AM

You received a message from pg245091@hotmail.com

I write on behalf of the board of directors of the Lake Charles Yacht Club (at the foot of the bridge). We welcome the renovation / replacement of the I10 bridge, which is long overdue. However, we object to the proposed route that goes across the middle of the lake: it would be an eyesore as well as a navigation hazard for small boats on the lake.



(I-10/I-210 West End to I-10/I-210 East End)
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Comments on the project will be accepted for 45 days following this public meeting.

riease Filit
Name: Bandy Leger
Address: (Louisiana Concrete / 401 N. Kirkman, LC
Email: rlege-@louisianacoxcrete: Com
Agency (if applicable):
Would you like to receive future updates on the project? Yes or No (circle one)
Comments:
A JB 1+ 15 Serious
for the impact of our business to keep
I-10 Bridge open whilefuntil new bridge
is built kompleted, Our business is on
east side, and is important to be able
(Continued on Back) to deliver our product on west side
of bridge ina timely manner.
For additional information, please visit the project website at www.i10lakecharles.com Page 72



(I-10/I-210 West End to I-10/I-210 East End)
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Please Print
Name: Denjamin Magarlon
Address: 4310 Ryan St LAKE CHARLES, LA 70605
Email: ben () im cal, la
Agency (if applicable): TMCAL
Would you like to receive future updates on the project? Yes or No (circle one)
Comments: PBAS 243 seem to have the most desired
belonce of metigating impacts, while PBAZ is
less costly than PBA 3, The potential for Addled
benefits to travel 4 tourism with a long-span could
be one wax to show long term of sets to the difference in

	10st between PBA 2 4 3,
	For the north South connectivity of surface level
	opportunity to be readdressed,
	The MPC 3 converted works
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(I-10/I-210 West End to I-10/I-210 East End)
State Project No. H.003931

Public Meeting Comment Form

Please provide your comments on the following items:

- Preliminary Alternatives
- · Alternatives Screening Methodology and Results
- Recommended Reasonable Alternatives for further evaluation in the Environmental Impact Statement (EIS)

The Recommended Reasonable Alternatives are as follows:

- Preliminary Build Alternative 2, Sub-Alternatives A-E
- Preliminary Build Alternative 3, Sub-Alternatives A-E

Please return this completed form at the comment table or to a Project Team member.

You can also submit comments online at www.i10lakecharles.com or by U.S. mail to the following address:

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

Please Print
Please Print Name: ERILIMANSE (
Address:
Email: +EXJUNS@ SMAIL. (M
Agency (if applicable):
Would you like to receive future updates on the project? Yes or No (circle one)
Comments: Looking bornard tothis jos being done!
(Continued on Back)



(I-10/I-210 West End to I-10/I-210 East End)
State Project No. H.003931

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I-10 Calcasieu Rîver Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

Please Print
Name: JOHA MARCOAL 70605
Name: JOHH MARCON 70605 Address: 512 ORCHARD DR-LAKE CHARLES
EMBIT: PHONE 337-478-0646
Agency (if applicable): RETIRED OLIN PROJ. 176R-
Would you like to receive future updates on the project? (Vestor No (circle one)
Comments: SEE ATTACHED.
(Continued on Back)

THURSDAY

JOHN MARGON-RETIRED ENG. PROJ. MGR. OLIN. 478-0646

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THE OPTION OF BRIDGE THRU OLIN SITE GOES THRU SOME UERY DIFFICULT TERRAINS#\$\$ Page 78

To: April English
Subject: Public Hearing

Date: Friday, August 04, 2017 7:41:02 PM

You received a message from marcmcdonald81@bellsouth.net

Will this graphics and information presented at the Aug 3 public meeting be posted on this website or elsewhere?

To: April English
Subject: Project impacts

Date: Thursday, July 27, 2017 11:25:58 AM

You received a message from brittneypoppell@gmail.com

Good afternoon,

Can you tell me if the Calcasieu River bridge project will require right of way acquisition?



(I-10/I-210 West End to I-10/I-210 East End)
State Project No. H.003931

Public Meeting Comment Form

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I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

Please Print
Name: 141 A.J. POWELL
Address: 500B N. HUNTINGTON ST. SUCHEIR, LA 70663
Email: apowell@ sulphur. org
Agency (if applicable): SurPHUR POLICE DEPT
Would you like to receive future updates on the project? Yes or No (circle one) Comments:
(Continued on Back)

To: April English

Subject: I10 Calcasieu Bridge Meeting

Date: Friday, August 04, 2017 10:20:12 AM

You received a message from patrickreilly015@gmail.com

Is it possible to receive and review the slide presentation shown at the August 3rd meeting? I was not able to make the meeting because of work, but am very interested in this topic.

To: April English
Subject: I-10 Bridge

Date: Saturday, August 05, 2017 6:48:49 AM

You received a message from cajunwelding@gmail.com

- 1. This bridge is part of an interstate system that is regulated, in part by the federal gov. and should be be either built with federal funds or at least with grants for most of the project.
- 2. 2-10 bridge supports south Lake Charles Area, another bridge should be built where the I-10 bridge is, on I-10, to keep main flow of traffic on interstate system that is only passing through as well as possibly keeping funding Fed. It also serves as alternate route when south lake charles and 2-10 ro too congested.
- 3. The new bridge has to be no less then 3 lanes per side rather then 2 due to the ever growing traffic. I know it will cost more but lake charles has long out grown its 2 lane system and will continue to become worse. Beaumont, Tx made the mistake of building a new or rebuilt 2 lane bridge and it only bottlenecks the traffic in both directions. Lake Charles needs to be smart and correct the danger of congestion while the chance is here.
- 4. Lake Charles could have built a new bridge for 1/2 the cost a few short years ago, now we will pay premium steel, concrete, and labor cost due to the many projects in the area so cost should not even be spoken. We knew we had to upgrade or rebuild a bridge that was falling apart at an alarming rate, and we waited. Now we will have to bite the bullet, pull up our boots, and pay over prices. The cost will only rise over time as they have since Lake Charles was founded almost 150 years ago. Just get it done and ask President Trump for some of that infrastructure funding he promised.
- 5. Don't over plan and spend a fortune on planners and pictures of a potential project, enough over priced project plans and studies on cost for a new bridge have been done in the past. Use one of the many studies and planning board reports that have already been done for this project. we have spent millions of dollars on at least 2 or 3 studies only to say we can't afford it. What a waste of tax dollars!!!

Jeff Robinson

A Community Voice

2221 St. Claude Avenue New Orleans, LA 70117 800-239-7379; 504-617-6215 fax; Lake Charles: 2913 Caroline Street Lake Charles, LA 70601 (337) 707-1517 www.acommunityvoice.org; info@acommunityvoice.com

August 2nd, 2017

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive, Suit 230 New Orleans, LA 70122

Re: I-10 Calcasieu River Bridge Project

To Whom it May Concern,

A Community Voice represents over 9000 low to moderate income families in Louisiana, many of whom reside throughout Imperial Calcasieu Parish. ACV is very much in favor of this bridge replacement project for the following reasons:

- The I-10 Calcasieu River Bridge is one of the most unsafe in the country, with a sufficiency rating of 9.9 out of 100.
- The bridge was never intended to be an Interstate bridge and cannot handle the current traffic load.
- Ongoing repairs to overlay, lighting and guardrails have put on extra weight on an already substandard structure.
- Bottleneck traffic congestion from three lanes dropping into two lines has caused multiple traffic accidents, exponentially impacted by the lack of shoulders.

While we are in favor of total bridge replacement, ACV requests that the following considerations be implemented in regards to this project:

- Sufficient and adequate alternate routes with traffic controls during bridge down time, especially during hurricane season to ensure the safety of all commuters.
- Due to the 1994 ethylene dichloride (EDC) pipeline spill, the construction area is contaminated with hazardous waste. As a result, the upmost safety precautions must be put in place with safe work practices employed and adequately trained workers. ACV hopes that these measures will be put in place prior and during all parts of the construction phrase to ensure the safety of the workers and residents. Constant monitoring practices must be established.
- Implementation of Lead Safe Work Practices for the removal and disposal of the existing bridge, as most bridges built around the time of the Calcasieu River Bridge contain lead based paint.
- Utilization of local Disadvantaged Business Enterprise (DBE) contractors and local residents for jobs, as
 the majority of large scale construction projects across Louisiana employee out of state contractors that
 fail to return investment into the communities in which they work.

Sincerely.

Lanny Roy President

A Community Voice



Please Print

I-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End)
State Project No. H.003931

Public Meeting Comment Form

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You can also submit comments online at www.i10lakecharles.com or by U.S. mail to the following address:

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

Name: ARLY SEARCY				
Address: 4518 SHERYL LANE LC L	A 70605			
Email: Carybook @ grail.com				
Agency (if applicable):				
Would you like to receive future updates on the project? Yes or No (circle one)				
Comments: Dotentral				
I'm concerned agot the impacts	to drilling H20.			
The option that avoids the ways	1 Contavillents			
is probably very costly ~ I want to	know more about			
the different Truss syltems to	teep from during			
bein the aquifer.	0			
(Continued on Back)				

From: Spain, Mike P.
To: April English

Subject: I-10 Calcasieu River Bridge Project

Date: Tuesday, August 01, 2017 9:29:52 AM

Attachments: <u>image001.png</u>

Good Morning,

Is there an engineers estimate on the construction cost/budget for this project?? We are trying to get an idea of the estimated size of the project. Hope all is well and greatly appreciate the help. If it's easier to talk on the phone, please don't hesitate to give me a call on my cell 407-367-9497. Thanks.

Mike



Mike Spain

Business Development Manager T 407-331-3100 Ext. 50117 M 407-367-9497 mpspain@laneconstruct.com

The Lane Construction Corporation 2601 Maitland Center Parkway Maitland, FL 32751

www.laneconstruct.com

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(I-10/I-210 West End to I-10/I-210 East End)
State Project No. H.003931

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> I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

Please Print
Name: Churles Stewart
Address: 1509 Hivin St, LC 70601
Email: charles Estewarta gmail.com
Agency (if applicable):
Would you like to receive future updates on the project? Yes or No (circle one)
comments: Without knowing the cost my in Itial recommendation is
either PBAZ or PBA3. WOULL consider BBAI of
the FRE contamilation over could be cleaned or is olated
Suh-Alts: Either Sub-Alt A, C, Or E, I am grainst
and of the options that extend to Enterine (Boil). It
(Continued on Back) the EDC area could be contained then Finall be a viable option.

August 10, 2017

Dear Bridge Builders,

As a long time resident of Westlake, La., I too am concerned with the traffic on the I-10 bridge. There is too much traffic for the structure. It is too difficult to get on the bridge at certain times of the day. There are several accidents each day on the bridge due to people in a hurry, inattention, vehicle problems, etc. and there are more big trucks on the road sometimes 5 or 6 in a row. Bad weather makes the probability of a wreck worse.

I don't know which plan will be accepted, but I do know that the current bridge will <u>NOT</u> last another 10 years. This bridge was built before there was an interstate through here. At that time, it served the community very well. Now in order to serve the community, there must be another bridge built.

Once a design is selected, prefab as much as possible to make the job go faster. It will be a big job no matter how it is done, but the more that can be gathered and assembled a head of time, the smoother the job will go. Hopefully, this will also cut down on the time needed to complete the project. However, we can not predict Mother Nature and her corporation will be needed.

I know that governments work slowly, but in this case, I think they better get together and get something done. The design must include future expansion of the area. You might say we out grew our bridges. No one could have guessed the amount of expansion we have had here recently.

I am sorry I missed the meeting in Lake Charles last week. I just wanted you to know how I feel about the bridge project.

Sincerely,

Mary Still P.O. Box 25

Westlake, La. 70669 (337) 582-1675

PS. GOOD LUCK!!

To: April English

Subject: Friend Ships Comments on I-10 Calcasieu River Bridge

Date: Saturday, August 12, 2017 9:19:19 PM

You received a message from dstipton@aol.com

FRIEND SHIPS AREAS OF CONCERN

Re: navigational needs for the Calcasieu River north of I-10

- To lower the bridge will be to permanently destroy the potential maritime economic development for our community and drastically reduce property values for all the land owners. Ports and the maritime industry have an annual \$33 billion impact to the State of Louisiana's economy, approximately 23% of the gross state product. Ports and the maritime industry have an impact of \$5.7 billion in the job market by supporting 270,000 jobs directly and indirectly. This is one in every eight jobs in the State. In 2004, the Lake Charles Harbor and Terminal District passed a resolution to keep the bridge at its current height. For every dollar that comes into Lake Charles, 46% is related to the Calcasieu River Channel and it is forecast that within 10 years, the area's maritime traffic will double.
- The Army Corp of Engineers has proposed the North Lake Charles Riverfront Parkway and Redevelopment Plan that includes several marinas. Lowering the bridge will severely limit the size of vessels that could utilize such marinas.
- The American Press published an editorial in 2008 that details reasons that the advocates of a lower bridge are shortsighted.
- Friend Ships has eight vessels that currently transit the Calcasieu River.
- Friend Ships provides a completely unique product and is a key resource to this region-including large scale disaster relief with hot meal service, commodity distribution, medical services and house to house assistance for the elderly and others in need. Our operations are one of kind. The impact to Friend Ships would be devastating when we are precluded from transiting the waterway if the proposed lowering of the bridge is enacted. It would virtually shut down our current operations, prohibit future growth and eliminate our ability to expand.
- After World War II the river banks in North Lake Charles housed hundreds of ships returning from the war.
- Prior to the current bridge being constructed, vehicles were stopped at least 435 times a month for the Willow Drive Bridge to open and allow marine traffic to travel on the Calcasieu River, thus it was determined by the Coast Guard that the height of the current bridge should be 135'. Later, a railroad bridge was built in close vicinity to the current railroad bridge that made navigation difficult and brought marine traffic to North Lake Charles to a minimum. This 2nd bridge was destroyed and dismantled in the 1970's. Since that time, it seemed to have been forgotten that this deep water channel was available and significant ship traffic did not return for some time but the tremendous potential of this area still exists as a most valuable asset to the community. In 2003, Friend Ships was made aware of this amazing deep water facility by a seismic company that was considering relocating their operation to North Lake Charles. Shortly thereafter, discussions of lowering the bridge became public and this, of course, would serve to discourage any maritime operation from relocating north of the I-10 Bridge.
- It is illegal to unreasonably obstruct the free navigation of the waters over which it is constructed according to 33 US Code 494. The Calcasieu River north of the I-10 Bridge is a remarkable, natural deep water channel that doesn't require costly dredging. According to the Coast Guard regulations and settled law, they cannot allow a structure to be built over navigable waters of the United States that does not provide for the reasonable needs of current and foreseeable future navigation.
- Our area is a natural safe harbor from storms, a very important safety benefit in our hurricane prone region.
- As a support to our humanitarian work, Park West Children's Fund/ Friend Ships is now authorized by the

Coast Guard as a TWIC security dock for the moorage of US and foreign vessels. We currently operate a productive business here at Port Mercy providing moorage for ships that supports our operations. The size of our own ships is only one part of the equation now in our interest in seeing the bridge stay high because we host ships of many different sizes and heights.

• We would like to stress that our long-term future, as well as that of all the land owners who will be affected by the potential lowering of the I-10 Bridge, cannot be adequately predicted. Throughout the 30 year history of Friend Ships, we have averaged one new vessel every 2.5 years. Since these vessels are provided to us through donation and as the humanitarian need arises, we have no way to know the length, depth or air draft of such vessels in advance.

RESTORE

P.O. BOX 233 LONGVILLE, LA 70652 (337)-725-3690 michaeltritico@yahoo.com

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive, Suite 230 New Orleans, LA 70122 August 10, 2017

Dear HNTB,

Please accept and place these comments into the public record of the August 3, 2017 Public Meeting on the I-10 Calcasieu River Bridge Project. I am submitting these comments as a supplement to my 10/25/2013 comments, which, although apparently completely ignored by HNTB, did express RESTORE's perspective on the bridge situation, a perspective which has not changed. RESTORE is especially disappointed that an alternative we suggested in the 2013 comments was not mentioned in any of the HNTB 2017 materials. For that reason I have sent in a separate envelope a copy of those 2013 comments and requested, as I do again now, that both those comments and these new ones be made part of the public record of the 2017 meeting.

The missing alternative, which should be at least presented to the public for consideration, (as was the "no build" alternative), is a complete relocation of the I-10 corridor to the north of the present latitude. If the corridor were up between Moss Bluff and Gillis or even on high ground between Gillis and Ragley, there would not have to be accommodation of ships nor would there be an issue with soils that have been turned to mush by EDC. We hope that you will add that alternative into the Environmental Impact Study and Statement so that there can be at least a fair consideration of what seems to us to be a logical solution to several problems at once. As for the opposition to that alternative by people who fear a loss of impulsive drivers getting off the Interstate and spending money in the city of Lake Charles, I addressed that issue in the 2013 comments. That issue should be a minor concern given the best interests of the public.

Now, let me go on to the 2017 materials:

Since RESTORE believes that the only sensible solution to the I-10 Bridge situation is a bypass north of Lake Charles at a latitude the would not require a massive bridge, I am reluctant to comment on any of the HNTB/DOTD alternatives. However, there is an error in PBA 4. It talks of two new bridge crossings over Contraband Bayou. The two crossings would actually be on the Clooney Island Loop of the Calcasieu River and not Contraband Bayou. (I would guess that the error arose from a matching error on Google maps.) That error should be corrected before including it in the EIS.

Still looking at PBA 4, that alternative is asinine. Maybe it is included simply as a way to have an alternative to reject, somehow showing that DOTD/HNTB did a lot of consideration of all alternatives. Regardless of the reason that it is included, surely the ruination of the aesthetic

magnificence of the lake by sticking an eyesore of concrete and steel all the way through and across it, runs counter to common sense. In addition to ruining a perfectly good-looking lake, the bridge would be a major new hazard to pleasure boaters, especially at night. Also, there are ships that would have to cross under the bridge at Clooney Island Loop and also to get to the building materials dock on the west shore of Lake Charles. Alternative PBA 4 surely did not warrant being floated, especially if the northern bypass alternative could not be included.

At some point during past meetings RESTORE asked why there could not be alternatives including a low level moveable bridge. We were told that the rules for the Interstate specifically prohibit drawbridges or turnstile bridges (and tunnels.) What has changed?

Although it may be true that the EDC plume has not yet reached Sulphur Avenue, as we pointed out in our 2013 comments, the Westlake municipal water wells just north of the railroad tracks seem to be drawing the EDC upgradient toward themselves. Losing the southern waterworks to EDC contamination will be a real setback for Westlake, but if the EDC then also destroys the soil beneath the new alternatives Sub Alts A-E and PBAs 2 and 3, all of this planning would look pretty foolish in hindsight.

It would seem obvious that there should be immediately a test well drilled somewhere north of the last set of monitoring wells which showed the presence of EDC. All pervious zones should be sampled for all chlorinated hydrocarbons, (not just EDC because we have seen in other areas where subsurface biodegradation can convert some chlorinated hydrocarbons to even more dangerous molecules such as vinyl chloride) as the well is being drilled. The sampling should learn from our experiences at the hazardous waste sites of Willow Springs and Carlyss that NAPL layers can exist in the ceilings and floors of pervious zones therefore sampling of each major pervious zone should occur at 3 levels, ceiling, mid-depth, and floor.

Also from our experiences at the two hazardous waste sites mentioned above, it should be acknowledged that the studies there by Kirk Brown and by Capazolli confirm that EDC causes a collapse of the crystalline structure of local clays. That cannot be ignored since the plume is migrating. It would be best to plan for future problems rather than planning to put alternatives into the places where such problems will reach eventually.

Another factor in the movement of the EDC is the tidal pump mechanism that USGS has confirmed in monitor wells near the Calcasieu River. Even as deep as the Chicot Aquifer the water levels in the nearshore wells rise and fall with the tidal pulses in the river. This constant movement of fluid is likely not being considered in any projections of arrival time of the EDC at the Westlake municipal wells but it should be factored into the projections especially before going to the trouble of building any of the alternatives that involve Sulphur Avenue.

Yet another factor that should be taken into account is the influence of the frequent vibrations sent down from the railroad through the layers of clay and sand in the project area. As the soil continues to degrade this factor will become more and more important. As I recall, in India many years ago, the rhythmic pulses of a train caused a sudden catastrophic collapse of the substrate and the train, tracks, and people were suddenly sinking in quicksand. I do not know how similar are the conditions here to the ones in India, but the more dynamic the conditions

here, the sooner more negative developments will surface. Those can and should be anticipated and taken into consideration alternative by alternative.

The failure of the 2017 presentation to have any diagrams showing the vertical and horizontal extent of the EDC contamination was, to me, inexplicable and inexcusable. Since that contamination is central to the planning of alternatives, how could it be left out of the public eye? Please do not keep holding back that information. It MUST be in the EIS and surely there have been some sampling and analyses later than the 2009 set. There should be annual sampling at least and the results should be made available online each time the sampling occurs, with notifications to all people who registered at the 2017 meeting.

Maybe someday there will be something actually happening in the field, such as a dismantling of the I-10 Bridge before it suffers more of those worrisome cracked I-beams as I saw in one of your poster photographs (certainly a fracture beyond what corrosion alone would have caused but more likely an evidence of severe stress caused by shifting substrate.) It would be better to do a planned removal of the bridge before a cataclysmic collapse, whether or not a replacement bridge is ready.

Thank you again for holding these meetings every few years and giving us the opportunities to comment.

Sincerely,

Michael Tritico, Biologist and President of RESTORE

Restore Explicit Symmetry To Our Ravaged Earth

RESTORE

P.O. BOX 233 LONGVILLE, LA 70652 (337)-725-3690 michaeltritico@yahoo.com

August 10, 2017

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive, Suite 230 New Orleans, LA 70122

Dear HNTB,

Thank you for holding the latest public meeting on the I-10 Bridge situation last week in Lake Charles and thank you for providing another opportunity to submit comments.

I attended the meeting. There was much good information there. However, I was disappointed that comments I submitted after the last meeting, 10/24/2013, were apparently ignored. Therefore I am enclosing a copy of those comments, dated 10/25/2013, in the hope that you will this time at least add to your list of alternatives the one that I have been proposing for years, even before the 2013 meeting. I realize that my suggested alternative, a total relocation of the I-10 corridor northward to a latitude that would make it easier to cross the Calcasieu River and on soil that has not been degraded by EDC, has likely been opposed by people who worry that a north bypass will impede visitation of Lake Charles by travelers. I addressed that concern in my 2013 comments. I do think that ALL alternatives should be at least presented for consideration.

I have re-read my 2013 comments and I would not change a single word in them. They cover the same issues that I saw in your 2017 HNTB presentation. Accordingly, I am hereby requesting that you include my 2013 comments in the public record of the 2017 meeting.

Later today I will have ready a fresh set of comments that apply more specifically to the materials you had at the Lake Charles Civic Center for the 2017 meeting. I will mail those in a separate envelope but I do hope that they, along with the 2013 comments and this cover letter will be acknowledged and made part of the public record on the I-10 Bridge situation.

Thank you again for providing these opportunities for public participation.

Sincerely,

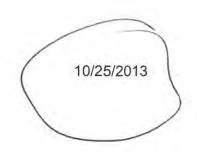
Michael Tritico, Biologist and President of RESTORE

Restore Explicit Symmetry To Our Ravaged Earth

RESTORE

P.O. BOX 233 LONGVILLE, LA 70652

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Road Suite 640 Baton Rouge, LA 70810



Dear sirs:

I attended the Public Meeting held last night at the Lake Charles Civic Center. The materials presented were very informative. The people who were there to answer questions about the exhibits were quite well-informed and very helpful. Thank you all for giving the people of this area that opportunity to get up-to-date on the bridge and highway plans.

I needed to get back home so I did not fill out the Comment Form while I was at the Civic Center, and I have many more comments than could fit onto the form so I am instead sending you this letter. I will first address the items for which the form specifically requests comments:

"Project Purpose & Need" There is not doubt that traffic congestion, safety, and bridge design issues need to be carefully studied and changes made so that existing problems can be removed and long-term options will be increased.

"Project Coordination Plan" Last night's meeting was, in a way, discouraging, since some of us have attended such meetings for years yet always see the situation stuck at roughly the same place, planning, discussion, changes of plans, more discussions... No practical change ever happens in the real world out there on the highway or bridge. I saw an estimated time of 3 years on one poster, for some sequence of events, and my first reaction was to just add a zero to the 3 since 30 years seems to be more realistic if we continue at the existing pace.

However, the ideas of, "the ability to provide feedback/submit questions on the project website at any time throughout the EIS process," quarterly project newsletters, and the other things that seem to be enhancements of the public's opportunities for participation are welcome improvements. Certainly the stagnation that seems to have become characteristic of the bridge and its corridor projects could hardly be any worse than it is already, but I would hope that the extra mechanisms for public participation do not somehow contribute to the negative inertia but might instead be managed as accelerators.

I would like to receive future updates on the I-10 Calcasieu River Bridge Project. Although I could receive short notifications at my e-mail address, <u>michaeltritico@yahoo.com</u>, I would like the newsletter or any lengthy correspondence to come to me by real mail.

"Other comments, questions, or concerns" I will now begin with those things.

Looking at the situation holistically it seems obvious that the existing bridge must be dismantled and that there is no way to build a safe and sufficient bridge in the "existing right of way" or anywhere near that location (because the substrate has been severely damaged by EDC contamination. I will discuss that in more detail shortly.)

Since there is no practical way to keep the river crossing where it now exists, the logical thing to do is to re-route Interstate 10 to high and solid ground north of the present location.

In one of the previous meetings I suggested a corridor up at the latitude of Joe Miller Road. There are other latitudes that could also provide locations for crossing the main fork and the West Fork of the Calcasieu River with no need for a bridge with ship height clearance and with less potential impact on wetlands. There is one corridor between Moss Bluff and Gillis now occupied by high voltage power lines. Maybe that one could become a dual use right of way. If not, going farther north to get out of the highly-developed north Moss Bluff zone, a corridor between Gillis and Ragley should have land less expensive to acquire than having to expropriate the new and planned subdivisions. Another advantage to going up to the South Ragley area would be that the new Interstate would then be out of the area shown by the National Hurricane Center's SLOSH model to be vulnerable to tropical storm surges.

What though, of the concern that a north route would deprive the City of Lake Charles of impulsive tourist stops or other commerce? Look at the French Quarter of New Orleans, or Fisherman's Wharf in San Francisco, or many other famous tourist stops nationwide. Most of those are not beneath Interstate highways. People who want to go to some attraction figure out what exit to take and they do it. Proper publicity would offset any tendency of lazy drivers to bypass anything worth seeing. As it stands right now downtown Lake Charles and the north shore of the Lake do not seem to be strongly magnetic for passing out-of-staters. A sensible planning effort on the parts of people developing new attractions combined with sensible highway planning could synergistically turbocharge Lake Charles as a family-oriented travel destination. The ongoing uncertainties about traffic could be removed finally. Having to exit the Interstate and come south a few miles to get to something special would not be any problem for anyone truly interested in a good experience. When I was a Ranger-Naturalist in Yosemite Park, all the park visitors had driven over 75 miles from the nearest Interstate Highway. They will come if it is worth visiting.

Another concern I have heard expressed repeatedly through the years, the impediments to navigation presented by low bridges at the latitude of Lake Charles, would also be alleviated by moving the I-10 Corridor north. The ships that do need to move under the existing bridge are few and do not often make the trip, but they are important ships, especially the Friendships that do very critical humanitarian work in times of disaster. Certainly it would not be right, even if it could be accomplished, to put in a low-level bridge unless it were a drawbridge. Opening a drawbridge for 20 minutes once or twice a year, at times of low road traffic, such as at 3 A.M., would be a small price to pay for Americans to be able to help fellow Americans in some disaster zone or people in other countries who might be suffering in some kind of crisis. Traffic tie-ups of far longer duration happen every week on the existing bridge.

(I have to wonder how long it will be before the railroad right-of-way also has to be moved north because of loss of load-bearing capacity beneath the tracks. We have already seen what the chlorinated hydrocarbons can do to track support in Fisherville. It is likely just a matter of time until a train wreck occurs in eastern Westlake adjacent to the I-10 Bridge. Maybe the railroad and highway relocation planning efforts can be combined in order to increase the senses of urgency and stakeholder cooperation.)

Let me go more into detail about my firm conviction that the soil beneath and parallel to the existing I-10 Bridge has been severely damaged and can no longer safely support that bridge or any new bridge that might be contemplated.

For a couple of years I have had public records requests in to the State and

Federal Highway agencies. I asked for the inspection reports for the bridge, the chemical analyses for soils there, and documents that might have included discussions about the possible impacts of the EDC leak upon conditions at the bridge. My concern has been that the bridge has become increasingly-unstable not just because of its age but also because the ground it is anchored within has been changing, has been softened by the EDC to the point where a catastrophic failure could occur.

The only way I can describe the agencies' earlier responses to my Sunshine Law and Freedom of Information Requests was "stonewalling." I persisted, however, and just this week, in response to my appeal at the Federal level, I received a new CD with 54 pages of material, much of which was redacted, as had been an earlier, ~3,000 page CD. However, I did see, despite many unredacted pages which were poorly-copied, washed-out looking, a few pages with quite pertinent information. That information further convinced me that my concerns are valid, concerns about the threat to the public from a possibly sudden shift and partial collapse of the bridge.

I know that you have not only the material I have seen but undoubtedly material much more recent than the most recent (year 2009) material in the CD I just received. What I saw in the 2009 material made complete sense to me when I thought back to what I have seen from my reviews of materials during situations at hazardous waste disposal sites as well as the chlorocarbon train wrecks at Livingston, Eunice, and Fisherville.

For example, there have been two studies commissioned by industrial companies, to determine what effect EDC has on local clays. Both studies showed that our regional clay is quickly and severely degraded by EDC. It loses its ability to bear weight and it loses its ability to retard movement of fluids. One study was done by Capozzoli and Associates for BFI at Willow Springs and the other was done by Kirk Brown and Associates for CWMI at Carlyss. The Louisiana Department of Environmental Quality has copies of each of those studies in the files of the two companies.

From the train wrecks we also saw that chlorinated hydrocarbons move quickly through soils and degrade the soils as they move through. In the case of Fisherville, the situation became something ironically spoken about as "the annual Fisherville train wreck" when rail cars would again occasionally derail at the same location because the track foundation experienced ongoing degradation despite the constant collection of material through recovery wells. Once the chlorinated hydrocarbons are released they cannot be fully-retrieved and they never stop turning the clay into mush.

Restore Explicit Symmetry To Our Ravaged Earth

To: April English

Subject: I-10 Proposed bridge alternatives

Date: Friday, August 04, 2017 9:34:48 AM

You received a message from QWICKONE@GMAIL.COM

When will a document be available showing the new proposed alternate routes as displayed at the August 3rd meeting for the I-10 Calcasieu River Bridge be available? Thank you.

To: April English

Subject: Public Comment about I-10 Calcasieu River Bridge Project

Date: Friday, August 04, 2017 10:38:23 AM

You received a message from volatilegx@gmail.com

I am in favor of the project to construct a new I-10 bridge with three lanes in each direction and a shoulder on each side. The new bridge should be placed just to the North of the existing bridge. The proposed location for the bridge to the South should be rejected, as it will destroy the scenic beauty of our Lake Charles.

To: April English

Subject: i10 bridge and samson st interchange.

Date: Tuesday, August 08, 2017 11:25:00 AM

You received a message from mb0234@yahoo.com

It seems to me that it would be more cost effective to reduce the proposed bridge that would not touch the EDC spill by adding a two lane exit ramp high enough to go over the RR track on Sampson and also have an entrance ramp coming back onto I-10 and looping under the interstate to continue east.

Also, we should think of future infrastructure needs, as apparently leaders in the past did not, and increase the number of lanes to eight instead of six. We could actually use those eight lanes right now.

Under no circumstances should we consider a compensated foundation for this bridge. Do not tamper with our water source; our ultimate source of survival.

To: April English

Subject: I-10 Bridege Lake Charles La.

Date: Tuesday, August 08, 2017 12:33:08 PM

You received a message from jrv@centurygrp.com

I would like to see a committee of local public officials meet with all parties involved with the pollution issue to get it settled as soon as possible. Also the design of the bridge could mimic the same I-10 Bridge over the Sabine River where large tug boats could continue to service the port property just north of it. Have three lanes each side and the outside west lane could veer over the railroad tracks and tie into the Westlake entrance road. We need to start immediately so that we can build the bridge just north of the existing bridge before it is shut down due to cracked beams etc.. As a small business owner in Sulphur we are seeing the impact of slow traffic with both bridges open. If I-10 is shutdown it will be a disaster for the economy in SW La. and have a large impact on adjacent states along the gulf coast. The neighboring states will help us get funding because I-10 is a major pipeline for the gulf coast economy.



Please Print

I-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End)
State Project No. H.003931

Public Meeting Comment Form

Please provide your comments on the following items:

- Preliminary Alternatives
- · Alternatives Screening Methodology and Results
- Recommended Reasonable Alternatives for further evaluation in the Environmental Impact Statement (EIS)

The Recommended Reasonable Alternatives are as follows:

- Preliminary Build Alternative 2, Sub-Alternatives A-E
- Preliminary Build Alternative 3, Sub-Alternatives A-E

Please return this completed form at the comment table or to a Project Team member.

You can also submit comments online at www.i10lakecharles.com or by U.S. mail to the following address:

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive Suite 230 New Orleans, LA 70122

Comments on the project will be accepted for 45 days following this public meeting.

1 1

NOTE: If you would like your comments to become part of the official public meeting record, they need to be post-marked no later than **August 14**, **2017**.

Name: Wendy Whelah
Address: 1509 Alvin St. LC 70601
Email: WMWhelan @ gmay.com
Agency (if applicable):
Would you like to receive future updates on the project? Yes or No (circle one)
Support PBA3 because it doesn't provoke the area contaminated under the current I 10 bridge I strongly oppose PBAI and PBA2 since they each disturb the EDC area.
(Continued on Rack)

for the same reasons listed above, I oppose sub-alt 7:	_
The sub-alt options that require the Enterp Blud are pour choices. Sub-alt A accept a are the only acceptable options since they preserve the Lake front attractions.	nse nd E

To: April English

Subject: Comments on I-10 Bridge project

Date: Monday, August 07, 2017 11:46:33 AM

You received a message from wranoskys@yahoo.com

I viewed the online video presentation of last Thursday's public meeting. I'm sorry I was not at that meeting. I applaud efforts to undertake the re-building of the I-10 bridge, since it's obviously a current high risk collapse. I am not an engineer, but if I grasped all the problems and choices we're faced with, I would support the most conservative approach to the replacement. That seems to me to be the long span or two bridges together North of the current bridge, to be anchored with the safest foundation outside of the EDC area.

Thanks for the opportunity to make my comment on this very important decision. Sincerely, Linda Wranosky, 4004 Woodcrest Street, Lake Charles, LA 70605



Agency / Public Meeting #3 Summary Report April 2019

I-10 CALCASIEU RIVER BRIDGE I-10/I-210 West End - I-10/I-210 East End



PUBLIC INVOLVEMENT SUMMARY REPORT



Elected Official/Agency Briefing and Public Meeting #3

State Project Number: H.003931 Calcasieu Parish, Louisiana April 25, 2019







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1.0 INTRODUCTION

The Louisiana Department of Transportation and Development (DOTD), in conjunction with the Federal Highway Administration (FHWA), are preparing an Environmental Impact Statement (EIS) for the I-10 Calcasieu River Bridge Project. The proposed project is approximately 9 miles in length and includes alternatives for improvements to I-10 in the Lake Charles region between the I-210 interchanges, including the Calcasieu River Bridge (see **Figure 1**).



Figure 1: Project Location Map

The purpose and need of the proposed Project is to (a) address the lack of system connectivity on I-10; (b) reduce congestion; (c) address roadway and bridge deficiencies; and (d) address roadway and bridge safety concerns. The alternatives developed to address the above needs will be evaluated in the EIS. An EIS studies a range of reasonable alternatives, demonstrates compliance with environmental laws, and provides a means for public and agency input into the decision-making process.

The following document summarizes the input obtained as part of the third round of agency and public meetings associated with the proposed project. The purpose of these meetings was to present project features such as the study area and purpose and need (previously presented at the 2013 Scoping Meeting) and obtain input on the proposed Preliminary Alternatives, including a new alternative, PBA5 with Sub-Alternative G, developed after the previous public meeting held in August 2017. Also presented at the meeting were the draft alternatives screening methodology, the draft screening results, and the LADOTD/FHWA recommended Reasonable Alternatives for detailed evaluation in the EIS. Meeting attendees were given the opportunity to comment on these items as well as all aspects of the project. Note: these are recommendations only; the Reasonable Alternatives will not be formally identified until public and agency input is incorporated into the screening of alternatives.

2.0 ELECTED OFFICIAL/AGENCY BRIEFING & PUBLIC MEETING #3

The second elected officials/agency briefing and third public meeting were held on Thursday, April 25, 2019 at the following location, pictured in **Figure 2**:

Recreation District #1 Multipurpose Complex 1221 Sampson Street Westlake, LA 70669

The elected officials/agency briefing was held from 2:30 PM to 3:30 PM, followed by the public meeting from 4:00 PM to 7:00 PM.

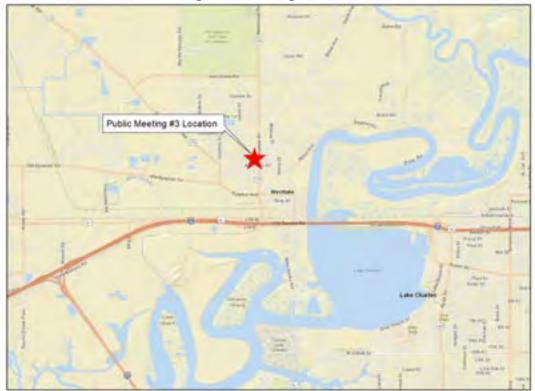


Figure 2: Meeting Location

2.1 Advertisement & Outreach

Multiple avenues of advertisement and outreach were utilized to inform the agencies, public, and other stakeholders about the meetings. They are as follows:

- Agency Meeting Invite Letters Agency meeting invitation letters mailed to over 100
 Federal, state, and local agency representatives and tribes; and over 40 local and
 legislative elected officials. See Appendix A-1 for example invite letter and list of invitees.
- Public Meeting Notices Postcards were mailed and emailed to over 200 individuals owning property located adjacent to the Preliminary Alternatives. See Appendix A-2 for a copy of the postcard.
- Newspaper Advertisements Ran in the Lake Charles American Press two weeks and one week prior to the public meeting. See Appendix A-3 for the meeting advertisement copy and tear sheets from the newspaper.
- **Virtual Public Meeting**—A virtual public meeting was created and placed on the project website, www.i10lakecharles.com. All exhibits and materials from the meeting were posted to the website on April 20, 2019.
- Press Release Sent to local television and media outlets. See Appendix A-4.

2.2 Meeting Attendance

The elected officials/agency briefing was attended by 7 agency representatives and elected officials. Public Meeting #3 was attended by 54 individuals, not including LADOTD, FHWA and the Consultant Team. Public meeting participants represented a wide range of interests and included members of the public, members of community organizations, elected officials and agencies. Copies of the sign-in sheets from the briefing and meeting are included in **Appendices B-1 and B-2**, respectively. Project Team sign-in sheets are provided in **Appendix B-3**.

2.3 Meeting Format and Materials

The briefing began with a welcome from LADOTD, followed by introductions of attendees and a brief presentation by the Project Team. A repeating presentation explaining the purpose of the meeting and providing an overview was then shown, followed by a question/answer session.

The nine stations were set up prior to the briefing so that attendees could view materials to be presented later in the evening to the public. A copy of the repeating presentation is included in **Appendix C-1**.

The public meeting utilized an open-house format with nine distinct stations, including a station where attendees could view the repeating presentation and meeting materials. Project Team members were available at every station to provide information and answer questions.

The nine public meeting stations are described below, in the order that they were intended to be viewed by the public. The materials available at each station are summarized in **Table 1** and described below.

Station 1: We Icome & Sign-In - At this station, members of the public signed in, learned about the meeting format, and received introductory handout materials. Materials handed out (**Appendix C-2**) included:

- A public meeting program guide describing the meeting format and station set-up;
- A project features handout describing the proposed improvements and Preliminary Alternatives; and
- A comment form.

Station 2: Presentation – This station was set up for attendees to view a repeating presentation on a large screen. The presentation provided a project overview, outlined the Preliminary Alternatives (including new alternative PBA 5 with Sub Alternative G), discussed the alternatives screening process, and instructed the public on how to submit comments. The presentation was designed to repeat after each showing so that attendees could view it at any time over the duration of the public meeting (**Appendix C-1**).

Station 3: Project Overview – Three exhibit boards (**Appendix C-3**) were on display at this station:

- A map of the project study area;
- An exhibit describing the purpose and need of the project and;
- An exhibit providing information about the EIS process and anticipated project timeline

Station 4: Environmental – Four exhibit boards (**Appendix C-4**) were on display at this station:

- Two constraints maps presenting the environmental constraints identified to-date (an east exhibit and west exhibit);
- An exhibit describing Section 106 of the NRHP, the Programmatic Agreement for Historic Bridges as it relates to the Calcasieu River Bridge.

Station 5: Pre liminary Alternatives and Alternatives Screening Process – Two exhibit boards (**Appendix C-5**) were on display at this station:

- An exhibit outlining the Preliminary Alternatives; and
- An exhibit presenting the screening process or how the Preliminary Alternatives will be narrowed to Reasonable Alternatives for further evaluation in the EIS.

Station 6: Features of the Preliminary Build Alternatives – Three exhibits (**Appendix C-6**) were on display at this station:

- A large 8-feet by 10-feet display of the project area with callouts for major features of the Preliminary Build Alternatives, including but not limited to what improvements are planned along the entire project corridor, different construction method possibilities in/over the EDC contamination area, and visual examples of how the new main-span of the Calcasieu River Bridge could look; and
- Three exhibit boards outlining access to/from I-10 at Sampson Street: One for Preliminary Sub-Alternatives A-C, one for Preliminary Sub-Alternatives D-F, and one for Preliminary Sub-Alternative G.

Station 7: Schematics – This station included drawings of the proposed Preliminary Build Alternatives and associated Sampson Street Sub-Alternatives laid out on long tables for public viewing. The station also included one exhibit board detailing which Preliminary Sub-Alternatives

were associated with each Preliminary Build Alternative and laminated schematic diagrams of the PBA+Sub-Alt combinations. The Potential Construction Detours were also presented on laminated sheets for review of phasing with pre-construction activities and potential detours during construction. Copies of these materials are provided in **Appendix C-7.**

Station 8: Screening Results – Three exhibit boards (**Appendix C-8**) were on display at this station:

- An exhibit outlining all 11 screening objectives and associated screening measures;
- An exhibit providing a high-level overview of the draft screening results; and
- An exhibit identifying the recommended Reasonable Alternatives for further evaluation in the EIS.

Multiple laminated copies of the draft alternatives screening matrices were available at this station providing the public with an opportunity to view details of the results from the alternatives screening process that led to the Reasonable Alternatives being recommended for evaluation in the EIS.

Station 9: We Want to Hear from You - This station included a sitting area and comment forms for meeting participants to complete and submit comment forms at the meeting venue. Station 9 also presented an exhibit detailing the various methods members of the public could obtain more information or provide comments on the project. At the end of the meeting, all written comments were collected from the comment box and are included in the compilation in **Appendix D**.

The materials described at each of the nine stations above are summarized in **Table 1.** Photos from the meetings are included in **Appendix E**. In addition to these materials, right-of-way specialists from LADOTD were available at a table to answer questions from the public and present information in the LADOTD Acquisition of Right of Way and Relocation Assistance Brochure **(Appendix F)**.

2.4 Virtual Public Meeting

Figure 3 illustrates the webpage used to present the virtual public meeting. The webpage was activated on April 20, 2019 and was maintained through the end of the public comment period on May 5, 2019. According to the website analytics (**Figure 4**), the virtual public meeting page was viewed 238 times during that period for an average period of 2 minutes 25 seconds.

Table 1: Public Meeting Materials

Station	Type	blic Meeting Materials Title
	Handout	Public Meeting Program Guide
Station 1: Welcome & Sign-In	Handout	Project Features
	Handout	Comment Form
Station 2: Presentation	Television	Repeating Presentation
	Exhibit Board	Study Area Map
Station 3: Project Overview	Exhibit Board	Purpose and Need
	Exhibit Board	Environmental Impact Statement (EIS) Process and Timeline
	Exhibit Board	Constraints Map (West)
Station 4: Environmental	Exhibit Board	Constraints Map (East)
	Exhibit Board	Section 106 of the National Historic Preservation Act
Station 5: Preliminary	Exhibit Board	Preliminary Alternatives
Alternatives and Alternatives Screening Process	Exhibit Board	Alternatives Screening Process
	Exhibit Board	Features of the Preliminary Build Alternatives
Station 6: Features of the	Exhibit Board	Preliminary Sub-Alternatives A-C
Preliminary Build Alternatives	Exhibit Board	Preliminary Sub-Alternatives D-F
	Exhibit Board	Preliminary Sub-Alternative G
	Exhibit Board	Preliminary Build Alternatives with Sub-Alternatives
Station 7:	Aerial Roll Plots	Preliminary Build Alternatives 1 – 3 with Sub-Alternatives A - E
Schematics	Aerial Roll Plots	Preliminary Build Alternative 4 with Sub-Alternatives A & B
	Laminated 11x17	Construction Phasing and Potential Detours
	Exhibit Board	Screening Objectives
Station 8: Screening Results	Exhibit Board	Screening Results
_	Laminated 11x17	Alternatives Screening Matrices
Station 9: We Want to Hear from You!	Exhibit Board	We Want to Hear from You!

Figure 3: Virtual Public Meeting Webpage

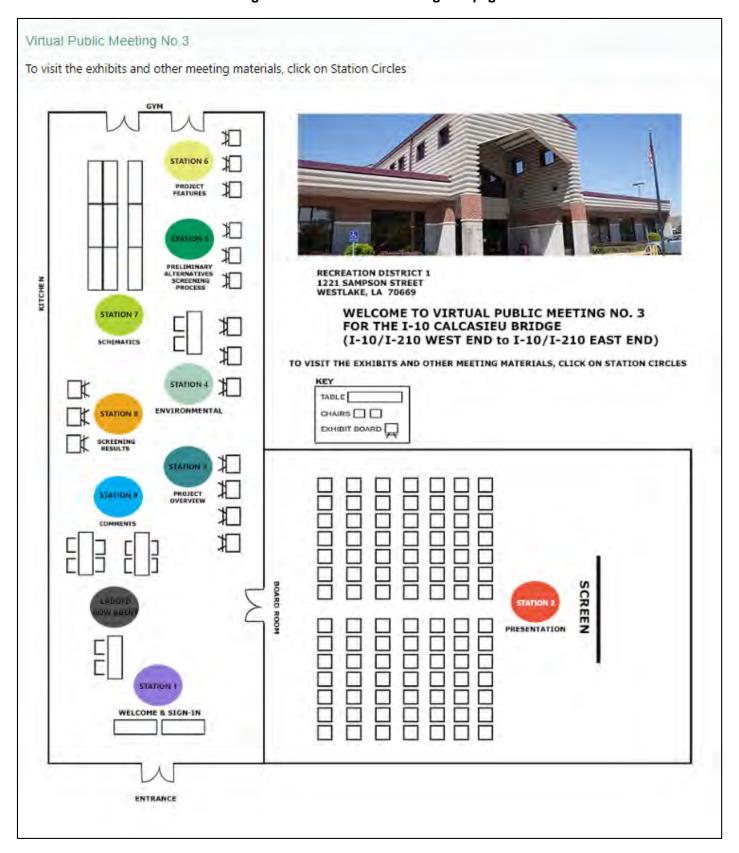
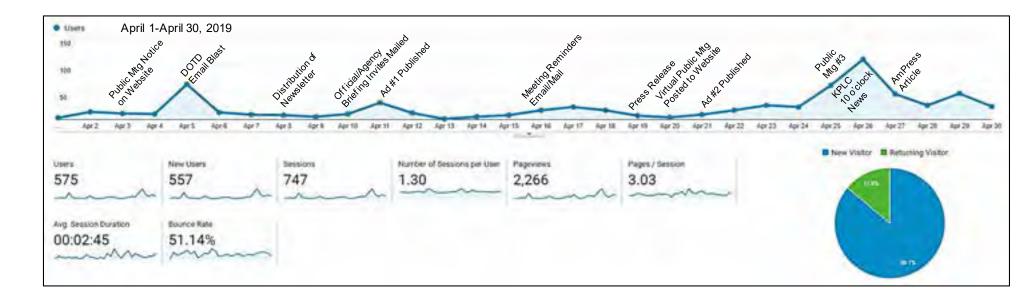


Figure 4: Website Analytics with Public Involvement Timeline

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2.5 Comments

2.5.1 Agency Comments

Table 2: Agency Comments

Name Organization		Title
Walter Council	IMCAL	Regional Transportation Planner III
Della Hoffpauir	Westlake City Council	Councilwoman District C

2.5.2 Public Meeting Comments

The public comment period opened on April 25, 2019 and ended May 5, 2019. Attendees could provide comments through a variety of methods, including the following:

- Submitting a written or verbal comment at Public Meeting Station 9;
- Mailing a written comment to I-10 Calcasieu River Bridge Project c/o HNTB Corporation, 10000 Perkins Rowe, Suite 640, Baton Rouge, LA 70810; or
- Logging on to the project website (<u>www.i10lakecharles.com</u>) and visiting the Virtual Public Meeting or selecting Contact Us.

Table 3 lists the number of comments received by method in which they were submitted.

Table 3: Number of Comments Received

Submission Method *	Number of Comments
Comment Form at Public Meeting	8
US Mail/E-Mail	2
Project Website	21
Total Comments Received	31

Copies of all comments received through these channels are included in **Appendix D-1**. **Table 4** summarizes the comments received by name and date, and provides a response from LADOTD.

Table 4: Comments and Response Matrix

Name				
(Last/First)	Date	Comment(s)	Source	Response
Harbison, Richard	5/3/19	Phillips 66 fully supports thoughtful improvements to the Calcasieu River crossing, believes that PBA 1-F should be included in the recommended reasonable alternatives, and should not be screened out. They disagree with DOTD's reliance on the 2016 data and its risk assessment of construction in the EDC area. Detailed summary of these comments are attached.	Mail	Detailed responses to these comments are provided in Appendix D-2.
Richardson, Danielle	4/26/19	 Do you know when they will buy property if this bridge happens? Suggests an additional bridge at the end of Mike Hooks road ending at the port area to help traffic congestion, as well as a ferry. Supports a new bridge and does not mind paying for it with tolls. 	Website	 Once the Reasonable Alternatives are identified, the potential ROW impacts will be assessed and disclosed in the EIS. Final determination about specific properties that need to be acquired will be determined after project planning is complete. At this time, a date for acquisitions has not been set. The proposed alternatives will address traffic and connectivity on the south side of the proposed mainline from the river to I-210/I-10 interchange west end including Mike Hooks Road and the port area. Comment noted.
Partin, Ryan	4/26/19	Close the entrance ramp from Westlake. It is too close to the bridge.	Website	The Sampson Street Sub-Alternatives would improve connectivity, congestion, and safety along Sampson Street and the I-10 Calcasieu River Bridge.
Gothreaux, Timothy	4/26/19	Need for new I-10 bridge is Imminent.	Website	Comment noted.
Cangelose, Laura	4/26/19	Desperate need for relief in the area. Temporary inconvenience is worth it for a long-term solution.	Website	Comment noted.
Woods, Melissa	4/26/19	Prefers doing nothing rather than causing more traffic delays and alternate routes. Does not support tolling or any of the alternatives	Website	Comment noted.

Table 4: Comments and Response Matrix (cont.)

Name	5.1.	0		D
(Last/First)	Date	Comment(s)	Source	Response
Steiner, Andrew	4/26/19	Does not support PBA 4. Supports moveable bridge for Sulphur Avenue and extension to I-10 to provide alternate route to mainline bridge. Sees advantage of and would accept extension all the way to Enterprise. Supports overpasses crossing the railroads.	Website	Comment noted.
Young, Blaine	4/26/19	 Supports the movable bridge connecting I- 10 and Sampson St. Supports overpass in Westlake for Westbound traffic. Another option for that could be build the overpass/interstate entrance off of Sulphur Ave. That way while its being built Sampson wouldn't be shut down. 	Website	1.Comments noted. 2.Only PBA 5-G would close Sampson Street during construction.
Hale, Mary	4/26/19	Supports the Sulphur Avenue extension. Supports any ideas to solve traffic/train conflicts. Supports tolls.	Website	Comments noted.
Benoit, Jeremy	4/26/19	Paying police officers to be present along the foot of the I-10 bridge especially westbound at the Lakeshore Dr. feeder road, and entrance ramp, should at least be implementing the zipper method to keep traffic flowing. Most of the problem lies in the fact that 18 wheelers merge to the left lanes, and can't climb the bridge well. Signs need to be replaced, and/or added to ensure they stay in right lane on bridge.	Website	Comments noted.
Rebekah	4/27/19	Does not support tolls; supports flat fee at an extremely reduced rate for residents of Calcasieu Parish.	Website	Comment noted.
Cain, Dave	4/27/19	Does not support tolls due to already high taxes.	Website	Comment noted.
Reagan	4/28/19	Does not support tolls.	Website	Comment noted.

Table 4: Comments and Response Matrix (cont.)

Name				
Name (Last/First)	Date	Comment(s) Great presentation, to update the public on your plans concerning the Calcasieu River-I-10 Bridge. Not impressed with length of study; This plan is to improve the I-10 corridor from I-210 to I-210 exits. Now, that is a great plan for Lake Charles, but it is VERY short sighted for those of us west of the west most I-210 exit. Although you plan would help Lake Charles residents exit I-10 onto the northern streets of Lake Charles, your plan does nothing about the three exchanges that must handle the full force of construction workers of all industries except	Source	Response
Ellender, Kelvin	4/28/19	for SASOL and about half of Phillips 66. Those of us who live west of the river must deal with the traffic on Hwy 27, Hwy 90, Cities Service and Old Spanish Trail. All of these are paths that the workers take to their jobs, that are over congested and are the real traffic issue. Yes, we need another, wider, I-10 bridge. Yes, we need a wider I-210 bridge. But the roads that the workers are traveling all lead there. Alleviate congestion by having larger bridges; But the money that you are planning on spending to improve the corridor from I-210 exit to I-210 exit, would be MUCH better spent widening the I-10 corridor where the most traffic congestion is.	Website	Comments noted.
Woodside, Joe	4/28/19	Does not support tolls.	Website	Comment noted.
Bengston, Lynn	4/29/19	Does not support tolls for Westlake residents. If tolled need other options for non-toll route.	Website	Comment noted.
Duhon, Matthew J.	4/29/19	Does not support tolls due to already high taxes.	Website	Comment noted.

Table 4: Comments and Response Matrix (cont.)

Name (Last/First)	Date	Comment(s)	Source	Response
Duhon, Howard	4/29/19	Does not support tolls.	Website	Comment noted.
DeVore, Daniel	4/29/19	Does not support tolls due to already high taxes.	Website	Comment noted.
Landry, Patrick	5/3/19	Supports moving forward with evaluation of PBA 2-C, PBA 3-A, and PBA 5-G. Other design recommendations include: • Elevate I-10 over US-171 to help improve I-10 geometry and interchange connectivity w/minimum impact. • I-10 near Ryan Street - Remove WB off ramp to LS Drive to reduce restricted movement controls. • Remove EB on ramp from LS Drive to improve ramp spacing and add connection from I-10 WB SR to I-10 EB SR to improve Ryan St. Access. • Include I-10 geometric improvements at RR overpass near Opelousas St. • Add 3 thru lanes at all 2 lane sections in study area including I-210 interchanges between ramp connections.	Website	The current proposal for I-10 is three lanes on the I-10 mainline and bridge in each direction between the I-210 interchanges. The traffic analysis to be prepared as part of the EIS will determine if this number will meet the needs of future traffic. Other proposed improvements along I-10 between the I-210 interchanges include: Replacement of the I-10 EB to I-210 ramp bridge. Six-lane overpass of PPG Drive. Changes to US 90 overpass to allow for widening of the I-10 mainline. Access improvements for Sampson St. to/from I-10. Six-lane overpasses with improved vertical clearance at Veterans Memorial Blvd., Ryan St., Bilbo St., Kirkman St., Enterprise Blvd., Shattuck St., and Opelousas St., and RR overpass near Opelousas St. New U-turns under the overpasses as needed. Improvements to US 171 overpass to allow for widening and improving vertical clearance. Replacement of the Calcasieu River Bridge Required drainage improvements.
Kramer, Daniel A.	5/3/19	Against the 73' minimum under-span height of the bridge. Bridge should continue to have the 135' vertical clearance to support the businesses north of the proposed Interstate 10 Calcasieu River Bridge, such as Louisiana Scrap Metals Recycling Lake Charles, L.L.C. and Friend Ships. Consideration of these businesses should include assistance with locating and acquiring commercial deepwater port facilities south of the proposed I-10 Calcasieu River Bridge	Website	In accordance with the USCG Bridge Program, Reasonable Needs of Navigation White Paper (2012) and as part of the EIS, LADOTD will evaluate if any current or reasonably foreseeable future navigation would be unreasonably obstructed by the proposed I-10 Calcasieu River Bridge. Mitigation for impacts to navigation may include modifications to reduce the air drafts of the affected vessels or identification of locations south of the bridge that could accommodate these.

Table 4: Comments and Response Matrix (cont.)

Balmos, David 5/3/19	1. Two suggestions for PBA 5-G Shift the alignment of Sampson Street north of I-10 to the east, still within existing ROW, the elevated interchange could be constructed while traffic remains on the current at-grade pavement. The shift eastward would essentially fall on the same location as the proposed new at-grade access road was shown at the public meeting. The at-grade access could be provided by the current pavement. Basically, switch the location of the proposed elevated pavement and the proposed at-grade pavement. Some partial closure of Sampson Street would still be required at the tie-in at the northern limit of the new-location alignment. But this approach would likely eliminate the need to fully close the Sampson Street intersection during construction. 2. Suggest that the westbound exit ramp from I-10 to Sampson Street be aligned with the westbound entrance ramp to I-10. There are multiple solutions to construct the elevated ramp across the EDC plume including shallow foundations (short spans with driven piles of a depth of 25' +/- that would be well above the depth of the EDC contaminates), or compensated foundations that are recommended as on one the feasible solutions being carried forward by the project team or fill sections with MSE retaining walls. Any of these solutions could be evaluated further to improve the operations of the future Sampson Street interchange.	Website	1. This suggestion was considered during development of PBA 5-G but was not pursued because it would not avoid closure of Sampson Street and would also require the acquisition of additional ROW at the tie-in to Sulphur Avenue where the ROW narrows (the proposed at-grade access road lies within existing ROW). However, if PBA 5-G is brought forward for evaluation and refinement in the EIS, the impacts from shifting of the alignment will be weighed against the potential benefit of reducing the frequency and duration of Sampson Street closures. 2. The project team developed PBA 5-G to maintain the interchange at Sampson Street and minimize the risks associated with the EDC contamination. If PBA 5-G is brought forward for evaluation in the EIS, the design will be refined to meet design criteria as well as optimize traffic operations.
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Table 4: Comments and Response Matrix (cont.)

Name				
(Last/First)	Date	Comment(s)	Source	Response
Knapp Jr., Leonard	5/1/19	 Suggest plan for 40 year minimum of bridge and under structure to support 8 lanes (4 each way, plus space for on off ramp slowdowns. Current bridge approaching 50 years of service - will we be less reliant on vehicles in future. Wider bridge - even if not utilized immediately. Also expand I-10 access roads to encompass larger volume. Also could then have separate lane - HOV. PBA 4 needs further work up to identify desirability and problems. PBA 5 - probably too steep to bring down without effect on EDC area. Then would need to span EDC area at grade anyway. PBA 1 - Object to going through EDC and create pathways to drinking water. PBA 2&3 - Probably okay if EDC area is not breached. Sampson Street Sub Alternatives goal should be for going both east and west from Sampson to access I-10 while going over RR tracks above or below and avoiding any disturbance of EDC contaminated area. Favors Sub-Alt E joining I-10 west of Ryan St. 	Public Meeting	 The current proposal for I-10 is three lanes on the I-10 mainline and bridge in each direction between the I-210 interchanges. The traffic analysis to be prepared as part of the EIS will determine if this number will meet the needs of future traffic. Comment noted.
Council, Walter	4/25/19	MPO staff, TAC, and TPC believe that a bike/ped shared path should be incorporated into the I-10 Calcasieu River Bridge project because this will be the only opportunity in the next 50-75 years to create a bike/ped connection from Lake Charles to Westlake.	Officials Briefing and Public Meeting	LADOTD Complete Streets Policy (2016) supports consideration of bicycle and pedestrian accommodations on all new and reconstruction roadway projects appropriate to the context of the roadway. However, LRS 32:263 prohibits the use of LA interstate highways by pedestrians, bicyclists, and other non-motorized vehicles.
Hoffpauir, Della	4/25/19	Supports PBA 5-G; does not support alternatives that propose extension to Ryan Street or Enterprise (Sub-Alts A, B, C, D, E)	Officials Briefing	Comment noted.

Table 4: Comments and Response Matrix (cont.)

Name (Last/First)	Date	Comment(s)	Source	Response
Johnson, Mike	4/25/19	Please use all federal tax dollars and state matching funds before a toll bridge	Public Meeting	Comment noted.
Price Jr., Rob	4/25/19	Concerned about truck access in and out of Mike Hooks Road.	Public Meeting	Impacts to access in and out of Mike Hooks Road during construction will be temporary and detours available. The traffic analysis to be prepared as part of the EIS will evaluate operations at the Sampson Street interchange and frontage roads on the south side of the proposed I-10 Calcasieu River Bridge project and determine if the proposed design should be reconsidered.
Rau, Fred	4/25/19	Supports PBA 5-G	Public Meeting	Comment noted.
Rau, Denise	4/25/19	Supports PBA 5-G and would like to expedite the EIS Process	Public Meeting	Comment noted.
Bergeron, Michael	4/25/19	Does not support Sulphur Ave extension that would relocate middle school (Sub-Alts A, B, C, D, E).	Public Meeting	Comment noted.
Jones, Sam Houston	4/17/19	Public Private Partnership should be considered.	Email	Funding sources that will be considered include Public Private Partnerships.

Appendices

Appendix A: Advertising and Outreach

A-1: Agency Invitation Letter and Mailing List

A-2: Postcard

A-3: Advertisements

A-4: Press Release

Appendix B: Meeting Sign-In Sheets

B-1: Public Sign-in Sheets

B-2: Elected Officials/Agency and Media Sign-in

Sheets B-3: Project Team Sign in Sheets

Appendix C: Meeting Materials

C-1: Presentation

C-2: Meeting Handouts

C-3: Station 3: Project Overview Exhibits

C-4: Station 4: Environmental Exhibits

C-5: Station 5: Preliminary Alternatives and Alternative Screening process Exhibits

C-6: Station 6: Features of the Preliminary Build Alternatives

C-7: Station 7: Schematics and Potential Construction Detours

C-8: Station 8: Screening Results Exhibits

C-9: Station 9: We Want to Hear From You! Exhibits

Appendix D: Comments

D-1: Comments Received

D-2: Response to Phillips 66 Comments

Appendix E: Meeting photos

Appendix F: LADOTD Acquisition of Right of Way and

Relocation Assistance Brochure

Appendix A: Advertising and Outreach

Appendix A-1: Agency Invitation Letter and Mailing List



Environmental SectionPO Box 94245 | Baton Rouge, LA 70804-9245

ph: 225-242-4502 | fx: 225-242-4500

John Bel Edwards, Governor Shawn D. Wilson, Ph.D., Secretary

April 10, 2019

Mr. Judd Bares President Calcasieu Parish Police Jury 2220 Bon Vie Drive Sulphur, LA 70665

RE: Environmental Impact Statement (EIS)

I-10 Calcasieu River Bridge (I-10/I-210 West End to I-10/I-210 East End)

Calcasieu Parish, Louisiana State Project No. H.003931 F.A.P. No. BR-10-1(212)29

Dear Mr. Bares:

The Louisiana Department of Transportation and Development (LADOTD) and the Federal Highway Administration (FHWA) invite you to an agency and public officials briefing for the above-captioned project. The purpose of the briefing is to present Preliminary Build Alternative (PBA) 5-G, which was developed by LADOTD in response to comments received after the public meeting held in August 2017. PBA 5-G proposes a fully directional interchange at Sampson Street in Westlake. Details of this PBA will be exhibited along with previously presented PBAs 1-4. We are soliciting your comments on PBA 5-G and the other proposed PBAs, the alternatives screening process, and the reasonable alternatives recommended for detailed evaluation in the Environmental Impact Statement (EIS).

The briefing will be held in the board room of Recreation District 1 Multipurpose Complex from 2:30 p.m. to 3:30 p.m. on April 25, 2019 at the below address:

Ward 4 Recreation District 1 Multipurpose Complex 1221 Sampson Street Westlake, LA 70669

Representatives from DOTD, FHWA, and the consultant team will facilitate a discussion on issues material to the additional PBA, the evaluation and screening of all PBAs, and the results of that screening leading to recommended Reasonable Alternatives to be studied in greater detail as part of the EIS.

We would also like to remind you that a public meeting will be held at the same location on the same day from 4:00 p.m. to 7:00 p.m. The public has been invited to this meeting to learn more about the project, discuss issues, and ask questions. Comments will be accepted at the meeting or by mail postmarked no later than May 5, 2019.

If you have any questions or would like to discuss the proposed project in more detail, please contact me at (225) 242-4501 or Mr. Joachim Umeozulu, the Project Manager, at (225) 379-1386.

Thank you in advance for your interest in this project.

Sincerely,

Noel A. Ardoin, P.E.

Environmental Engineer Administrator

M. ardoin

Enclosure: Project Location Map

cc: Project File

Joe Umeozulu, DOTD

Salutation	First Name	Last Name	Title	Agency/Organization	Phone	E-mail	Mailing Address	City	State	Zip
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VIr.	Rodney	Geyen	Council Member, District C, President	City of Lake Charles	(337) 433-4018	citycouncil@cityoflc.us	326 Pujo St	Lake Charles	LA	70601
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	· ·					· · ·		Sulphur	LA	70663
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Mr.	Jeremy	Cryer	Council Member, Seat B	City of Westlake		c/o City Clerk	P.O. Box 700	Westlake	LA	70669
VIs.	Betty	Bates	Council Member, Seat C	City of Westlake		c/o City Clerk	P.O. Box 700	Westlake	LA	70669
∕Ir.	Dan	Racca	Council Member, Seat D	City of Westlake		c/o City Clerk	P.O. Box 700	Westlake	LA	70669
۸r.	Kenny	Brown	Council Member, Seat E	City of Westlake		c/o City Clerk	P.O. Box 700	Westlake	LA	70669
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	· ·		· ·					Washington	DC	20515
lonorable	Mike	Johnson Abarbara MAD	United States House Of Representatives	District 04	(202) 225-2777 (202) 225-8490		327 Cannon HOB 417 Cannon HOB	Washington Washington	DC DC	20515 20515
lonorable Ionorable	Ralph Garret	Abraham, M.D. Graves	United States House Of Representatives United States House Of Representatives	District 05 District 06	(202) 225-8490		430 Cannon HOB	Washington	DC	20515
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Senator	John	Kennedy	U.S. Senator	U.S. Senate	(202) 224-4623	2 1 10 2 6 41	SR383, Russell Senate Building	Washington	DC	20510
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Salutation	First Name	Last Name	Title	Agency/Organization	Phone	E-mail	Mailing Address	City	State	Zip
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Mr.	Stephen	Pfeffer	DOTD Liaison	U.S ARMY CORPS OF ENGINEERS NEW ORLEANS DISTRICT		stephen.d.pfeffer@usace.army.mil	email only			

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	CPPJ Gravity Drainage			CPPJ Gravity Drainage District Number 5 Ward 4			1331 Swisco Road
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Dr.	CHUCK	Call blowii	Secretary	Environmental Quality Louisiana Department of	(223) 219-3933		1301 Gadwall Street
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				Natural Resources			P.O. Box 44487
				Interagency Affairs & Field		charles.reulet@la.gov	Baton Rouge Louisiana
Mr.	Charles	Reulet	Administrator	Services Division	(225) 342-0861		70821-4487
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			Conservation, Assistant	Natural Resources Office of		richard.ieyoub@la.gov	BATON ROUGE, LA 70804-
Mr.	Richard	leyoub	Secretary	Conservation	(225) 342-5540		9275
_				Louisiana Department of			
			Executive Director &	Public Safety Highway		lisa.freeman@la.gov	PO BOX 66614
Ms.	Lisa	Freeman	Governor's Representative	Safety Commission	(225) 925-6991		BATON ROUGE, LA 70896

	1			Louisiana Division of	I		
				Administration Office of		Jonathan.Robillard@la.gov	P.O. Box 44124
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Me	Andrea	McCarthy	Section 106 Reviewer / GIS	Louisiana Division of Historic Preservation	(225) 342-8164	amccarthy@crt.la.gov	Baton Rouge LA 70804
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Mr.	Doug	Zettlemoyer	Regional Coordinator	Region 5	(225) 405-9174		Baton Rouge, LA 70806
IVII.	Doug	Zettiemoyei	Regional Coordinator	Lousiana Department of	(223) 403-3174		P.O. Box 98000
Mr.	Kyle	Balkum	Biologist Program Manager	•	(225) 765-2819	kbalkum@wlf.la.gov	Baton Rouge, LA 70898
	ŕ						,
				Lousiana Department of		cperry@wlf.la.gov	
				Wildlife and Fisheries		cperry@wii.ia.gov	2000 Quail Drive
Ms.	Carey Lynn	Perry	Program Manager	Natural Heritage Program	(225) 765-3982		Baton Rouge, LA 70808
D.:	Charles "Chip"	McGimsey	State Archaeologist and	Louisiana Division of	(225) 219-4598	cmcgimsey@crt.la.gov	P. O. Box 44247
Dr.	· · · · · · · · · · · · · · · · · · ·	,	Director	Archaeology Lousiana Office of Cultural	, ,		Baton Rouge, LA 70804 P.O. Box 44247
Ms.	Kristen	Sanders	Assistant Secretary	Development	(225) 342-8200	ksanders@crt.la.gov	Baton Rouge, LA 70804
IVIS.	Kristeri	Sanders	Assistant Secretary	·	(223) 342-8200		LSU
				NATIONAL MARINE			MILITARY SCIENCE
	Brandon	Howard	Fishery Biologist	FISHERIES SERVICE	225-389-0508 x207	brandon.howard@noaa.gov	BUILDING, ROOM 266
				HABITAT CONSERVATION			SOUTH STADIUM DRIVE
Mr.				DIVISION			BATON ROUGE, LA 70803
				NOAA Fisheries Southeast			
	David	Bernhart	Assistant Regional	Regional Office Office of	(727) 824-5301	david.bernhart@noaa.gov	263 13th Avenue South
Mr			Administrator	Protected Resources			St. Petersburg, Florida
Mr.				U.S. Department of			33701-5505 801 Cherry Street, Unit #45,
				Housing and Urban		TX Webmanager@hud.gov	Ste. 2500
Ms.	Beth A.	Van Duyne	Regional Administrator	Development	(817) 978-5600	TA Webinanager@nud.gov	Fort Worth, TX 76102
	200.71	Tan Dayne	TO DO TO THE TOTAL OF		1,5-7,57.0 0000		
				U.S. Department of the		hsweyers@usgs.gov	3916 Sunset Ridge Rd.
Ms.	Holly	Wyers	Regional Director	Interior, Geological Survey	(678) 924-6609		Raleigh, NC 27607
				U.S. DEPT OF INTERIOR			
	Bob	Vogel	Regional Director	NATIONAL PARK SERVICE	(404) 507-5600	Stan Austin@nps.gov	100 ALABAMA STREET, SW
Mr.							ATLANTA GA 30303

							Mailcode: 6SF-RL
			PM & Superfund Reuse	U.S. Environmental		luckett.casey@epa.gov	1445 Ross Avenue
Ms.	Casey	Luckett Snyder	Coord.	Protection Agency			Dallas, TX 75202-2733
				U.S. Environmental			Fountain Place 12th Floor,
				Protection Agency		houston.robert@epa.gov	Suite 1200
				Office of Planning and		nouston.robert@epa.gov	1445 Ross Avenue
Mr.	Robert	Houston	Chief Manager	Coordination			Dallas, TX 75202-2733
			CHIEF, OFFICE OF	U.S. ENVIRONMENTAL			1445 ROSS AVE.
	Rhonda	Smith	PLANNING AND	PROTECTION AGENCY	(225) 665-8006	smith.rhonda@epa.gov	Suite 1200
N.A			COORDINATION (6EN-XP)	REGION 6	,		Mail Code: 60EJTIA
Ms.			· , ,				DALLAS, TX 75202-2733 1445 ROSS AVE.
				U.S. ENVIRONMENTAL			Suite 1200
	Anne L.	Idsal	Regional Administrator	PROTECTION AGENCY	(214) 665-2100	Idsal.anne@epa.gov	Mail Code: 6RA
Ms.				REGION 6			DALLAS, TX 75202-2733
IVIO.							SUITE 1200
				U.S. ENVIRONMENTAL			1445 ROSS AVE.
	Omar	Martinez	EPA Sole Source Aquifers	PROTECTION AGENCY	(214) 665-8485	Martinez.Omar@epa.gov	Mail Code: 6WG-SG
Mr.				REGION 6			DALLAS, TX 75202-2733
			<u> </u>	U.S. Environmental			
				Protection Agency			1445 ROSS AVE, STE. 1200
				Water Quality Protection		gutierrez.raul@epa.gov	M.C. #6WQ-EM
Dr.	Raul	Gutierrez	Environmental Scientist	Division	214-665-6697		DALLAS, TX 75202-2733
				U.S. ENVIRONMENTAL			1445 ROSS AVE, STE. 1200
	Tom	Nystrom	Associate Director	PROTECTION AGENCY	(214) 665-8331	Nystrom.Thomas@epa.gov	M.C. #6WQ-EM
Mr.				WETLANDS SECTION			DALLAS, TX 75202-2733
				U.S. Environmental			1200 Pennsylvania Ave.
				Protection Agency, Office		ross.david@epa.gov	NW, 4101M
Mr.	Dave	Ross	Assistant Administrator	of Water	(202) 564-5700		Washington, DC 20460
				U.S. FISH & WILDLIFE			1875 Century Blvd NE, Suite
l.,	Leopoldo "Leo"	Miranda	Regional Director	SERVICE	(404) 679-4000	leopoldo_miranda@fws.gov	
Mr.				SOUTHEAST REGION U.S. Fish & Wildlife Service,			Atlanta, GA 30345
				1			
				Southwest Louisiana		joshua marceaux@fws.gov	200 Dulles Drive
Mr	lochua	Marcoa	Eich and Wildlife Diele -i-t	National Wildlife Refuge	(227) 774 5022		
Mr.	Joshua	Marceaux	Fish and Wildlife Biologist	Complex	(337) 774-5923		Lafayette, LA 70506
							Building
							819 Taylor Street, Room
l			1	Federal Transit			14A02
Mr.	Robert C.	Patrick	Administrator	Administration, Region 6	817-978-0550		Fort Worth, TX 76102
							4310 Ryan Street, Ste. 330
Ms.	Cheri	Soileau	Executive/MPO Director	IMCAL			Lake Charles, LA 70605
	CHETT	Joneau	Excedite/Will & Bilector	IIII CA LE	ı	1	take charles, LA 70005

Appendix A-2: Postcard



You are invited to a Public Meeting for the I-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End) State Project No. H.003931

Thursday, April 25, 2019
Recreation District 1
1221 Sampson Street, Westlake, LA 70669

Come and go anytime between 4:00 p.m. and 7:00 p.m. during this open house event. There will be a continuous, repeating slide presentation for viewing.

Join us to review and comment on:

- 1. Preliminary Build Alternative 5-G and other proposed PBAs
- 2. Alternatives Screening Methodology & Screening Results
- 3. Reasonable Alternatives Recommended for Detailed Evaluation in the EIS

Comments will be accepted at the Public Meeting. You may also attend a virtual public meeting and make comments by logging on to the project website at

www.i10lakecharles.com

Please contact Ms. Lynn Maloney-Mujica with HNTB at (225) 368-2800 at least 5 days prior to the Public Meeting if special assistance is needed for meeting participation.

Appendix A-3: Newspaper Advertisements

NOTICE OF PUBLIC MEETING

Notice is hereby given that the Louisiana Department of Transportation and Development (LADOTD) and the Federal Highway Administration (FHWA) will conduct an open-forum public meeting for:

State Project No. H.003931 Federal Aid Project No. BR-10-1(212)29 I-10 Calcasieu River Bridge (I-10/I-210 West End to I-10/I-210 East End) Calcasieu Parish, Louisiana

The meeting will be held at the following place and time:

THURSDAY

April 25, 2019 4:00 p.m. to 7:00 p.m.

Ward 4, Recreation District 1 **Multipurpose Complex** 1221 Sampson Street Westlake, LA 70669

The proposed project includes widening and infrastructure improvements to I-10 between the I-210 interchanges, including the Calcasieu River Bridge. The purpose of the proposed project is to improve system connectivity, traffic congestion, roadway and bridge deficiencies, and safety issues.

The purpose of the public meeting is to present Preliminary Build Alternative (PBA) 5-G, which was developed by LADOTD in response to comments received after the public meeting held in August 2017. PBA 5-G proposes a fully directional interchange at Sampson Street in Westlake. Details of this PBA will be exhibited along with previously presented PBAs 1-4. Public comments on PBA 5-G and the other proposed PBAs, the alternatives screening process, and the reasonable alternatives recommended for detailed evaluation in the Environmental Impact Statement (EIS) will be accepted at the meeting.

Persons interested in the proposed project are invited to attend the meeting in person to review the informational materials and comment on the information presented. The meeting will be held in an open-house format and a slide presentation will be repeated continuously between 4:00 p.m. and 7:00 p.m. Project team members will be available to explain the exhibits and answer questions.

If you are unable to attend the meeting, you may attend a virtual public meeting by visiting the project website at https://www.i10lakecharles.com/. The presentation and other informational materials will be available for viewing at the virtual meeting link from April 18 until May 5, 2019. Comments and questions may be submitted, and questions answered during this time.

All comments received at the public meeting, through the virtual meeting link, and written comments mailed to the address below and post-marked by May 5, 2018 will become part of the meeting record.

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe, Suite 640 Baton Rouge, LA 70810

If you require special assistance due to a disability or require an interpreter to participate in this meeting, please contact Ms. Lynn Maloney-Mujica with HNTB Corporation at least five (5) working days prior to the meeting date by email at maloneymujica@hntb.com, by phone at (225) 368-2826, or by mail to HNTB Corporation, 10000 Perkins Rowe.

Baton Rouge, LA 70810.



LOCAL • MONEY

FINAL PHASE OF PUBLIC SAFETY BUILDING PROJECT

Jennings moves forward on exterior renovations

By Doris Maricle dmaricle@americanpress.com

JENNINGS - The Jennings City Council on Tuesday awarded a contract for exterior renovations to the Public Safety Building, lo-

cated at 110 N. Broadway St. L.K. Breaux and Associates of Crowley was awarded the low bid of \$339,400 for the

The work includes washing and painting the exterior, hurricane-proofing glass doors for the fire department and landscape work including masonry planters, LED lighted walkways for employees entering and exiting the

building and painting the parking lot.

The council withdrew a \$14,875 plan to add wooden sunshade protection to the front doors of the building after the project came in over the \$300,000 budgeted.

The project is the last phase of a renovation project to the Public Safety Building, which houses the Jennings fire and police departments.

"Mayor (Terry) Duhon saw all the interior renovations through and we decided to re-elevate the way the bond money is spent and finish the renovations," Mayor Henry Guinn said.

Architect Wayne Domingue of Architects

Southwest in Lafayette said the project will enhance the exterior appearance of the Public Safety Building and provide some functional elements, including lighting to the facility.

2.920

In 2015, the city sold \$1 million in public improvement sales tax revenue bonds to help renovate the Public Safety Building and city's street department to ease overcrowding and make offices more accessible to the public.

The project is part of a four-phase improvement project for the city's infrastructure, which has included construction of a new city court and City Hall.

AT SOWELA TECHNICAL COMMUNITY COLLEGE

Annual career fair connects students, businesses

By Marlisa Harding mharding@americanpress.com

Sowela Technical Community College hosted its annual career fair on Wednesday with more than 45 business/

agencies and four-year universities available for interested students and alumni to Joseph Lavergne, director

of recruitment/career planning and placement at Sowela, said the networking event is designed to support students who are just beginning, will soon graduate or have earned degrees but are still seeking employment.

"As students come and look at the different vendors they can gain a good view of the job market. Plus, we invite hu-

man resource managers not only representatives so they can get a true feel of the company that could potentially hire them."

Jessie Saunooke, a Louisiana transfer degree student, said the fair changed her outlook on life after graduation. Upon meeting with a representative from Louisiana State University she learned her grade-point average qualifies her for their business school.

"I thought I was pretty sure about what I wanted to do but that's got my wheels turning now. It's opened doors," she said. "There's all kinds of other possibilities out there."

Daniela Delabat, a graphic arts student, said she initially didn't see any representatives preparing you for it."

for graphic designers but did not let that deter her pursuits.

"I began asking who needed a designer because even though the company's not based on it they may need to do ads or promotions," she

Her tenacity paid off. "Some of them said, 'Yes,' and I left them my resume,"

Both students agreed their experience at the college prepared them well for the opportunity to meet with prospective employers.

"Since I walked in the graphic art department, the first they give you is a separate class online on how you should dress and how to answer questions," Delebat said. "From just the start, they're

NOT AS LONG AS HE'S UNDER AUDIT BY THE IRS

Trump says he won't deliver tax returns to Congress

By The Associated Press

WASHINGTON - President Donald Trump, facing a congressional deadline for his administration to provide his tax returns, said Wednesday that he "won't do it" while he's under audit by the IRS.

Trump told reporters at the White House that "I would love to give them, but I'm not going to do it while I'm under audit." The IRS says there's no rule against subjects of an audit from publicly releasing their tax filings.

Rep. Richard Neal, D-Mass., chairman of the House Ways and Means Committee, has asked the IRS to turn over six years' of the president's tax returns by the end of the day. Trump has broken with decades of precedent by not voluntarily releasing his returns to the public.

Trump's position has long been that he is under audit and therefore could not release his returns. But in recent weeks, he has added to the argument, saying publicly and privately that the American people elected him once without seeing his taxes and would do so again.

"Remember, I got elected last time — the same exact issue," Trump said. "Frankly, the people don't care."

The president has told those close to him that the attempt to get his returns were an invasion of his privacy and a further example of the Democratic-led "witch hunt" — like special counsel Robert Mueller's probe — meant to damage him.

Trump has repeatedly asked aides as to the status of the House request, and has inquired about the "loyalty" of

the top officials at the IRS, according to one outside adviser who requested anonymity to discuss private conversations.

Treasury Secretary Steven Mnuchin, who supervises the IRS, said Tuesday that he's not seeking direction from the White House on whether to comply. He said the department would likely respond by Neal's deadline but didn't say whether he would provide the returns as demanded.

Democrats don't expect the department to comply, but they haven't sketched out their next steps.

Neal has adopted a methodical approach to seeking Trump's returns. He has the option of eventually seeking to subpoena the records or to go to court if the IRS does not comply, but it's not clear he'll adopt a more confrontational approach just yet.

NOTICE OF PUBLIC MEETING

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State Project No. H.003931 Federal Aid Project No. BR-10-1(212)29 I-10 Calcasieu River Bridge (I-10/I-210 West End to I-10/I-210 East End) Calcasieu Parish, Louisiana

The meeting will be held at the following place and time

> THURSDAY April 25, 2019 4:00 p.m. to 7:00 p.m.

Ward 4, Recreation District 1 Multipurpose Complex 1221 Sampson Street Westlake, LA 70669

The proposed project includes widening and infrastructure improvements to I-10 between the I-210 interchanges, including the Calcasieu River Bridge. The purpose of the proposed project is to improve system connectivity, traffic congestion, roadway and bridge deficiencies, and safety issues

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All comments received at the public meeting, through the virtual meeting link, and written comments mailed to the address below and post-marked by May 5, 2018 will become part of the meeting record.

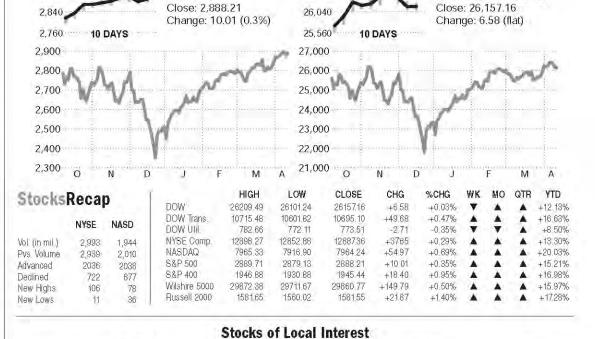
I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe, Suite 640 Baton Rouge, LA 70810

If you require special assistance due to a disability or require an interpreter to participate in this meeting, please contact Ms. Lynn Maloney-Mujica with HNTB Corporation at least five (5) working days prior to the meeting date by email at maloneymujica@hntb.com, by phone at (225) 368-2826, or by mail to HNTB Corporation, 10000 Perkins Rowe, Baton Rouge, LA 70810

S&P 500

26,520

Dow Jones industrials



			WEEK			YTD		Parasian .			WEEK			YTD	
NAME	TICKER	LOW	HIGH	CLOSE	CHG	%CHG	DIV	NAME	TICKER	LOW	HIGH	CLOSE	CHG	%CHG	DIA
AT&T Inc	T	26.80	36.39	31.88	+.13	+11.7	2.04	KCSo pt	KSUp	24.11	33.12	26.22	(11)	+5.7	1.00
AMD	AMD	9.52	34.14	27.83	+.59	+50.8	***	KC Southn	KSU	90.55	120.34	117.12	+.66		1.44
Aegon	AEG	4.42	7.48	4.95		+6.5	.28e	Keycorp	KEY	13.66	21.91	16.39	+.13		.56
Alphabet A	GOOGL			1206.45	+3.76	+15.5	0.00	Kroger s	KR	23.25	32.74	24.98	+1.04	-9.2	.56
Altria	MO	42.40	66.04	55.42	+.31	+12.2	3.20	Lowes	LOW	81.16	117.70	113.76	+.62		1.92
Amazon Amerisafe	AMSF	50.46	2050.50 67.96	58.24	+11.49	+23.0	1.00f	Lyft Inc n MFA Fncl	LYFT MFA	66.10 6.40	88.60 8.15	60.12 7.35	-7.32 +.02		.80
Anadarko	APC	40.40	76.70	46.80	+.63	+6.8	1.20	MRC GIbl	MRC	11.62	22.98	18.41	+.38		.00.
Apple Inc	AAPL	142.00	233.47	200.62	+1.12	+27.2	2.92	Macquarie	MIC	33.71	47.74	41.83	+.14		5.246
AresCap	ARCC	14.50	17.63	17.33	+.11	+11.2	1.60f	MarathnO	MRO	12.57	24.20	17.45	+.19		.20
ATMOS	ATO	82.53	104.07	100.43	15	+8.3		MarathPt s	MPC	54.29	88.45	63.60	+1.44	+7.8	2.12
AuroraC n	ACB	4.58	10.32	9.10	+.29	+83.5	124	MartMM	MLM	150.75	232.88	204.84	14	+19.2	1.92
Avingr nrs	AVGR	0.18	2.44	.83	09	+176.7	1781	MarvellTch	MRVL	14.34	22.95	22.34	+.63	+38.0	.24
BP PLC	Bb	36.28	47.83	45.04	+.05	+18.8		MasterCrd	MA	170.14	240.93	237.06	+1.15	+25.7	1.32
BP Pru	BPT	17.00	37.23	22.53	-1.05	+4.1	4.14e	MetLife	MET	37.76	48.92	44.83	+.53	+9.2	1.68
BkotAm	BAC	22.66	31.91	29.07	+.18	+18.0	.60	MicronT	MU	28.39	64.66	42.17	+.45		400
BedBath	BBBY	10.46	21.63	19.41	+.94	+71.5	.64	Microsoft	MSFT	90.28	120.82	120.19	+.91	+18.3	1.84
BioPhrmX	BPMX	0.05 44.30	0.29	.09	01	-15.7	101	MidsthBcp	MSL MUFG	10.07	16.15	11.48	64 01	+8.3	.04
BrMySq CKX Lands	BMY CKX	9.70	63.69	45.89 10.22	09	-11.7 -0.9	1.64 .12m	MitsuUFJ Mosaic	MOS	4.64 24.06	6.87 37.37	5.04 27.36	08	+3.5	.10
CVS Health	CVS	51.77	82.15	53.87	+.15	-17.8	2.00	NRG Egy	NRG	29.85	43.66	42.26	+.10	+6.7	.12
CaesarsEnt	CZR	5.84	13.54	9.05	+.26	+33.3	2.00	Netflix s	NFLX	231.23	423.21	363.92	79	+36.0	- 12
CapOne	COF	69.90	101.26	85.63	+.53	+13.3	1.60	Novavax	NVAX	0.49	2.58	.57	+.02		
CapsteadM	CMO	6.58	9.37	8.72	+.01	+30.7	.49	OcciPet	OXY	56.83	87.67	67.18	10	+9.4	3.12
CntryLink	CTL	11.52	24.20	12.55	+.16		1.00m	Oceaneerg	OII	10.74	28.62	16.95	+.37	+40.1	.60
CheniereEn	LNG	53.98	71.03	67.05	13	+13.3	.56p	PG&E Cp	PCG	5.07	49.42	18.87	+.05	-20.5	2.12
ChesEng	CHK	1.71	5.60	3.40	+.06	+61.9	44	PNM Res	PNM	34.95	47.92	45.68	12		1.16
Chevron	CVX	100.22	131.08	125.49	05	+15.4		PPG s	PPG	94.37	116.73	114.90	+.62		1.92
Cisco	CSCO	40.25	55.80	55.82	+.64	+28.8	1.40f	PayPal n	PYPL	70.22	107.51	107.51	+.58		
Citigroup	C	48.42	75.24	65.52	+.32	+25.9	1.80	Penney	JCP	0.92	3.54	1.26	05		4.44
CocaCola Compost o	KO CMCSA	41.45	50,84 41.14	46.64	03 +.04	-1.5 +20.6	1.60f .84f	Pfizer Phillips66	PFE	34.37 78.44	46.47 123.97	42.73 98.61	11 +1.24	-2.1 +14.5	3.20
Comcast s ConAgra	CAG	20.22	39.43	29.39	+1.85	+37.6	.85	PortGE	POR	39.18	52.60	51.27	07	+11.8	1.45
ConocoPhil	COP	56.75	80.24	66.97	+1.27	+7.4	1.22	Precipio n	PRPO	0.11	0.68	.30	02		1.40
ContraFct h	CFRX	0.36	2.93	.65	+.11	-57.5	1000	PrecDrill	PDS	1.62	4.14	2.76	+.06		.28
CousPrp	CUZ	7.53	10.10	9.70	+.17	+22.8	.29f	Qualcom	QCOM	48.56	76.50	56.33	-1.11	-1.0	2.48
DNP Selct	DNP	10.00	11.72	11.61	+.02	+11.6	.78	RiteAid	RAD	0.51	2.12	.57	+.01	-19.8	
Danaher	DHR	94.59	134.50	131.50	33	+27.5	.68f	Roku n	ROKU	26.30	77.57	58.57	75	+91.2	-
DeltaAir	DAL	45.08	61.32	57.86	+.91	+16.0	1.40	Rowan	RDC	7.77	20.87	10.93	08		.40
DenburyR	DNR	1.39	6.75	2.50	+.09	+46.2	V-75	RoyDShllA	RDS/A	55.04	73.86	65.01	+.44	+11.6	3.76
Disney	DIS	97.68	120.20	117.16	+.30	+6.8	1.76f	Sasol	SSL	27.40	39.73	33.29	+.31	+13.7	1.41e
DowDuPnt Down Different	DWDP	32.87	48.36	38.50	+.28	+7.1	1.52	Semtech	SMTC	38.95	60.55	52.38	+1.05		0.0
DukeRlty Pay 6	DRE	24.67 26.01	31.07 43.00	30.83	+.43	+19.0	.86	SiriusXM	SIRI	5.48 4.82	7.70	5.99 12.02	+.02		.05
eBay s EnCana g	EBAY ECA	5.00	14.28	37.72 7.23	+.29	+34.4	.14p .06	Snaplnc A n SthnCopper	SCCO	29.01	15.96 58.09	41.07	+.32		1.50e
ENSCO	ESV	3.19	9.51	3.97	04	+11.5	.04	SwstnEngy	SWN	3.23	6.23	4.73	09		1.006
Entergy	ETR	75.45	96.87	94.59	12	+9.9	3.64	Sysco	SYY	59.26	75.98	67.42	+.46	+7.6	1.56
ExxonMbl	XOM	64.65	87.36	81.56	37	+19.6	3.28	TeekLNG	TGP	10.74	18.80	15.20	+.09	+37.9	.56
Facebook	FB	123.02	218.62	177.82	+.24	+35.6		Tegna	TGNA	10.00	15.58	15.42	+.68	+41.9	.28
FstData n	FDC	14.73	27.15	25.69	45	+51.9	344	TelData45	TDI	20.55	26.75	26.53	+.31	+17.3	1.66
FiveStar h	FVE	0.31	1.60	.53	03	+10.6	444	Tellurian rs	TELL	5.90	12.45	10.06	+.21	+44.7	
FordM	F	7.41	12.15	9.33	+.12	+22.0	.60a	TevaPhrm	TEVA	14.50	25.96	14.66	+.11	-4.9	.736
FrptMcM	FCX	9.60	19.70	13.49	+.11	+30.8	.20	TexInst	TXN	87.70	118.48	114.92	+.98		3.08
GenElec	GE	6.66	15.59	9.16	06	+21.0	.04	TortEnInf	TYG	18.36	30.50	23.63	+.04	+18.7	2.62
GlacierBc	GBCI	35.77	47.67	41.91	+.96	+5.8		Toyota	TM	111.12	138.68	122.18	+.12		
VjGrace	GRA	59.22	78.93	77.53	+.16			USSteel	X	17.08	39.23	17.25	52		.20
HP Inc Hallibrtn	HPQ HAL	18.46 24.70	27.08 54.91	19.97 31.04	+.38	-2.4 +16.8	.64	ValeroE VitalThera	VLO VTL	68.81	126.98 9.75	88.43	+3.32		3.60
HarleyD	HOG	31.36	46.79	39.33	+.50	+15.3		VMware	VMW	99.02	188.51	189.31	+3.61		26.81p
Hemisphrx rs		0.14	0.44	.17	03	-3.9	1.001	WalMart	WMT	81.78	106.21	99.60	+.91	+6.9	
HercTGC	HTGC	10.57	14.17	12.70	+.06	+14.9	1.24	WalgBoots	WBA	53.85	86.31	54.51	+.01	-20.2	1.76
Histogen h	HSGX	0.08	3.05	.18		+104.5		WsteMInc	WM	79.96	104.80	102.93	+1.29		
HomeDp	HD	158.09	215.43	199.43	-1.47	+16.1	5.44	Weathfintl	WFT	0.22	3.75	.60	02		
HuntBnosh	HBAN	11.12	16.53	13.30	+.13	+11.6	.56	WeinRlt	WRI	23.80	32.02	29.77	+.44		1.58a
Huya n	HUYA	14.44	50.82	23.21	-1.12	+49.9		WellsFargo	WFC	43.02	59.53	47.79	35		1.80
lberiabnk	IBKC	60.82	87.50	74.31	+.55	+15.6		Welltower	WELL	49.58	79.41	76.52	+.73		
IndiaFd	IFN	17.60	23.07	21.37	+.10		1.70e	WestlkChm	WLK	59.78	124.30	74.93	+.29		1.00
Inpixon rs	INPX	0.73	42.88	1.18	+.38	-63.0	97.	WorldFuel	INT	19.79	33.17	31.10	+.62		.24
Intel	INTC	42.36	57.60	55.75	+.43	+18.8	1.26	WldW Ent	WWE	36.28	97.69	94.18	+3.14		.48
JPMorgCh	JPM	91,11	119.24	105.34	+.47	+7.9	3.20	ZayoGrp	ZAYO	20.27	39.66	31.13	+1.68	+36.3	-

issue in the last year. The 52-week high and low figures date only from the beginning of trading, rs - Stock has undergone a reverse stock split of at least 50% within the past year, s - Stock has split by at least 20 percent within the last year, Dividend Footnotes; a - Extra dividends were paid, but are not included, e Amount declared or paid in last 12 months. I - Current annual rate, which was increased by most recent dividend announcement, m - Current annual rate, which was decreased by most recent dividend announcement, p - Initial dividend, annual rate not known, yield not shown.

Stock Footnotes: g - Dividends and earnings in Canadian dollars. h - Does not meet continued-listing standards. If - Late filing with SEC in - Stock was a new

MutualFunds -- TOTAL RETURN/RANK --FAMILY FUND NAV CHG YTD 5YR 1YR 3YR American Funds AmrenBalA m 27.05 +.04 +9.0 +6.7/B+8.0/ +9.3/A +.11 +14.6 CptWldGrlncA m 48.85 +1.2/D+11.1/C +6.7/ CptllncBldrA m 61.07 +.10 +6.2/D +4.8 EuroPacGrA m 52.18 +.05 +15.7 -3.7/C +10.1/B +4.6 FdmtllnvsA m 59.64 +.18 +14.4 +6.4/D +13.9/B +11.2GrtAmrcA m 49.72 +.18 +16.3 +9.1/D +16.6/C +12.6/ IncAmrcA m 22.34 +.04 +9.1 +4.6/B +8.1/E +6.4 InvCAmrcA m 38.25 +.08 +13.3 +6.6/D +12.1/D +9.8/ 45.80 +.06 +12.0 +9.1/C +13.5/C +10.5 +3.6/A Dodge & Cox 13 67 +.02 +3.9 +4.5/A +3.1 +1.5 Stk 187.84 +.78 +12.9 +5.8/B +15.4/A +9.7/ DoubleLine **TtlRetBdl** 10.53 +.01 +2.0 +4.3/B +2.6/B +3.1/ Fidelity 500ldxlnsPrm 100.37 +.36 +15.9 +10.9/A +14.4/A +11.8 Contrafund 12.77 +.06 +17.0 +10.2/D +16.8/C +13.5/ TtlMktldxInsPrm +.40 +16.4 82.17 +10.2/B +14.5/A +11.3 Franklin Templeton CATxFrIncA1 m 7.40 +.01 +3.3 +5.3/A +2.6/B +4.3 GlbBdA m 11.50 +.01 +3.3 +31/B +5.1/A +18/ **GlbBdAdv** 11.45 +.01 +3.4/B +5.4/A +2.1 IncA1 m 2.34 +.01 +11.3 +7.2/A +9.2/A +4.0/ IncC m 2.37 +11.0 +6.6/A +8.5/A +3.5/D Lord Abbett AffiliatedA m 14.71 +.05 +12.3 +11.6/B +8.6/B PIMCO Inclust 12.03 +3.4 +4.4 +6.5 +5.5 **TtlRetIns** 10.11 +.01 +2.8 +3.8/D +2.7/B +2.7/B T. Rowe Price **AfrcMdlEs** +.18 +11.5 -3.9/D+10.6/B +1.3/0 +11.7/B Ealne 30.61 +.09 +12.7 +3.8/D+7.3/DNewInc +.01 +3,1 +4.0/C +2.0/C +2.4/0 9.36 SciandTech 40.03 +.20 +27.5 +12.1/D +24.1/B +17.9/0 ShrtTrmBd 4.69 +.01 +16 +3.1/B +17/C +1.3/0 Vanguard 500ldxAdmrl 266.71 +.95 +15.9 +10.9/A +14.4/A +11.8/A DevMldxAdmrl 13.39 +.04 +12.4 -4.2/B +8.8/A +3.3/B InTrTEAdmrl 14.15 +2.5 +5.1/A +2.1/B +3.1/B +.92 +15.9 Insidxins 261.41 +10.9/A +14.4/A InsldxInsPlus +.93 +15.9 261.43 +10.9/A +14.4/A +11.8/A PrmCpAdmrl 137.22 +.67 +13.5 +9.6/D +17.7/B +13.8/B **TMSmCpAdm** +.94 +15.2 +9.7/A **TtBMIdxAdmrl** 10.66 +.02 +2.8 +4.4/B +1.8/D +2.5/B TtlnBldxAdmrl 22.29 +.04 +3.1 +5.2/A +3.1/A +4.0/A

Fund Footnotes: b - Fee covering market costs is paid from fund assets. d - Deferred sales charge, or redemption fee. f - front load (sales charges). m - Multiple fees are charged, usually a marketing fee and either a sales or redemption fee. NA - not available. p - previous day's net asset value, s - fund split shares during the week. x - fund paid a distribution during the week. Source: Morningstar and the AF

28.54

17.06

71.87

71.88

71.85

69.90

+.10 +12.9

+.41 +12.9

+.06 +12.8

+.34 +16.4

+.34 +16.4

+.35 +16.3

+.14 +9.8

-4.1/B

-4.3/C

+10.3/B

+10 3/B

+10.2/B

+7.5/A

TtInSldxAdmrl

TtISMIdxAdmrl

TtlnSldxlnv

TtlSMldxIns

TtlSMldxlnv

WingtnAdmrl

TtlnSldxlnsPlus 114.17

Commodities

FUELS	CLOSE	%CHG	%YTD
Crude Oil (bbl)	64.61	+0.98	+42.3
Ethanol (gal)	1.33	+1.45	+5.0
Heating Oil (gal)	2.09	+2.09	+23.8
Natural Gas (mm btu	2.70	+0.04	-8.2
Unleaded Gas (gal)	2.07	+3.51	+56.6
METALS			
Gold (oz)	1309.10	+0.43	+2.4
Silver (oz)	15.20	+0.22	-1.5
Platinum (oz)	903.20	+1.04	+13.5
Copper (lb)	2.92	-0.31	+11.2
Palladium (oz)	1366.40	+0.04	+9.0
AGRICULTURE			
Cattle (lb)	1.26	-0.02	+0.9
Corn (bu)	3.62	+0.49	-3.5
Cotton (lb)	0.78	-0.60	+7.5
Lumber (1,000 bd ft)	342.40	+0.71	446
Orange Juice (lb)	1.09	-0.73	-13.2
Rough Rice (cwt)	10.24	-0.53	+1.4
Soybeans (bu)	9.02	+0.36	+2.2
Sugar 16 (lb)	0.27	+1.47	+6.9
Sugar-World (lb)	0.13	+0.23	+6.5
Wheat (bu)	4.58	-0.33	-9.0
Tag of the San Control	W. 1		

(Change figures reflect current contract.)

Interest Rates

TREASURIES	LAST	PVS	NET CHG	1YR AGO
3-month T-bill	2.41	2.40	+0.01	1.72
6-month T-bill	2.45	2.44	+0.01	1.91
52-wk T-bill	2.40	2.42	-0.02	2.06
2-year T-note	2.32	2.34	-0.02	2.34
5-year T-note	2.27	2.30	-0.03	2.63
7-year T-note	2.37	2.39	-0.02	2.74
10-year T-note	2.47	2.50	-0.03	2.80
30-year T-bond	2.90	2.91	-0.01	3.02

Quotable

"It is not a radical idea to say that in the United States, every American who goes to a doctor should be able to afford the prescription drug he or she needs.

+3.3/B

+3.3/B

+3.2/B

+11.3/A

+113/A

+11.2/A

+9.4/A

+9.3/A

+14.5/A

+14 5/A

+14.4/A

- Sen. Bernie Sanders of Vermont, unveiling a new version of his "Medicare for All" plan.

FROM PAGE A4 · NATION

TRUMP

Continued from A1

The aftershocks from the Mueller report released Thursday will help shape the next two years of Trump's administration. But while the report may cause some Democrats to take a renewed look at impeachment despite long odds of success in Congress, the legal threat to Trump that seemed so dangerous upon Mueller's appointment in May 2017 has waned.

At the outset, that appointment led Trump to predict "the end of my presidency." The White House struggled to recruit top Washington attorneys, many of whom were reluctant to work for a temperamental, scandal-prone president who repeatedly claimed he would be his own best legal mind.

The initial strategy of the Trump legal team, including White House attorney Ty

The legal threat that seemed so dangerous upon Mueller's appointment in May 2017 has waned.

Cobb and personal defense lawyer John Dowd, was to be as cooperative as possible with Mueller's prosecutors and ensure that investigators got access to the documents they requested and the witnesses they wanted to interview. The Trump lawyers hoped to bring about a quick conclusion to the investigation.

Believing he could exonerate himself, Trump initially expressed a willingness to sit for an interview with Mueller's team. A date was set for that to take place at Camp David. But then the president's lawyers moved away from the plan, in part by arguing that the special counsel already

had gotten answers to his

"It became the most transparent investigation in history," Jay Sekulow, one of the president's personal lawyers, said in an interview.

Still, there was internal tumult along the way, including the March 2018 departure of Dowd, a veteran and experienced criminal defense attorney, and the additions of Giuliani and the husbandwife team of Martin and Jane

Even as the legal team professed cooperation with Mueller's prosecutors, the lawyers expressed impatience, frustration and skepticism in a series of private letters that challenged the credibility of the government's witnesses and the demands to interview the president.

Those private complaints were dwarfed by louder public protests. Trump spent months engaging in daily attacks on Mueller's team.

MEDIA

Continued from A1

stronger today than it did before the release of this report," Kyle Pope, editor of the Columbia Journalism Review, said Friday.

Trump's supporters believe that Mueller's determination that there was not enough evidence to show that the president or his team worked with the Russians to influence the 2016 election delegitimizes the attention given to the story.

But the news stories were, for the great part, accurate.

Mueller's report shows The New York Times and The Washington Post were correct when they reported in January 2018 that Trump ordered White House counsel Don McGahn to make sure Mueller was fired, and that Mc-Gahn decided to resign rather than carry that out. When the Times first reported the story,

'The Mueller report confirmed again and again that stories in The New York Times for the past two years were the opposite of "fake news." '

Elisabeth Bumiller Times Washington bureau chief

Trump described it as "fake news, folks, fake news.'

The Mueller report also showed Trump directed aides to ask McGahn to publicly deny the story, and ultimately asked himself, too. McGahn refused, saying the story was

accurate, the report found. In a July 2017 story, the Times reported that the president personally wrote a statement in which he falsely said that an election year meeting between some Russians and his son, Donald Jr., was about the adoption of Russian children, rather than about obtaining potentially damaging information on Hillary Clinton's campaign. Trump's counsel repeatedly responded that the president had no role in writing the statement, yet months later testified under oath to investigators that Trump had dictated it.

Mueller's report also backed up the newspaper's stories, which the administration denied, that Trump demanded loyalty from then-FBI Director James Comey and that Trump had asked Comey to end an investigation into former national security

adviser Michael Flynn. "The Mueller report confirmed again and again that stories in The New York Times for the past two years were the opposite of 'fake news,'" said Elisabeth Bumiller, the paper's Washington bureau chief.

Columbine honors 13 lost on 20th anniversary

By The Associated Press

LITTLETON, Colo. — Community members in suburban Denver marked the 20th anniversary of the Columbine High School shooting Saturday with a remembrance ceremony that celebrated the school's survival and by volunteering at shelters, doing neighborhood cleanup projects and laying flowers and cards at a memorial to the 13 people killed.

"We're changed," Dawn Anna, whose daughter Lauren Townsend was among the students killed in the school's library, said before a crowd of more than 2,000 gathered in a park near the high school. "We're weaker in some places, but hopefully we're stronger in most of them. Our hearts have giant holes in them. But our hearts are bigger than

they were 20 years ago."

The events ended a threeday slate of somber gatherings honoring the victims and lending support to their families, survivors of the April 20, 1999, attack and the school's students and staff.

The decades since have brought similar violence at schools in America, and some survivors and victims' families have found themselves acting as a support system for those affected by other

The afternoon ceremony at times had the feel of a reunion.

Former students introduced classmates to their husbands or wives and the crowd jumped to their feet when a retired teacher led a call-andresponse cheer of the school's

"We are" Ivory Moore



Cassandra Sandusky, right, a graduate of Columbine High School, pauses with her friend, Jennifer Dunmore, at a row of

crosses bearing the names of the victims of the attack.

yelled, his voice straining.

"Columbine!" the crowd replied, some pumping their fists in the air.



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DEATHS

Continued from A4

John Joseph Walsh



John Joseph Walsh, 62, of Lake Charles, passed away at 7 p.m. Friday, April 19, 2019, in his residence.

John was born July 22, 1956, in Houston, Texas, and

was raised in New York, where he graduated from Westhampton Beach High School in 1975. In 1976, he moved with his family to Lake Charles, where he attended Delta Technical College and completed the Technical Drawing program. John shared his cooking abilities as a cook for Granger's and Loggerheads.

He will be remembered as a happy, go lucky, kind soul. John was an avid collector of multiple things including knives, swords, and coins.

Those left to cherish his memory are his mother, Doris Walker Corbello of Lake

NOTICE OF PUBLIC MEETING

Federal Highway Administration (FHWA) will conduct an open-forum public meeting for:

of New River, Ariz., Doris Mercado (Greg) of Basset, Va., Bernice McCown of Lake Charles, Marian Hammer (Charles) of Lake Charles, and Charles Walsh (Marlene) of Lake Charles; and numerous nieces and nephews.

He was preceded in death by both of his fathers, William F. Walsh and Louis Corbello.

Cremation was entrusted to Johnson Funeral Home.

Words of comfort to the family may be expressed at www.johnsonfuneralhome. net.



Charles; siblings, Shirley Tuerlings (Bob) of Shirley. N.Y., William Walsh (Marie)

Notice is hereby given that the Louisiana Department of Transportation and Development (LADOTD) and the

The meeting will be held at the following place and time:

> THURSDAY April 25, 2019 4:00 p.m. to 7:00 p.m.

Ward 4, Recreation District 1 **Multipurpose Complex** 1221 Sampson Street Westlake, LA 70669

The proposed project includes widening and infrastructure improvements to I-10 between the I-210 interchanges, including the Calcasieu River Bridge. The purpose of the proposed project is to improve system connectivity, traffic congestion, roadway and bridge deficiencies, and safety issues

The purpose of the public meeting is to present Preliminary Build Alternative (PBA) 5-G, which was developed by LADOTD in response to comments received after the public meeting held in August 2017. PBA 5-G proposes a fully directional interchange at Sampson Street in Westlake. Details of this PBA will be exhibited along with previously presented PBAs 1-4. Public comments on PBA 5-G and the other proposed PBAs, the alternatives screening process, and the reasonable alternatives recommended for detailed evaluation in the Environmental Impact Statement (EIS) will be accepted at the meeting.

Persons interested in the proposed project are invited to attend the meeting in person to review the informational materials and comment on the information presented. The meeting will be held in an open-house format and a slide presentation will be repeated continuously between 4:00 p.m. and 7:00 p.m. Project team members will be available to explain the exhibits and answer questions.

If you are unable to attend the meeting, you may attend a virtual public meeting by visiting the project website at https://www.i10lakecharles.com/. The presentation and other informational materials will be available for viewing at the virtual meeting link from April 18 until May 5, 2019. Comments and questions may be submitted, and questions answered during this time.

All comments received at the public meeting, through the virtual meeting link, and written comments mailed to the address below and post-marked by May 5, 2018 will become part of the meeting record.

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe, Suite 640 Baton Rouge, LA 70810

State Project No. H.003931

Federal Aid Project No. BR-10-1(212)29

I-10 Calcasieu River Bridge

(I-10/I-210 West End to I-10/I-210 East End)

Calcasieu Parish, Louisiana

to participate in this meeting, please contact Ms. Lynn Maloney-Mujica with HNTB Corporation at least five (5) working days prior to the meeting date by email at maloneymujica@hntb.com, by phone at (225) 368-2826, or by mail to HNTB Corporation, 10000 Perkins Rowe,

If you require special assistance due to a disability or require an interpreter Baton Rouge, LA 70810.



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Appendix A-4: Press Release



FOR IMMEDIATE RELEASE

April 18, 2019

Contact: Lynn Maloney-Mujica

Project Manager HNTB Corporation 225-368-2800

I-10 Calcasieu River Bridge Project Public Meeting #3 Announced

Louisiana Department of Transportation and Development to Hold a Public Meeting for

I-10 Calcasieu River Bridge Project

Westlake, La – The Louisiana Department of Transportation and Development (LADOTD) and the Federal Highway Administration (FHWA) will be hosting an open-forum public meeting on April 25, 2019 for State Project No. H.003931, the I-10 Calcasieu River Bridge Project. The purpose of the proposed project is to improve system connectivity, traffic congestion, roadway and bridge deficiencies, and safety concerns.

The purpose of the public meeting is to present Preliminary Build Alternative (PBA) 5-G, which was developed by LADOTD in response to comments received after the public meeting held in August 2017. PBA 5-G proposes a fully directional interchange at Sampson Street in Westlake. Details of this PBA will be exhibited along with previously presented PBAs 1-4.

Public comments on PBA 5-G and the other proposed PBAs, the alternatives screening process, and the reasonable alternatives recommended for detailed evaluation in the Environmental Impact Statement (EIS) will be accepted at the meeting. The public is invited to attend the meeting any time between 4:00 and 7:00 pm. A slide presentation that describes the project will be repeated continuously and project team members will be available to explain exhibits and answer questions.

The date, location, and time for the public meeting is:

Thursday, April 25th, 2019, 4:00-7:00 p.m. Recreation District 1, Multipurpose Complex 1221 Sampson Street Westlake, LA Anyone unable to attend the meeting in person may attend a virtual public meeting by visiting the project website at https://www.i10lakecharles.com/. The presentation and other informational materials will be available for viewing at the virtual meeting link until May 5, 2019.

All comments received at the public meeting, through the virtual meeting link, and written comments mailed and postmarked by May 5, 2019, will become a part of the meeting record. For more information on the I-10 Calcasieu River Bridge Project Meeting, please contact Lynn Maloney-Mujica with HNTB Corporation at (225) 368-2800.

Appendix B: Meeting Sign in Sheets

Appendix B-1: Public Sign-In



I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End) Thursday, April 25, 2019 | 4:00 pm – 7:00 p.m.

NAME	The part of the part		1221 Sampson Street Westlake, LA 70669
VICTUR GANCHEZ	ORGANIZATION	EMAIL or MAILING ADDRESS (For Project Undates)	70009
OI PANCHEZ	MGP USA		
Chris Ray	WSD USA		
Robby OQuin	Lake Chicks :		
BART RYALLIPZAC	LAKE CHARACE CO		
Churk Klackley	0 11		
GRORGE SWIFT	Swia Alliene		
James Evanner	Lund Ind S		
HAR MCMillin	CAL PARSH PJ		
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like Johnyn,	11 1 2		
Tich Clintman	Mothet Lalles &		
les tarnum	CPPT		
Cade Harrison	Resident		



I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End) Thursday, April 25, 2019 | 4:00 pm = 7:00 p.m.

NAME	ORGANIZATION	EMAIL or MAILING ADDRESS (For Project Updates)	
Patrick HETATIVE	Oats & Marino	The Project Openes)	
Vine PRADERIO	Massiary		
RICHARD HEBERT			
SHIRLEY HERERT			
Butch Babineaux	Fenstermaker		
ROB TRICE JE	DUNHAM PRICE		
Mike Paul	TRC		
LENN KNAPP			
HAN THOOP			
ION CLAYTE	TRE		
mke Fruger			
Land nunt		/4	
Bobby Letard	NCCH Board		
97 Brandon Jones	Carplete Streets		
H Cheno,			
Tom Varnick	TERROCCA		
Jim Rock	I-10 Tage FOOCE		
ERICH TINES	KIEWIT		
Lenda Vincent	LRC 1		
Perry Vincent	Wireless		
Jeannie Weize	Alliana for		
	Positive		
	Growth		Dans



I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End) Thursday, April 25, 2019 | 4:00 pm - 7:00 p.m.

NAME	ORGANIZATION	EMAIL or MAILING ADDRESS (For Project Undates)	7.00
Michael Tritico	RESTO RE	ADDRESS (For Project Undates)	
JADE RUNG	Rung & Accoc / FX		
Josh. Dog. 4582	WLPD	7	
Ju Moore	WLPD	Transition of the Control of the Con	
Clara Payne	9/11/15		
Michael Bergeron	-WH		
JASON BOAZ	ISLE - CAPRI		
BOB SCHMINT	HUVAL		
Rundy Bondgean	LA Scrip Meta		
Cande De serte	each for		
Jay Comeaux	Westlake um		
Edwin Dellouen	Elizar		
Harlie HHertun	Public /LEPC		
Denise & Fred Rau	Citizen		
Denielle Richardson	CHIZEN	C	



I-10 Calcasieu River Bridge Project (1-10/1-210 West End to I-10/1-210 East End) Thursday, April 25, 2019 | 4:00 pm - 7:00 p.m.

NAME	ORGANIZATION	EMAIL or MAILING ADDRESS (For Project Updates)
Byran Racra	Meyer + Assoc.	The state of Appreciation of the Project Optates)
Byran Racra Chadyn States	-	
Tommie States		
DAVID TRHYE		
Zhenga.		
ole.		

Appendix B-2: Elected Official/Agency and Media Sign-In

10 LAKE CHARLES

PROJECT BRIEFING

ELECTED AND LOCAL OFFICIALS SIGN-IN

I-10 Calcasieu River Bridge Project (1-10/1-210 West End to I-10/I-210 East End) Thursday, April 25, 2019 | 2:30 pm - 3:30 p.m.

NAME	TITLE	ORGANIZATION	EMAIL ADDRESS
3 HARDEY	MAJON	City of Westlake	MAYOR OCHYOTHER HAKE, con
Dan Raceg		en City of Wastlah	
Stacy Dowden	AN Director	CO Sulphun	sdowden Dsulphun.org
Coby Sammir	Gold Reg	Cong. Higgins	Cosy. Sammis@house, mail. gov
1		1	/
	1/4		



PROJECT BRIEFING

I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End) Thursday, April 25, 2019 | 2:30 pm - 3:30 p.m.

AGENCY SIGN-IN

NAME	TITLE	AGENCY	EMAIL ADDRESS
Della Hoffpanii		City of us	LAIAIL AUDRESS
Walter Kennis	Respond Constrol	Cours 3	1 15-11
1M.14 - V - 1	Tarallo	MAN	Hector Villasceal & Lit. GOV
MATHA MANCH	1 symposterite 11 116	* IMCAL	walters)mcul.la

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1 byte 1	



PUBLIC MEETING #3

I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End) Thursday, April 25, 2019 | 4:00 pm - 7:00 p.m.



NAME	TITLE	ORGANIZATION	EMAIL ADDRESS	PHONE NUMBER
Drew Marine	reporter	KPLC	dmarine@kplcTv.com	337-377-6281
Vallisa Hordin	x lepoll	Hunerices Pross	whateling americanpres con	494-4062
()		5	

Appendix B-3 Project Team Sign-In



PROJECT BRIEFING

I-10 Calcasieu River Bridge Project (1-10/1-210 West End to I-10/1-210 East End) Thursday, April 25, 2019 | 2:30 pm - 3:30 p.m.

PROJECT TEAM / OTHER SIGN-IN

NAME / / /	ORGANIZATION	EMAIL ADDRESS
KICK HATTHAWMY	HNTB	rihathawaye hutbeen
Courmey Blean 12'	HNTB	chiechiechnib com
Saithin Hathamay	HNTB	celeathaway@hutto.com
Elizabeth Bauman	HNTB	ebauman@hntb.com
Meredith Taylor	HNTB	metaylor @ hotb.com
JOHN BASILICA	HNTB	JAASILICA @ HNTB. COM
MARIO DURBIR	INVTB.	modurace harb.com
Kate Pryson	HNIE	Lyprejean @ hutb.com
Aravind Tankasala	HNTB	1 1
Branan Stein	HNTB	
Ed FIKE	CEL	efike accostalence com.
Sara Ham	CEI	shahn@constatenocon
Dusty Bastim	HNTB	
ynn Maloney	HNTB	
Joubant Harris	KOTA	Joubert Harris e La Grav
Nicholas Latks	DOID	Nicholas. Lacks @ la gov
BRYLONHARRIS	DOTD	PORAYLON, HARRIS W.CA. GOV
BOB MAHONEY	FHUTA	Robert. mahon=y@ dot.gov
Tanymy york	DOTO	tammy york & la.gov
Dan Duberville	D070	don. duberville @ la.gov
Noel Andoin	DOTA	nucleandon als. gov.



PROJECT BRIEFING

PROJECT TEAM / OTHER SIGN-IN

I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End) Thursday, April 25, 2019 | 2:30 pm – 3:30 p.m.

NAME	ORGANIZATION	EMAIL ADDRESS
Tim Mickel	LADOTIS	
Joochim Uneozulu	40000	Joachim umerzuli els for
Enc Kelwah	LA DOTO	200 K-10 1 CV
John Erwin Marc Hoffmann	HNTB	
Marc Hoffmann	HWTB	mhotfmane HATB.com



PUBLIC MEETING #3

PROJECT TEAM SIGN-IN

I-10 Calcasieu River Bridge Project (1-10/I-210 West End to I-10/I-210 East End) Thursday, April 25, 2019 | 4:00 pm – 7:00 p.m.

KAME	ORGANIZATION	EMAIL ADDRESS
Richard Duchamp	Seidd Palstate-Agens	richard duchampa LA. 60V
Kelly Kemp	DOTO Sec 25	Kelly, Kemp (LA. GOV
Zhengzheng (Jenny) Fu	×7-	Zhenazhera. Lu @ local
Keny Kemp Zhengzheng (Jenny) Fu Syra Juli	HUM	Zhengzheng. fu@ laigou bryznjung a hoth. in
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Appendix C: Meeting Materials

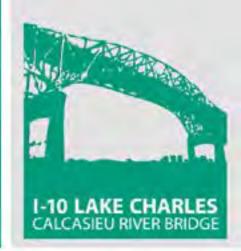
Appendix C-1: Presentation

I-10 IMPROVEMENTS

I-10/I-210 WESTEND - I-10/I-210 EAST END INTERCHANGES

PUBLIC MEETING

April 25, 2019



Purpose of the Public Meeting



Present, answer questions, and solicit public comment on:

Proposed Preliminary Alternatives

Alternatives Screening Process

Screening Results

A New Alternative PBA 5G

Project Overview





Design details along entire project corridor are presented at Stations 6 and 7

Purpose & Need

1-10 LAKE CHARLES

3 Lanes

Traffic Bottleneck

- 1. Inadequate System Connectivity
- 2. Increased Traffic Congestion
- 3. Roadway and Bridge Deficiencies
- 4. Roadway and Bridge Safety Concerns











See Station 3 for Purpose and Need Details

Environmental Impact Statement (EIS)



Studies range of reasonable alternatives

Demonstrates compliance with environmental laws

Provides a means public input into the decision-making process

See Station 3 for EIS timeline

Section 106 of the National Historic Preservation Act



- Consider effects of Federal undertakings on historic properties
- Occurs along with EIS process
- Calcasieu River Bridge eligible for the National Register of Historic Places



- Bridge evaluated in accordance with the Programmatic Agreement (PA) for historic bridges
- See Station 4 for Section 106 and PA details

Preliminary Alternatives





No Build

Future conditions if the project were not constructed. Existing conditions plus committed projects.



Transportation Systems Management (TSM)

Promoting efficiency through improvements to existing infrastructure. Includes intersection improvements, turn prohibitions, traffic control improvements, signal improvements/synchronization, etc.



Transportation Demand Management (TDM)

Alternatives to driving. Includes public transit, rideshare promotion, telecommuting, flexible work hours, establishing park and ride facilities, etc.



High Occupancy Vehicle Lanes (HOV)

Roadway lane(s) reserved for 2 or more persons, by busses, and vanpools.



Preliminary Build Alternatives (PBA)

Five PBAs with seven different Sampson St. Sub Alternatives. See Station 6 for details.

Development of Preliminary Build Alternatives



- Feasibility Study that evaluated several build alternatives and bridge rehabilitation
- Multiple marine use/bridge height studies
- Public and agency coordination
- Discovery of ethylene di-chloride (EDC) contamination near I-10/Sampson St. interchange.





PBAs 1, 2, 3 and 5

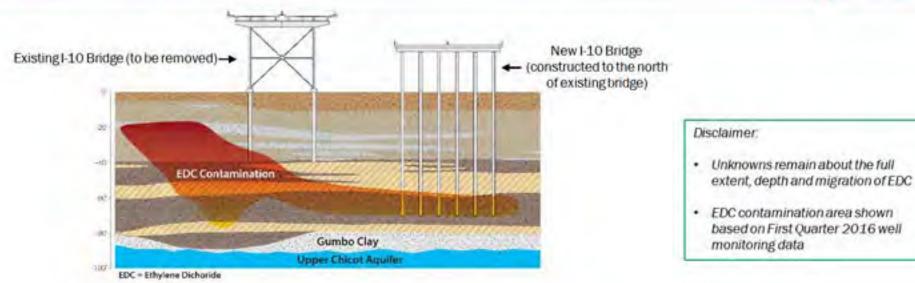


Bridge replacement immediately north of existing bridge



PBA 1 | Driven Piles

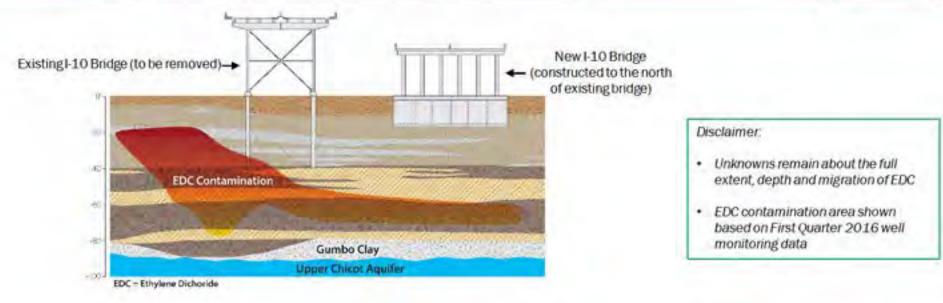






PBA 2 | Compensated Foundation







PBA 3 | Long Span Bridge



Long-Span Bridge Examples











- Bridge replacement south of existing I-10
- Avoids construction in EDC area
- 2 new bridge crossings over Bayou Contraband







Mechanically Stabilized Earth (MSE) Retaining Wall Examples







Sampson Street



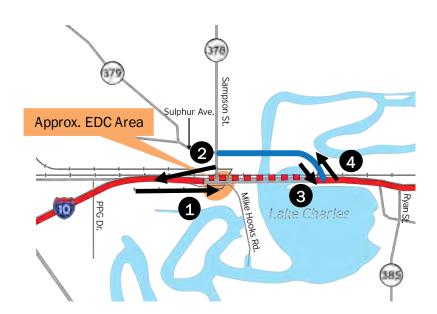
- Multiple trains a day block access to/from I-10
- Elevating Sampson Street above railroads requires driving piles in EDC area
- To avoid/minimize risk, the project team developed technical solutions
- Options to circumvent at-grade railroad crossings Sub-Alternatives A E



Sub Alt A



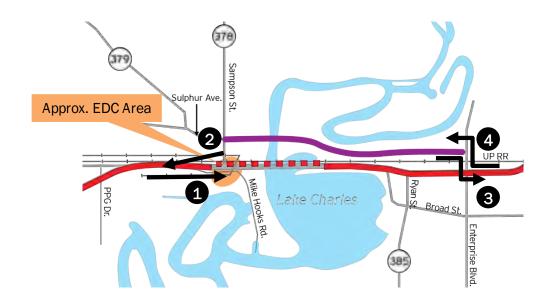
- Sulphur Ave. Extension to I-10 West of Ryan St.
 - 1. EB I-10 exit ramp to Sampson St.
 - 2. WB I-10 entrance ramp from Sampson St.
 - 3. EB I-10 entrance ramp along Sulphur Ave. extension from Sampson St.
 - 4. WB I-10 exit ramp along Sulphur Ave. extension to Sampson St.



Sub Alt B



- Sulphur Ave. extension to Enterprise Blvd.
 - 1. EB I-10 exit ramp to Sampson St.
 - 2. WB I-10 entrance ramp from Sampson St.
 - 3. EB I-10 entrance ramp at Enterprise Blvd. along Sulphur Ave. extension from Sampson St.
 - 4. WB I-10 exit ramp at Enterprise Blvd. along Sulphur Ave. extension to Sampson St.



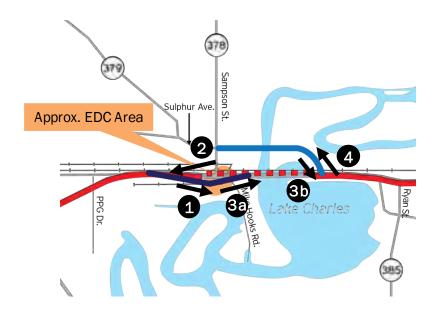
Sub Alt C



Sulphur Ave. extension to I-10 west of Ryan St.

Intersection improvements at Sampson St. south of I-10

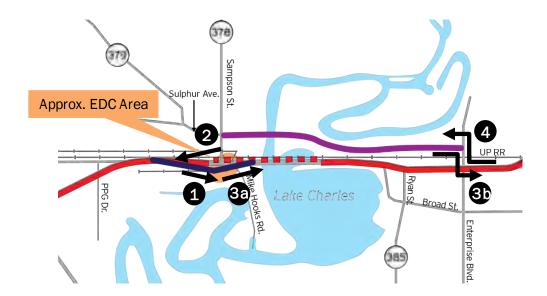
- 1. EB I-10 exit ramp to Sampson St.
- 2. WB I-10 entrance ramp from Sampson St.
- 3. EB I-10 entrance ramp access:
 - a. from Sampson St.
 - b. along Sulphur Ave. extension from Sampson St.
- 4. WB I-10 exit ramp along Sulphur Ave. extension to Sampson St.



Sub Alt D



- Sulphur Ave. extension to Enterprise Blvd.
- Intersection improvements at Sampson St. south of I-10
 - 1. EB I-10 exit ramp to Sampson St.
 - 2. WB I-10 entrance ramp from Sampson St.
 - 3. EB I-10 entrance ramp access:
 - a. from Sampson St.
 - b. along Sulphur Ave. extension to Enterprise Blvd. from Sampson St.
 - 4. WB I-10 exit ramp from Enterprise Blvd. along Sulphur Ave. extension to Sampson St.

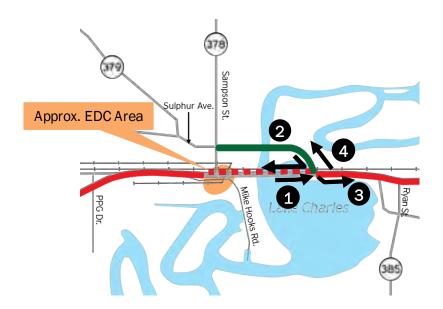


Sub Alt E



- Sulphur Ave. extension to fully directional, elevated interchange to I-10 west of Ryan St.
 - 1. EB I-10 exit ramp along Sulphur Ave. extension to Sampson St.
 - 2. WB I-10 entrance ramp along Sulphur Ave. extension from Sampson St.
 - 3. EB I-10 entrance ramp along Sulphur Ave. extension from Sampson St.
 - 4. WB I-10 exit ramp along Sulphur Ave. extension to Sampson St.

Note: No direct access to/from I-10 at Sampson St.



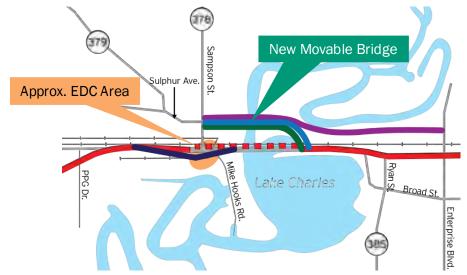
Sub Alts A-E



Movable Bridge Examples



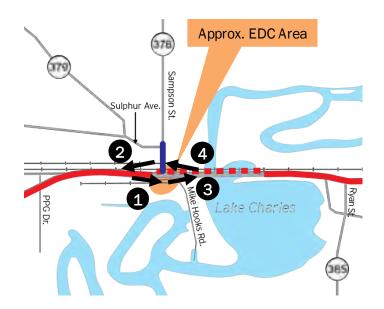








- Fully directional, elevated interchange over at-grade railroad tracks
- Drives piles in EDC area
 - 1. EB I-10 exit ramp at Sampson St.
 - 2. WB I-10 entrance ramp at Sampson St.
 - 3. EB I-10 entrance ramp at Sampson St.
 - 4. WB I-10 exit ramp at Sampson St.

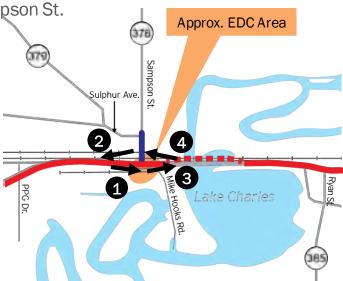


Sub Alt G



- Fully directional Sampson Street interchange is elevated over I-10 mainline connecting to the bridge on the east and to the at-grade roadway on the west
- Mechanically Stabilized Earth (MSE) retaining wall for bridge in area of EDC with shallow foundations; utilizes existing piles from I-10 mainline for some ramp movements
 - 1. EB I-10 exit ramp at Sampson St.
 - 2. WB I-10 entrance ramp at Sampson St.
 - 3. EB I-10 entrance ramp at Sampson St.

4. WB I-10 exit ramp at Sampson St.



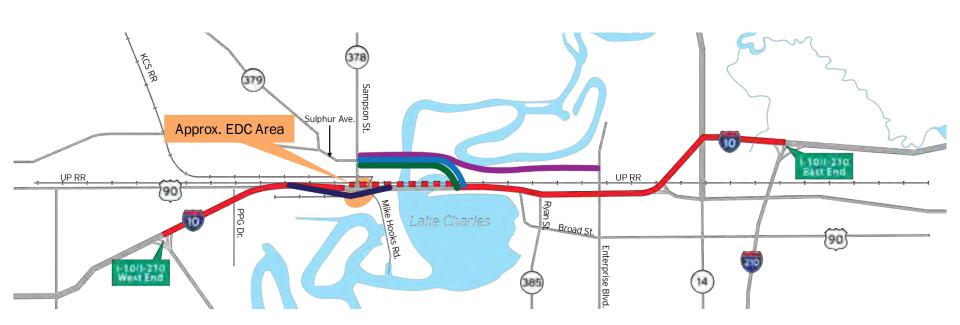
PBA 1 | Sub Alt F





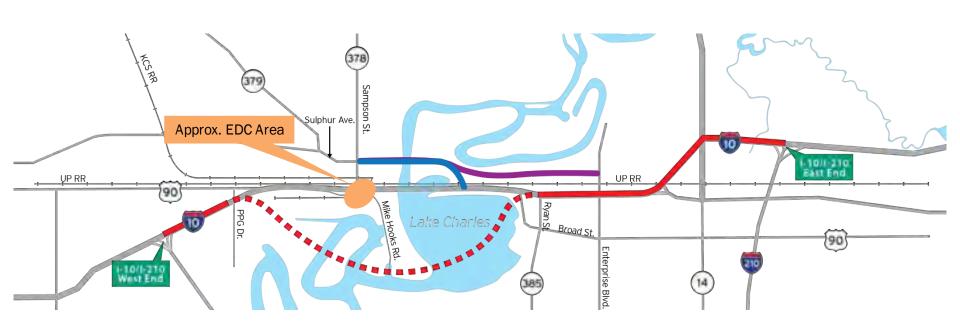






PBA 4 | Sub Alts A-B





PBA 5 | Sub Alt G

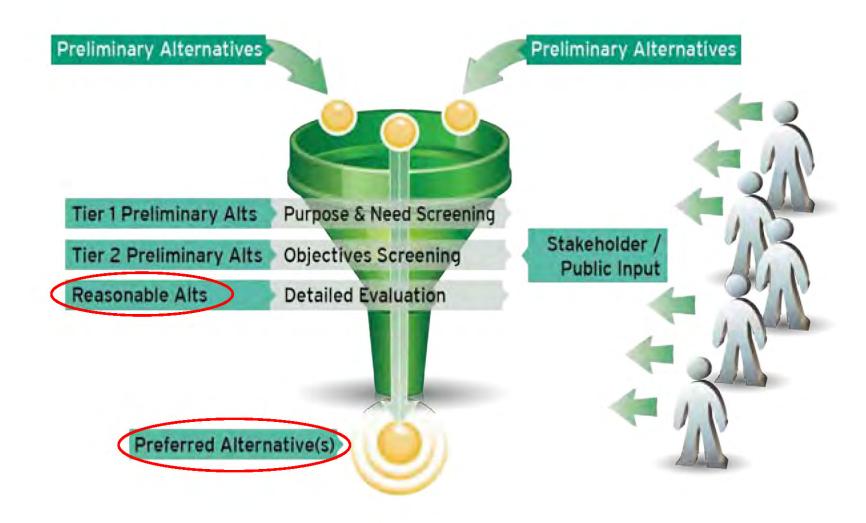




See Stations 6 and 7 for additional details on the PBAs and Sampson St. Sub-Alternatives

Alternatives Screening Process









Does the Preliminary Alternative:

- Address system connectivity?
- Improve congestion?
- 3. Improve roadway and bridge deficiencies?
- Improve roadway and bridge safety?

Alternatives fail to meet the Purpose & Need = No further study



Alternatives meet the Purpose & Need = Move to Tier 2 Objectives Screening







- Engineering, Cost, Environmental and Public & Agency Input Objectives
- Impacts matrix presents impacts side by side for all alternatives

-	
FXA	mple:
	TIPIOT

	Objectives		
Alternative	Minimize ROW Impacts (Acres)	Optimize Construction Cost (\$)	Avoid/Minimize Impacts to Natural Resources (Acres)
Alt 1			
Alt 2			
Alt 3			

Example:

	Minimize ROW Impacts		
√	LOW	1-10 acres	
	MEDIUM	10-20 acres	
	HIGH	20+acres	

 Recommendation of Reasonable Alternatives based on professional judgement with consideration given to ALL project objectives

See Station 8 for screening results & recommended Reasonable Alternatives

Project Funding Options



- Highway Priority Program
- Federal Discretionary Grants
- State General Obligation Bonds
- State General Fund Appropriation
- Contributions from Federal or Other State Agencies
- Contributions from Local Government(s)
- Private Contributions
- Tolls or other Revenue Generating Fees

We Want to Hear From You!



- Please review materials at the public meeting stations
- Please provide comments at Station 9
- Comments on project received in 10 calendar days from tonight's meeting will become part of the transcript
- Comments post-marked by May 5, 2019 will become part of the public meeting transcript
- Comments may also be submitted by attending a Virtual Public Meeting at: www.i10lakecharles.com

Thank You!



Thank you for your interest in the I-10 Improvements

(I-10/I-210 west end to I-10/I-210 east end)



Appendix C-2: Meeting Handouts

I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE I-10/1-210 WEST END - I-10/1-210 EAST END State Project No. H.003931





Public Meeting #3

THURSDAY, APRIL 25, 2019 4:00 p.m. - 7:00 p.m.

Ward 4, Recreation District 1, Multipurpose Complex 1221 Sampson Street, Westlake, LA 70669

Welcome! Thank you for attending today's public meeting.

- Sign in
- View presentation
- Visit each station
- Ask questions
- · Provide comments

The purpose of the meeting is to provide an opportunity to gather information and provide comments on the following:

- Preliminary Alternatives
- Alternatives Screening Methodology
- Screening Results
- Reasonable Alternatives Recommended for Further Analysis in Environmental Impact Statement (EIS)

Station Checklist

Station 1 - Welcome & Sign-In

Station 2 - Presentation

Station 3 - Project Overview

» Study Area, Purpose & Need, EIS

Station 4 - Environmental

» Constraints Maps, Section 106

Station 5 – Preliminary Alternatives & Screening Process

Station 6 – Features of the Preliminary Build Alternatives

Station 7 - Schematics

Station 8 - Screening Results

Station 9 - We Want to Hear from You!

Public meeting materials distributed tonight are also available at the project website www.i10lakecharles.com

Ways to Comment

Please provide written comments on the comment form and return completed forms at the comment table. Comments will also be accepted by:

- U.S. Mail at: I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe, Suite 640 Baton Rouge, LA 70810
- Logging on to www.i10lakecharles.com and attending Virtual Public Meeting #3
- Verbally at tonight's public meeting

Comments on the project will be accepted for 10 days after this public meeting.

Comments received or postmarked by MAY 5 will become part of the public meeting record.

I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE I-10/1-210 WEST END - I-10/1-210 EAST END State Project No. H.003931

What is the Project and Why is it Needed?

The proposed project includes improvements to I-10 between the I-10/I-210 east and west interchanges in the Lake Charles region, including the Calcasieu River Bridge, a distance of approximately 9 miles. The project is needed to address the following four needs:

1. Inadequate System Connectivity

I-10 outside the project limits is three lanes in each direction, which reduces to two lanes in each direction within the project limits. The lane reduction can result in traffic bottlenecks that in turn decrease traffic operations and reduce the amount of space for motorists to maneuver.

2. Increased Traffic Congestion

The number of vehicles traveling on the Calcasieu River Bridge in the future project design year (2040) is anticipated to exceed the bridge's capacity by more than 37,000 vehicles per day.

3. Roadway and Bridge Deficiencies

The Calcasieu River Bridge has existing structural integrity issues such as corrosion, cracking of the bridge deck, and an inadequate load limit for an interstate highway. Functional deficiencies along the facility include steep bridge approach grades, no shoulders on the bridge, and I-10 entrance and exit ramp spacing and weaving distances that do not meet current design guidelines.

4. Roadway and Bridge Safety Concerns

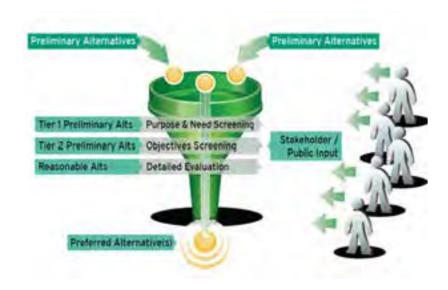
Conflict points create safety hazards along I-10 and at the Sampson Street at-grade railroad crossings. The steep bridge grades slow traffic on the up-slope and make it more difficult to stop on the down-slope and the low vertical clearance of the bridge has led to over-height vehicle collisions with the bridge trusses.

	Estima	Estimated EIS Timeline		
Public Meeting	Prepare Draft EIS		Public Hearing	and ROD Approval
Summer 2018 Fall 2018 Winter 2018 Spring 201	9 Summer 2019 Fall 2019 W	/inter 2019 Spring 2020 Summer 2	020 Fall 2020 Win	nter 2020 Spring 2021
Development of Additional Alternative and Revised Screening Process	Finalize Reasonable Alternatives	Recommended Preferred Alternative	Prepare Fir	nal EIS

Preliminary Alternatives

Preliminary Alternatives under evaluation include:

- No-Build Alternative
 Includes existing conditions plus committed projects
- Transportation Systems Management Alternative
 Examples: intersection and traffic control improvements
- Transportation Demand Management Alternative Examples: public transit and rideshare promotion
- High Occupancy Vehicle Alternative
 Lanes reserved for use by 2 or persons in a vehicle
- Five Preliminary Build Alternatives (PBA)
 Replacement of the Calcasieu River Bridge and seven different Sampson St. Sub-Alternatives



Alternatives Screening Process

The Preliminary Alternatives will undergo a two-tiered screening process. Tier 1 will evaluate the ability of the Preliminary Alternatives to meet the purpose and need of the Project. Tier 2 will evaluate the ability of the remaining Preliminary Alternatives to meet the objectives of the project. The alternatives remaining at the end of this screening are the Reasonable Alternatives, which will be evaluated in detail within the EIS, leading to the recommendation of a Preferred Alternative.

PROJECT FEATURES





Project Limits = I-10 from I-10/I-210 West End Interchange to I-10/I-210 East End Interchange

Preliminary Build Alternatives (PBA)

Project includes five Proposed Preliminary Build Alternatives (PBA)

- PBA 1, PBA 2, and PBA 3 all include a new Calcasieu River Bridge constructed immediately north of existing bridge and ending west of the EDC area (Figure A)
- PBA 4 includes a new Calcasieu River Bridge constructed south of the existing bridge with two new bridge crossings of Bayou Contraband (Figure B)
- PBA 5 (a new alternative) includes a new Calcasieu River Bridge constructed immediately north of existing bridge and ending east of the EDC area (Figure C)

<u>Differences in Construction in Ethylene Di-Chloride (EDC) Contamination Area</u>

- PBA 1 = Driven piles in EDC contamination area
- PBA 2 = Compensated Foundation above EDC contamination depth
- PBA 3 = Long-Span Bridge over EDC contamination area
- PBA 4 = Avoids construction in EDC contamination area
- PBA 5 = new bridge ending at the eastern boundary of the EDC contamination area.

All PBAs include the following improvements along I-10 between the project limits (Figure D):

- 1. Proposed widening of I-10 to six, 12-foot lanes (three in each direction) with 12-foot shoulders
- 2. Proposed replacement of I-10 EB to I-210 SB ramp bridge
- 3. Proposed 6-lane overpass at PPG Dr.
- 4. Proposed replacement/improvement of US 90 overpass to allow I-10 to be widened
- 5. Improve Sampson St. access to I-10
- 6. Proposed 6-lane overpasses to improve vertical clearance & new U-Turns under the overpasses at the following locations: Veterans Memorial Blvd., Ryan St., Bilbo St., Kirkman St., Enterprise Blvd., Shattuck St., Railroad Crossing, and Opelousas St.
- 7. Proposed improvements to US 171 overpass to allow I-10 to be widened and improve vertical clearance

Figure A: PBAs 1, 2, 3 & 5

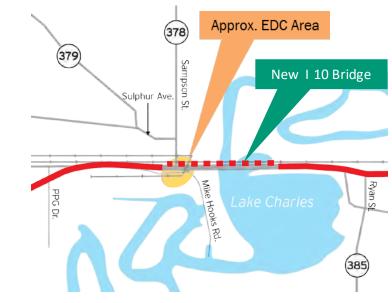


Figure B: PBA 4

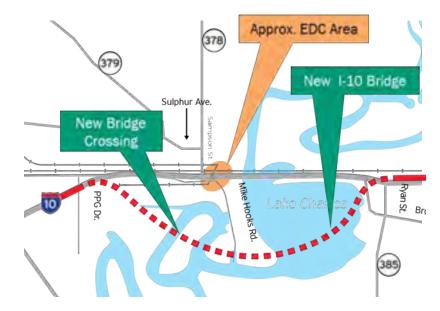


Figure C: PBA 5







PROJECT FEATURES





Project Limits = I-10 from I-10/I-210 West End Interchange to I-10/I-210 East End Interchange

Sampson St. Sub-Alternatives (Sub-Alts)

Project includes seven proposed Sampson St. Sub-Alts, labeled A-F

- Sub-Alt A = Sulphur Ave. extension to West of Ryan St.
- Sub-Alt B = Sulphur Ave. extension to Enterprise Blvd.
- Sub-Alt C = Sulphur Ave. extension to West of Ryan St. & intersection improvements at Sampson St. south of I-10
- Sub-Alt D = Sulphur Ave. extension to Enterprise Blvd. & intersection improvements at Sampson St. south of I-10
- Sub-Alt E = Sulphur Ave. extension to fully directional, elevated interchange to I-10 west of Ryan St.
- Sub-Alt F = Fully directional, elevated interchange over Sampson St. at-grade railroad tracks
- Sub-Alt G = Fully directional, elevated interchange over Sampson St. at-grade, brings the main bridge to grade sooner, relocation of pipe racks and railroad spurs.
- Sub-Alts A-E all include an eastward extension of Sulphur Ave. over the Calcasieu River that would require a new moveable bridge (Figures E & F)
- Sub-Alts A-E avoid driving piles in the EDC contamination area
- Sub-Alt F requires driving piles in the EDC contamination area;
- Sub-Alt G requires a retaining wall on top of the EDC contamination area.
- PBA 1, 2, 3, 4, & 5 include one or more Sampson St. Sub-Alts
 - PBA 1 is paired with Sub-Alt F;
 - PBA 2 and PBA 3 are paired with Sub-Alts A-E;
 - PBA 4 is paired with Sub-Alts A & B;

Figure E: Sulphur Ave. Extension Moveable Bridge

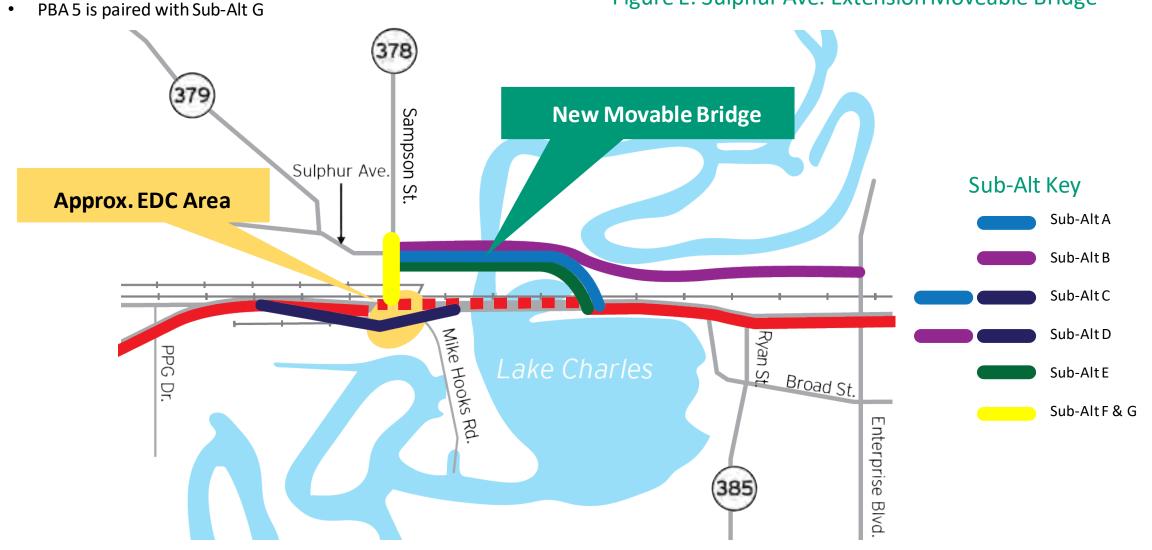


Figure F: Moveable Bridge Examples







I-10 Calcasieu River Bridge Project

(I-10/I-210 West End to I-10/I-210 East End)
State Project No. H.003931

Public Meeting Comment Form

Please provide your comments on the following items:

- 1. Preliminary Build Alternative (PBA) 5-G and other Proposed Preliminary Build Alternatives
- 2. Alternatives Screening Methodology and Results
- 3. Recommended Reasonable Alternatives for further evaluation in the Environmental Impact Statement

The Recommended Reasonable Alternatives are as follows:

- Preliminary Build Alternatives 2-A & 2-E
- Preliminary Build Alternatives 3-A & 3-E
- Preliminary Build Alternative 5-G

(Continued on Back)

Please return this completed form to **Station 9** or to a Project Team member.

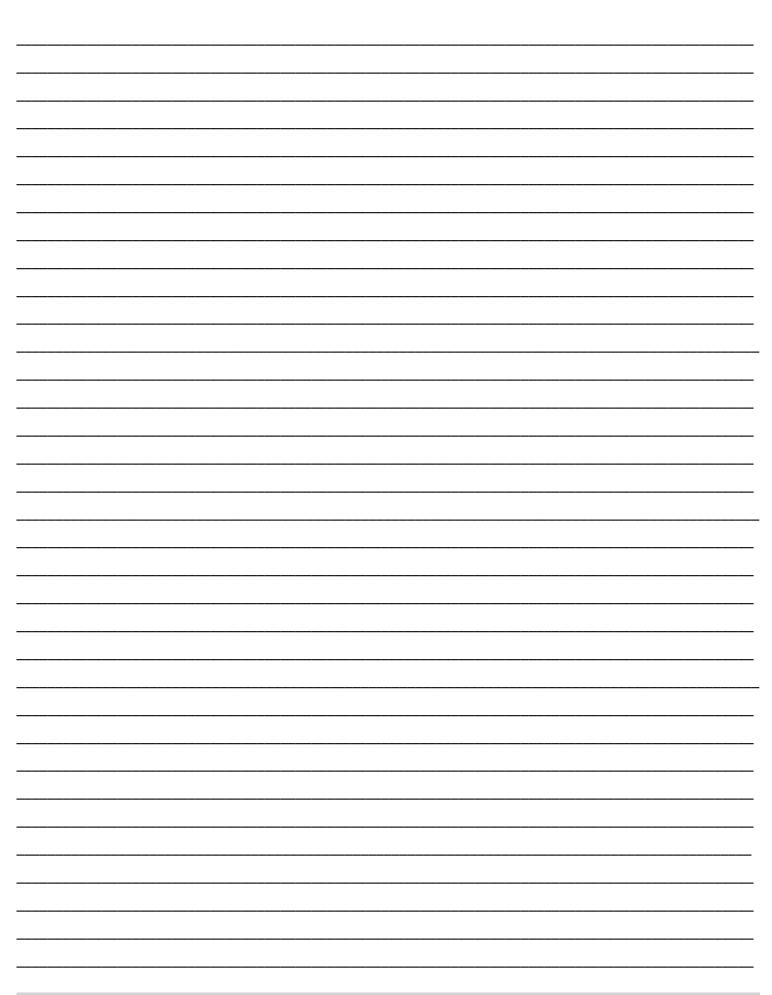
You can also submit comments online at www.i10lakecharles.com or by U.S. mail to the following address:

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe Suite 640 Baton Rouge, LA 70810

Comments on the project will be accepted for 10 days following this public meeting.

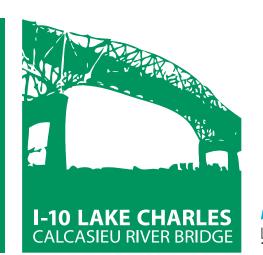
NOTE: All comments received at the public meeting, through the virtual meeting link, and written comments mailed to the address below and post-marked by May 5, 2018, will become part of the meeting record.

Please Print
Name:
Address:
Email:
Agency (if applicable):
Would you like to receive future updates on the project? Yes or No (circle one)
Comments:

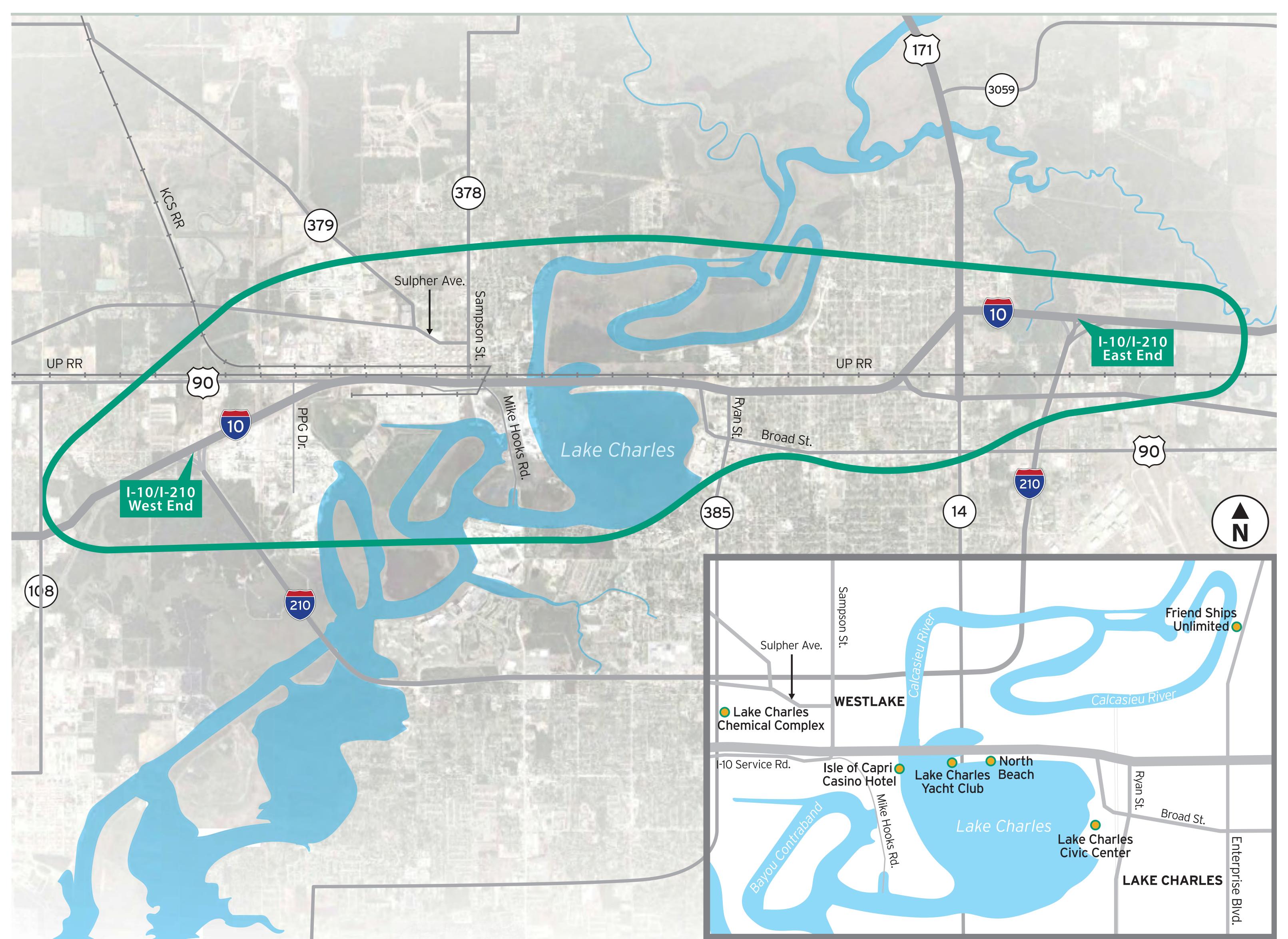


Appendix C-3: Station 3 Exhibits

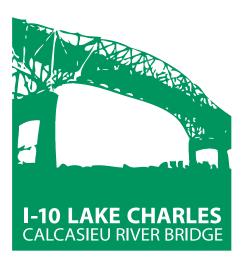
I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE
I-10/1-210 WEST END - I-10/1-210 EAST END
State Project No. H.003931







I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE I-10/1-210 WEST END - I-10/1-210 EAST END State Project No. H.003931





NEED

(Problems)

Lack of System Connectivity

I-10 outside the I-10/I-210 east and west interchanges (project limits) is three lanes in each direction, which reduces to two lanes in each direction within the project limits, causing traffic bottlenecks.

Increased Traffic Congestion

It is anticipated that in the future project design year of 2040, the number of vehicles traveling per day on the Calcasieu River Bridge will exceed the bridges capacity by more than 37,000 vehicles per day.

Roadway and Bridge Defigencies

Calcasieu River Bridge structural integrity issues such as corrosion and cracking of the bridge deck. Bridge approach grades, the vertical clearance above the bridge, and shoulder widths do not meet current design guidelines.

Roadway and Bridge Safety Concerns

Crossing, merging, and diverging conflict points create safety hazards along I-10 and at the Sampson Street at-grade railroad crossings. Roadway and bridge deficiencies also create safety hazards, such as the steep bridge grades that slow traffic on the up-slope and make it more difficult to stop on the down-slope.

PURPOSE

(Solutions)

Improve System Connectivity

By providing a consistent number of through lanes both within and outside the I-10/I-210 interchanges.

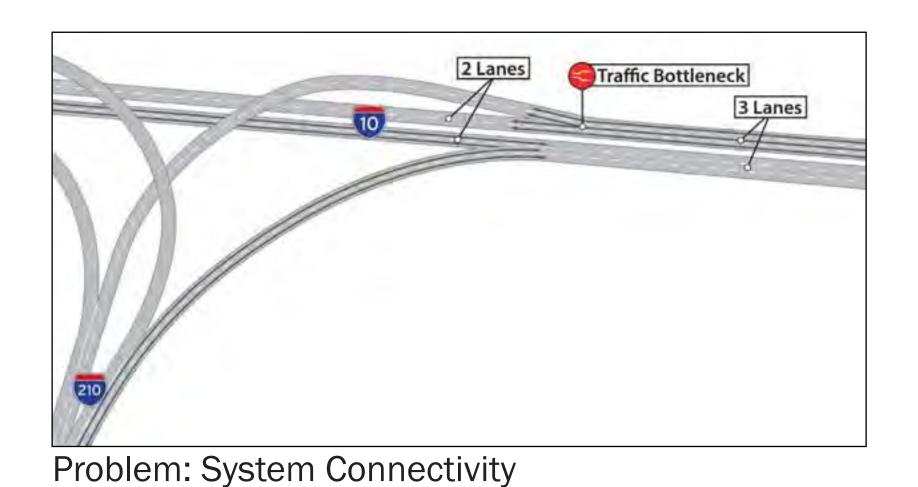
Reduce Traffic Congestion

By providing additional infrastructure to I-10, including improvements to the Calcasieu River Bridge, to accommodate growth and aid in congestion relief.

Improve Roadway Deficiencies
By providing infrastructure improvements that remedy structural integrity issues and improve the existing facility to meet current design guidelines.

Improve Roadway and Bridge Safety

By providing infrastructure improvements to I-10, the Calcasieu River Bridge, and Sampson Street that improve/reduce existing points of conflict and improve functional deficiencies that create safety hazards for motorists.



Problem: Steep Bridge Grade



Problem: Low Vertical Clearance



Problem: Cracking of Bridge Truss

EIS PROCESS AND TIMELINE





What is an EIS?

An Environmental Impact Statement (EIS) is a full-disclosure document that details the process through which a transportation project is developed. It includes a reasonable range of alternatives, demonstrates compliance with environmental laws and provides a means for public input into the decision making process.

Why Prepare an EIS?

An EIS is carried out for major federal actions that significantly affect the quality of the environment. An EIS is prepared in accordance with the National Environmental Policy Act of 1969 (NEPA), which establishes a process for analyzing and disclosing the impacts of federal actions on the environment.

EIS Timeline

Public Meeting Prepare Draft EIS

Public Hearing

Anticipated FEIS and ROD Approval

Fall 2018 Winter 2018 | Spring 2019 | Summer 2019 | Fall 2019 | Winter 2019 | Spring 2020 | Summer 2020

Fall 2020 Winter 2020 Spring 2021

Development of Additional Alternative and Revised Screening Process

Finalize Reasonable Alternatives Recommended Preferred Alternative

Prepare Final EIS

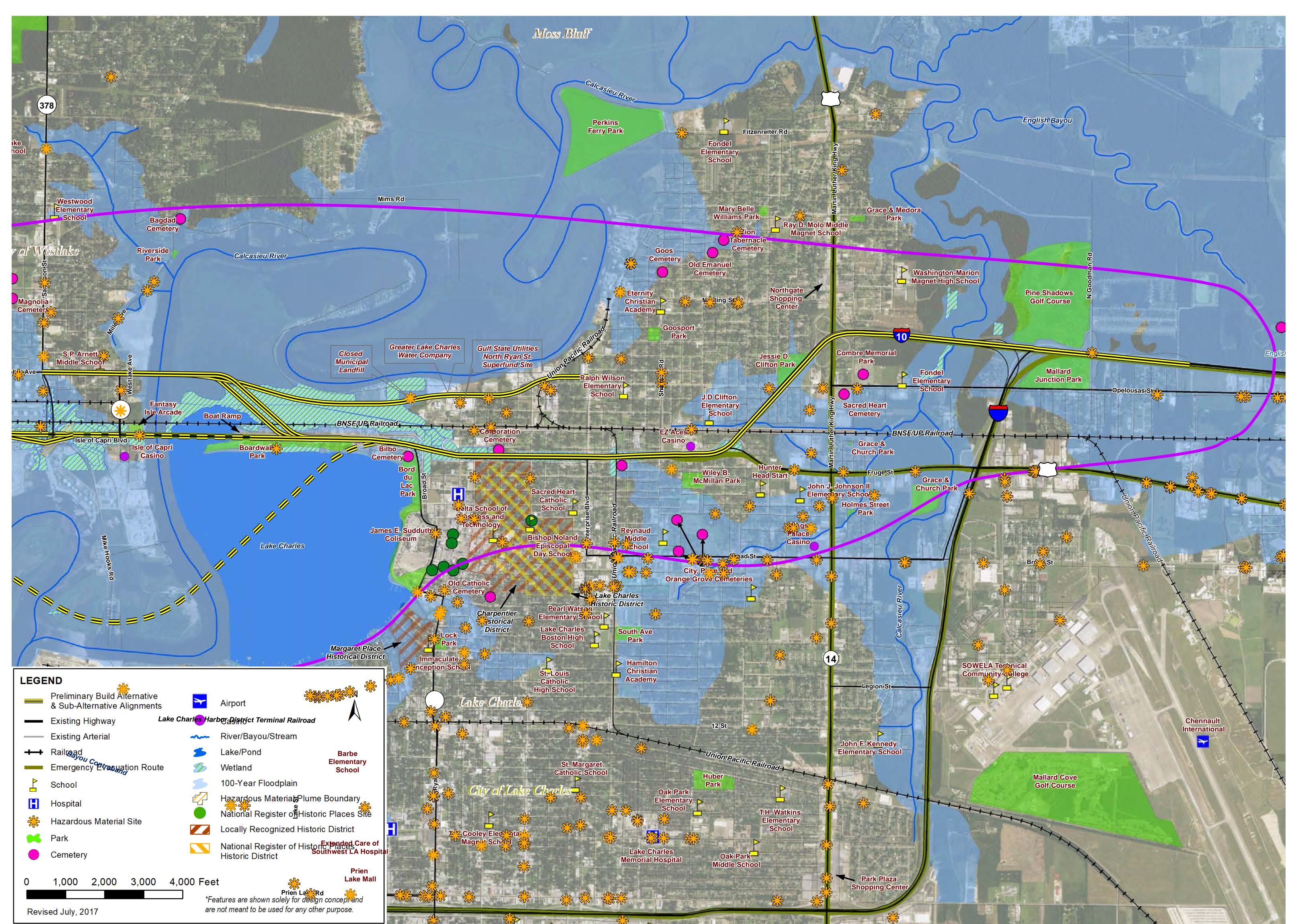
Appendix C-4: Station 4: Environmental Exhibits

CONSTRAINTS MAP (EASTERN LIMITS)

I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE
I-10/1-210 WEST END - I-10/1-210 EAST END
State Project No. H.003931





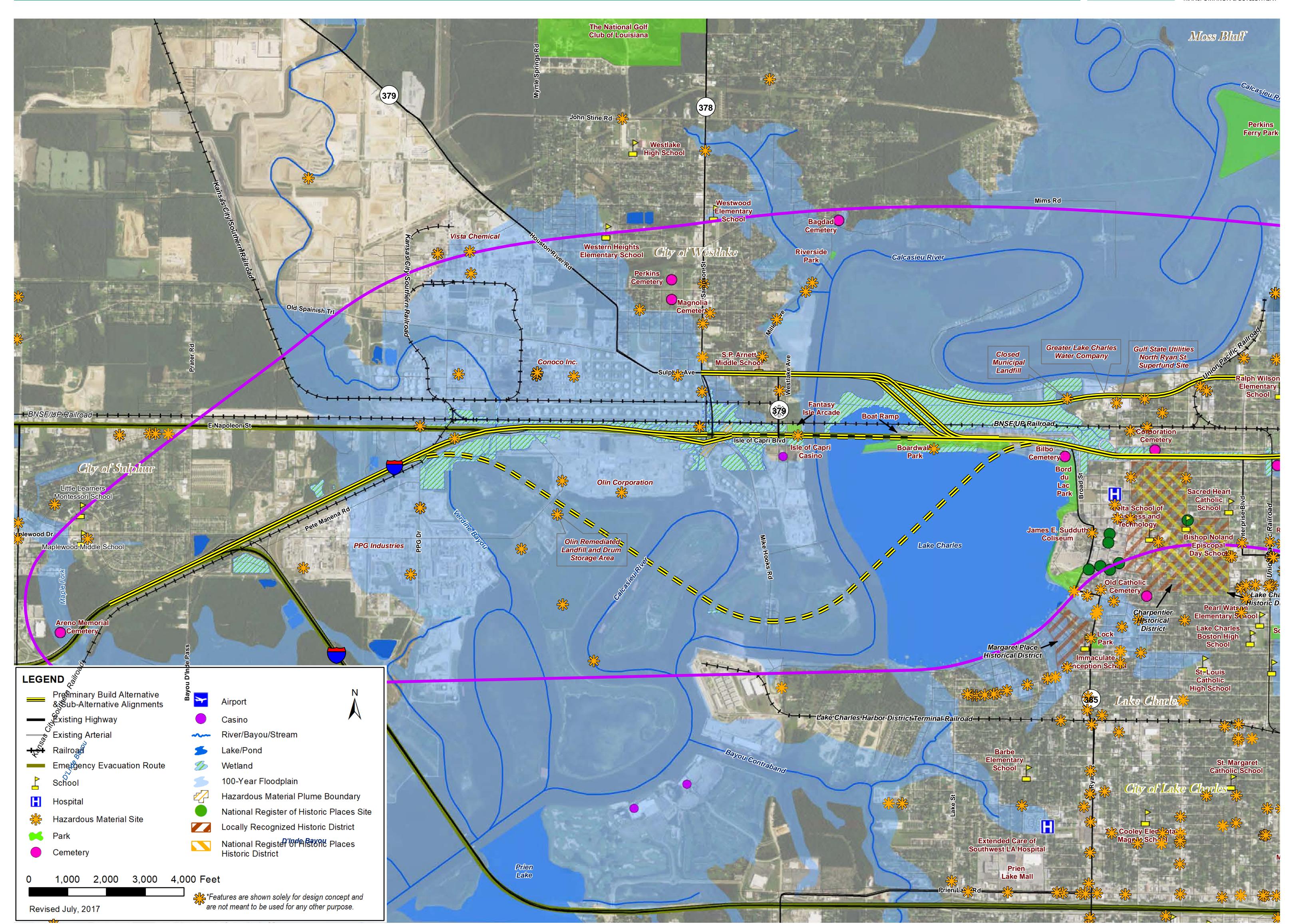


CONSTRAINTS MAP (WESTERN LIMITS)

I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE
I-10/1-210 WEST END - I-10/1-210 EAST END
State Project No. H.003931









- Considers the effects of Federal undertakings on historic properties
- Section 106 process occurs along with EIS preparation

Calcasieu River Bridge

- Eligible for the National Register of Historic Places
- Evaluated in accordance with Programmatic Agreement (PA) for Historic Bridges
- Designated in PA as a Non-priority bridge not ideal candidate for long term preservation
- Comments on project, including bridge, accepted for 45 days *
- DOTD to market bridge in effort to encourage relocation and adaptive reuse of bridge

Other Historic/Potentially Historic Properties

Section 106 Consultation Process within NEPA Timeline

Establish Area of Potential Effects & Identify Historic Properties

Completed once Reasonable Alternatives are identified 2

Formal Consultation with Identified Consulting Parties

Historic properties are identified and evaluated while the Draft EIS (DEIS) is under preparation

3.

Assess & Consult on Effects

Completed as part of the formal consultation as the DEIS is under preparation

4

Resolution of Adverse Effects

Completed following adverse effects assessment and prior to the DEIS public hearing

5.

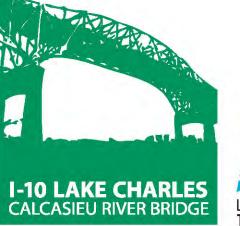
Develop MOA

Completed after the DEIS public hearing and before approval of the Final EIS (FEIS)

^{*}Comments received within 10 calendar days of the public meeting will become part of the official public meeting record.

Appendix C-5: Station 5: Preliminary Alternatives and Alternative Screening Process Exhibits

PRELIMINARY ALTERNATIVES







No Build

Future conditions if the project were not constructed. Existing conditions plus committed projects.



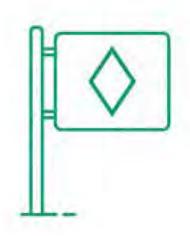
Transportation Systems Management (TSM)

Promoting efficiency through improvements to existing infrastructure. Includes intersection improvements turn prohibitions, traffic control improvements, signal improvements/synchronization, etc.



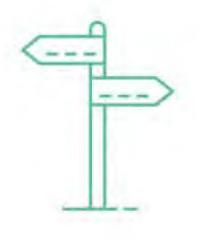
Transportation Demand Management (TDM)

Alternatives to driving. Includes public transit, rideshare promotion, telecommuting, flexible work hours, establishing park and ride facilities, etc.



High Occupancy Vehicle Lanes (HOV)

Roadway lane(s) reserved for exclusive use by cars with a driver and one or more passengers and by busses and vanpools.



Preliminary Build Alternatives (PBA)

Five PBAs with seven different Sampson St. Sub Alternatives.







Tier 1 Preliminary Alts Purpose & Need Screening

Tier 2 Preliminary Alts Objectives Screening

Reasonable Alts Detailed Evaluation

Preferred Alternative(s)

Preliminary Alternatives

Stakeholder / Public Input

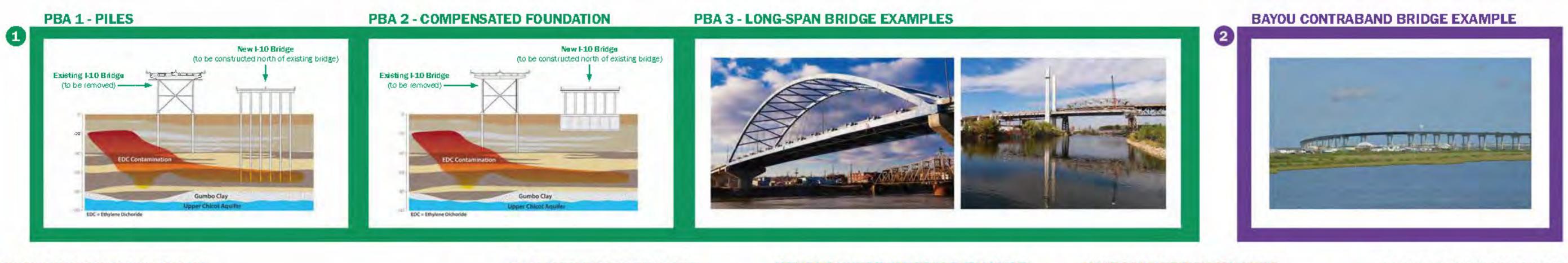
Appendix C-6: Station 6: Features of the Preliminary Build Alternatives

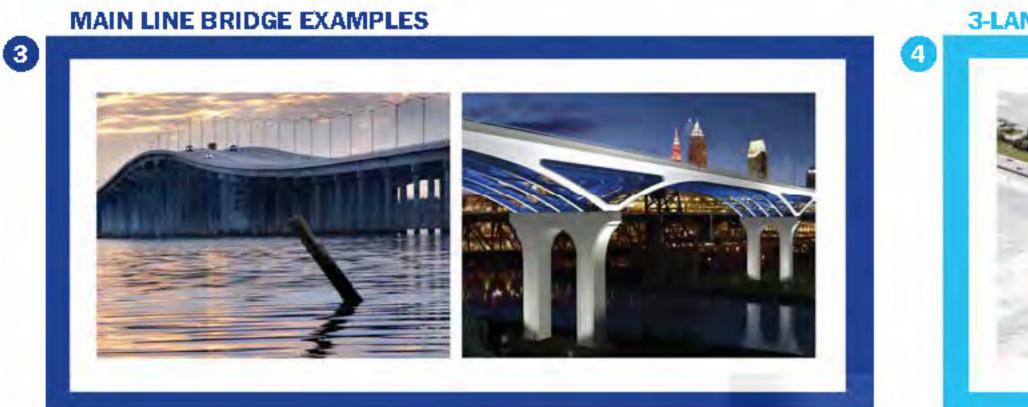
FEATURES OF THE PRELIMINARY BUILD ALTERNATIVES

I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE I-10/1-210 WEST END - I-10/1-210 EAST END State Project No. H.003931

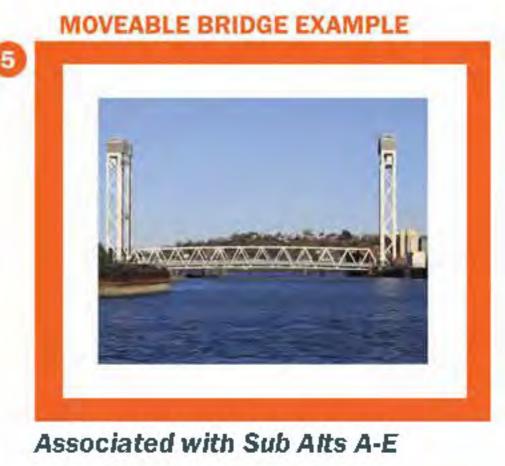














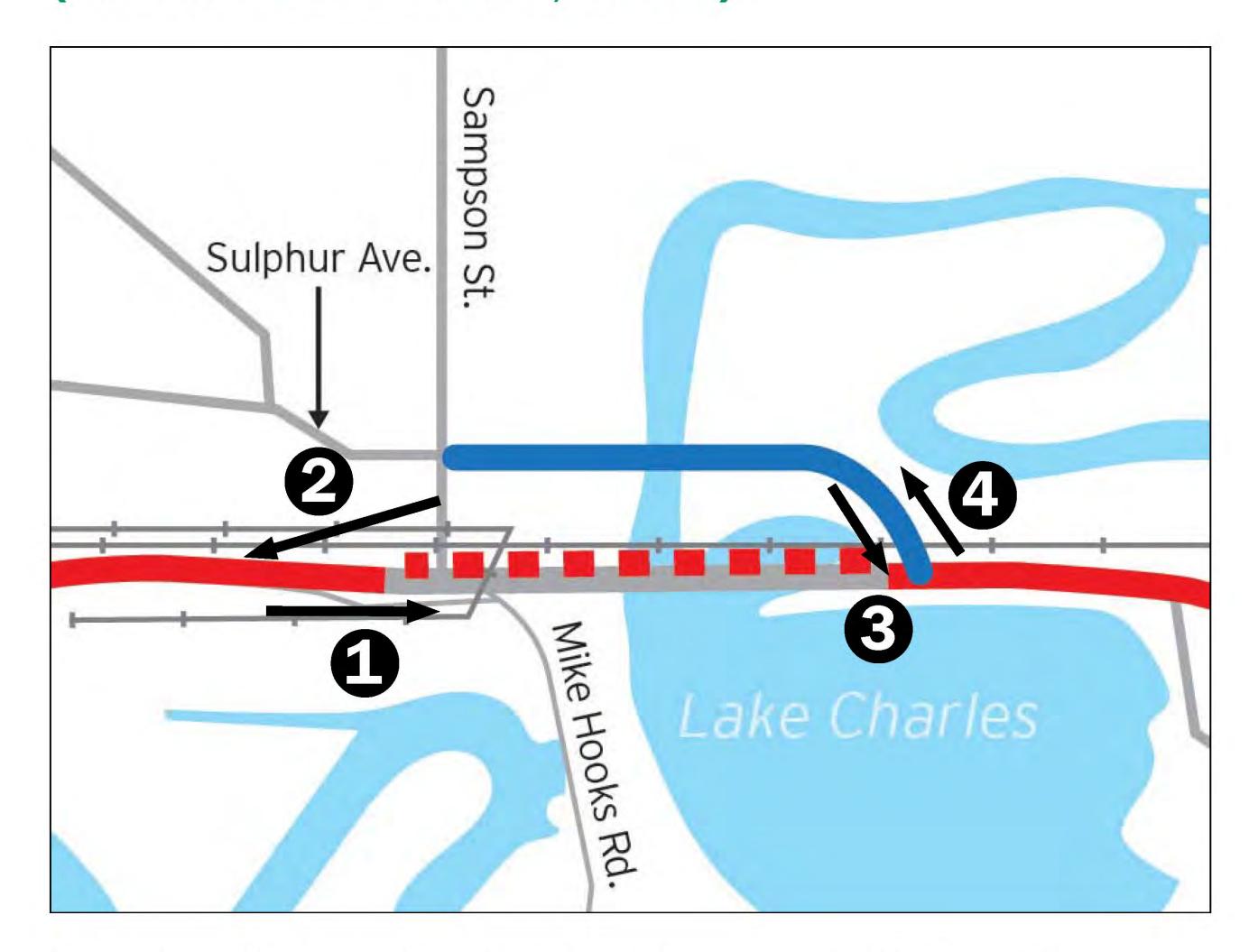
WWW.110LAKECHARLES.COM





SUB ALT A

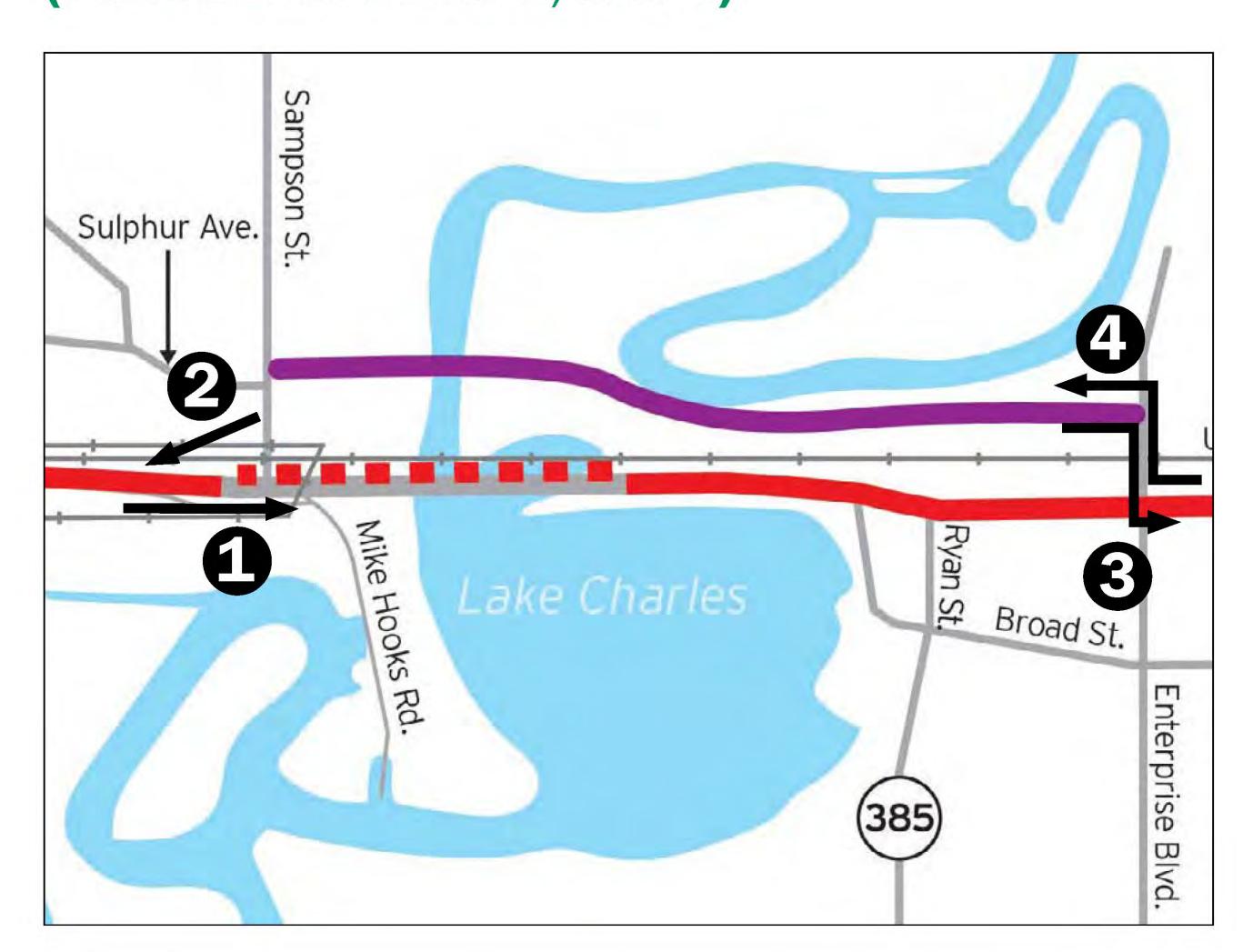
(Paired with PBAs 2, 3 & 4)



Sulphur Ave. extension to I-10 west of Ryan St.

- EB I-10 exit ramp to Sampson St.
- WB I-10 entrance ramp from Sampson St.
- EB I-10 entrance ramp along Sulphur Ave. extension from Sampson St.
- WB I-10 exit ramp along Sulphur Ave. extension to Sampson St.

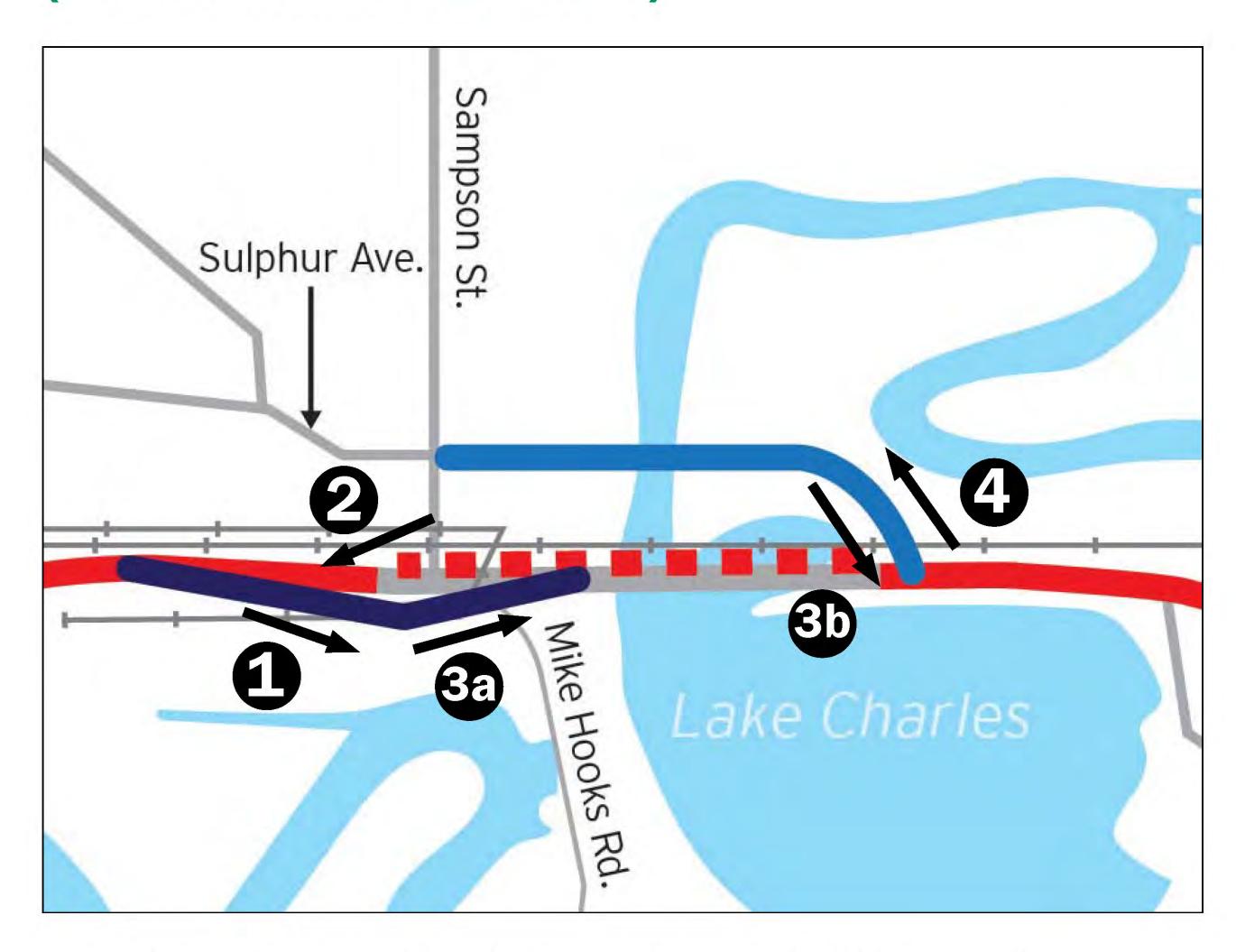
SUB ALT B (Paired with PBAs 2, 3 & 4)



Sulphur Ave. extension to Enterprise Blvd.

- EB I-10 exit ramp to Sampson St.
- WB I-10 entrance ramp from Sampson St.
- EB I-10 entrance ramp at Enterprise Blvd. along Sulphur Ave. extension from Sampson St.
- WB I-10 exit ramp at Enterprise Blvd. along Sulphur Ave. extension to Sampson St.

SUB ALT C (Paired with PBAs 2 & 3)



- Sulphur Ave. extension to I-10 west of Ryan St.
- Intersection improvements at Sampson St. south of I-10
- EB I-10 exit ramp to Sampson St.
- WB I-10 entrance ramp from Sampson St.
- EB I-10 entrance ramp access:

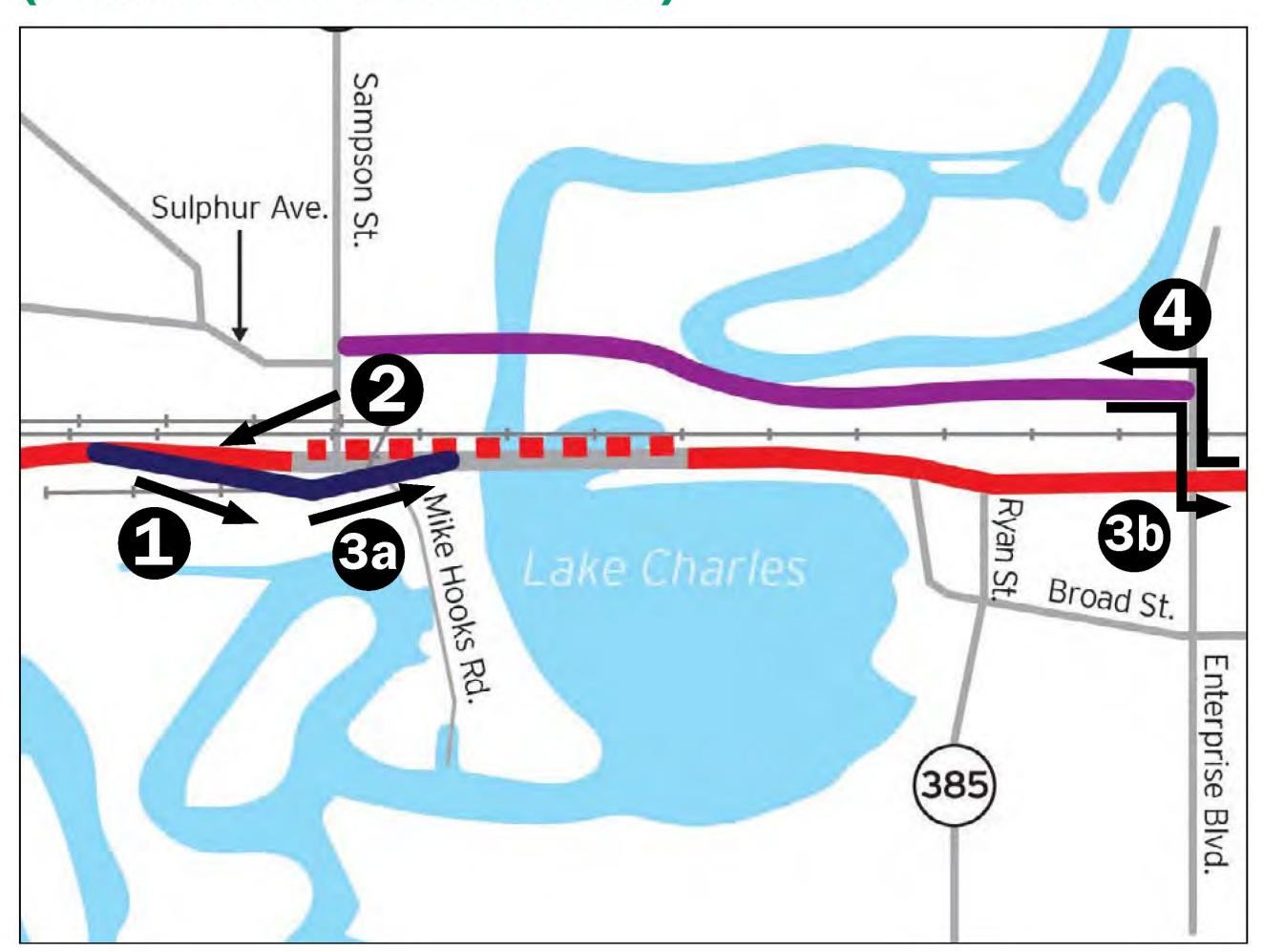
 a. from Sampson St.
 b. along Sulphur Ave. extension from Sampson St.
- WB I-10 exit ramp at Enterprise Blvd. along Sulphur Ave. extension to Sampson St.





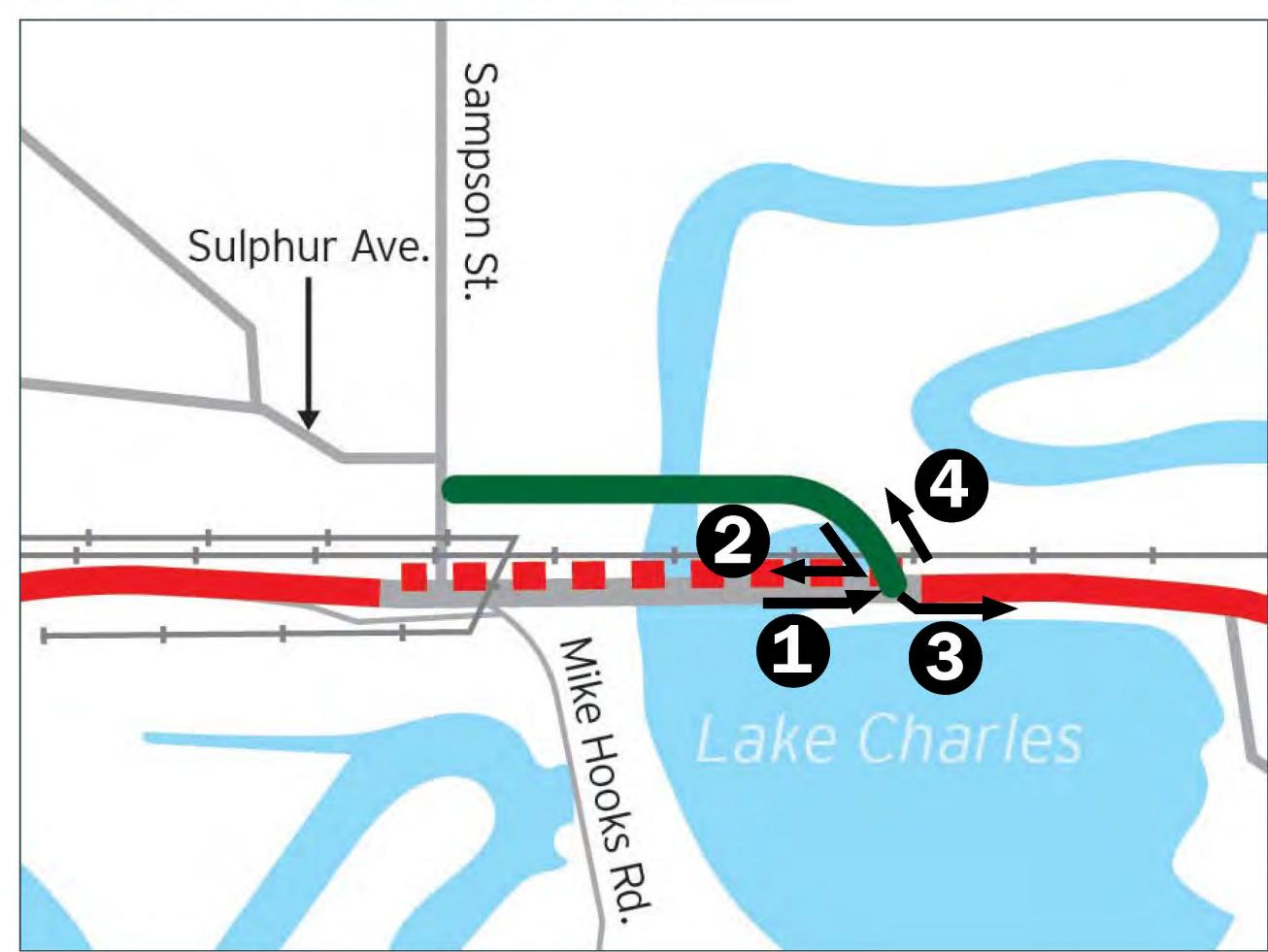
SUB ALT D

(Paired with PBAs 2 & 3)



- Sulphur Ave. extension to Enterprise Blvd.
- Intersection improvements at Sampson St. south of I-10
- EB I-10 exit ramp to Sampson St.
- WB I-10 entrance ramp from Sampson St.
- B I-10 entrance ramp access: a. from Sampson St. b. along Sulphur Ave. to Enterprise Blvd. from Sampson St.
- WB I-10 exit ramp from Enterprise Blvd. along Sulphur Ave. extension to Sampson St.

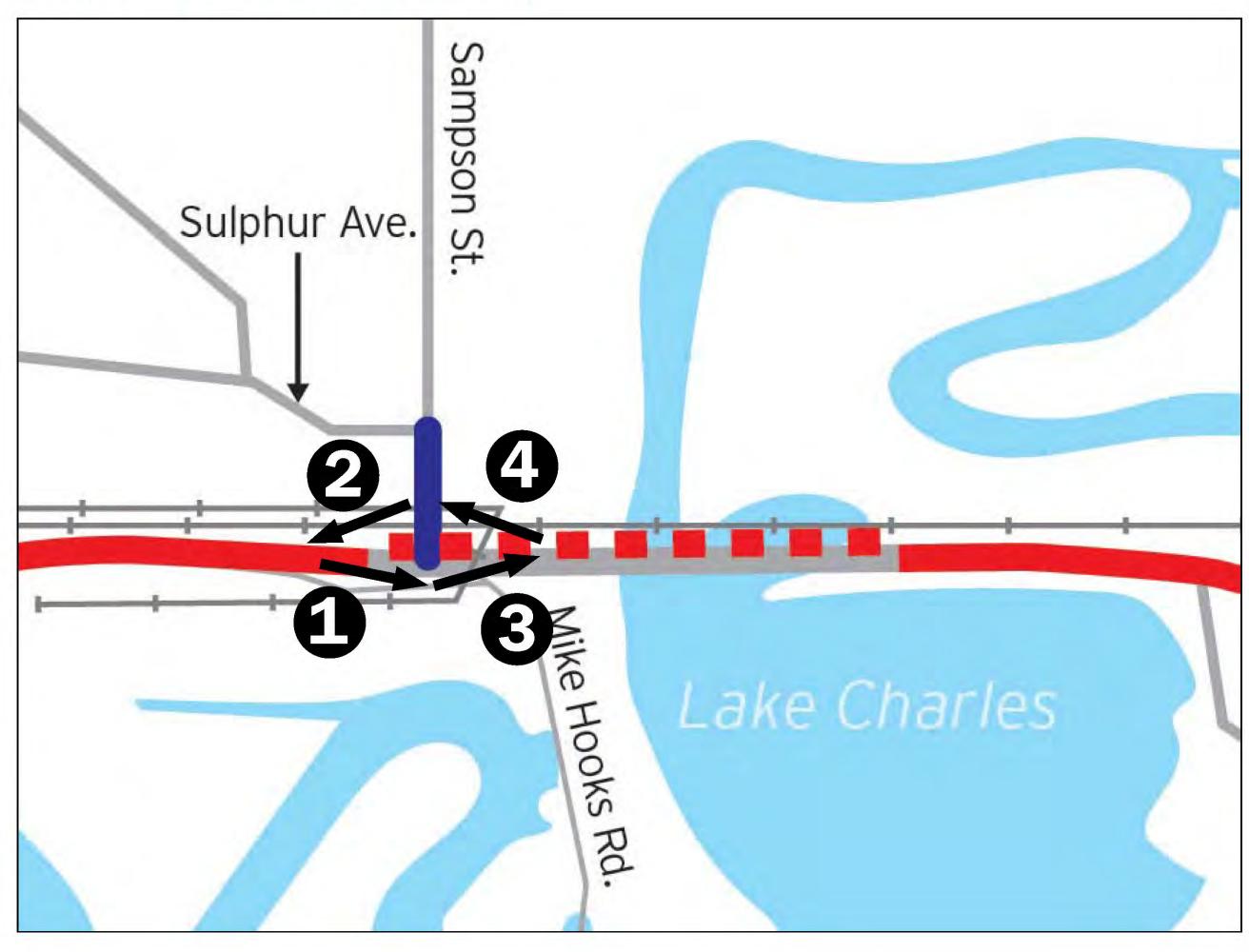
SUB ALT E (Paired with PBAs 2 & 3)



- Sulphur Ave. extension to fully directional, elevated interchange to I-10 west of Ryan St.
- EB I-10 exit ramp along Sulphur Ave. extension to Sampson St.
- WB I-10 entrance ramp along Sulphur Ave. extension from Sampson St.
- EB I-10 entrance ramp along Sulphur Ave. extension from Sampson St.
- WB I-10 exit ramp along Sulphur Ave. extension to Sampson St.

Note: No direct access to/from I-10 at Sampson St.

SUB ALT F (Paired with PBA 1)



- Fully directional, elevated interchange over at-grade railroad tracks
- Drives piles in EDC area
- EB I-10 exit ramp at Sampson St.
- WB I-10 entrance ramp at Sampson St.
- B I-10 entrance ramp at Sampson St.
- WB I-10 exit ramp at Sampson St.

PRELIMINARY BUILD SUB ALTERNATIVE (G)

I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE

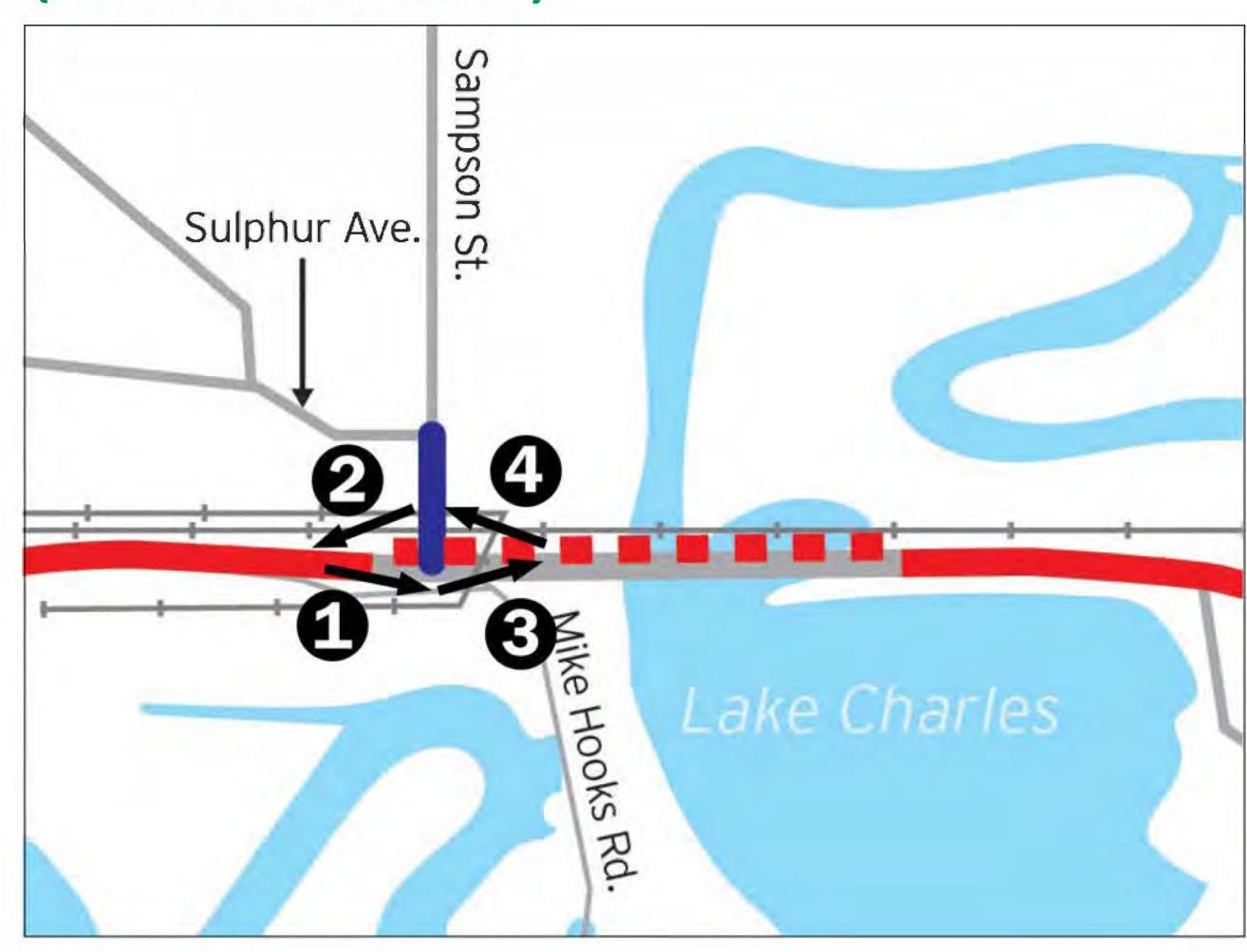
I-10/1-210 WEST END - I-10/1-210 EAST END State Project No. H.003931





SUB ALT G

(Paired with PBA 5)

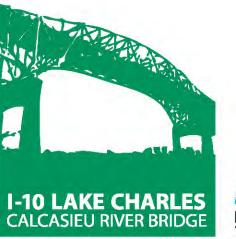


- I-10 bridge ends at the eastern boundary of the EDC area
- Mainline I-10 crosses EDC area on an MSE Wall coming to grade at the western boundary of the EDC area
- Fully directional, Sampson Street interchange is elevated over I-10 mainline connecting to the bridge on the east and to the at-grade roadway on the west
- EB I-10 exit ramp at Sampson St.
- WB I-10 entrance ramp at Sampson St.
- B I-10 entrance ramp at Sampson St.
- WB I-10 exit ramp at Sampson St.

Appendix C-7: Station 7: Schematics

PRELIMINARY BUILD ALTERNATIVES WITH SUB ALTERNATIVES

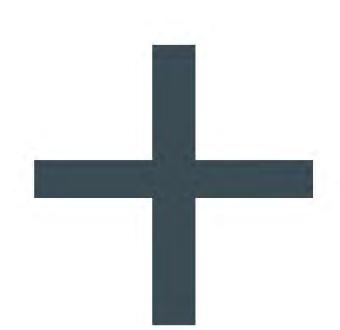
I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE I-10/1-210 WEST END - I-10/1-210 EAST END State Project No. H.003931





PRELIMINARY BUILD ALTERNATIVES (PBAs)

- **PBA 1** I-10 corridor improvements, new bridge immediately north of existing bridge, pile foundation in EDC contamination area
- **PBA 2 -** *I-10* corridor improvements, new bridge immediately north of existing bridge, compensated foundation above EDC contamination depth
- **PBA 3 -** *I-10* corridor improvements, new bridge immediately north of existing bridge, long-span bridge over EDC contamination area
- **PBA 4 -** I-10 corridor improvements, new bridge south of existing bridge, 2 new bridge crossings of Bayou Contraband, avoids construction in EDC Area
- **PBA 5** I-10 corridor improvements, new bridge immediately north of existing bridge, MSE wall over EDC contamination area



SAMPSON SUB-ALTERNATIVES (SUB-ALTS)

- A. Sulphur Ave. extension to West of Ryan St.
- B. Sulphur Ave. extension to Enterprise Blvd.
- C. Sulphur Ave. extension to West of Ryan St. & intersection improvements at Sampson St. south of I-10
- D. Sulphur Ave. extension to Enterprise Blvd. & intersection improvements at Sampson St. south of I-10
- Sulphur Ave. extension to fully directional, elevated interchange to 10 west of Ryan St.
- F. Fully directional, elevated interchange over Sampson St. at-grade railroad tracks
- **G.** Fully directional interchange on Sampson St. over mainline I-10, railroad tracks relocated



HOW THE PBAs MATCH UP WITH THE SUB-ALTS

DA	

PBA 1 - F

PBA 2

PBA 2 - A

PBA 2 - B

PBA 2 - C

PBA 2 - D

PBA 2 - E

PBA 3

PBA 3 - A

PBA 3 - B

PBA 3 - C

PBA 3 - D

PBA 3 - E

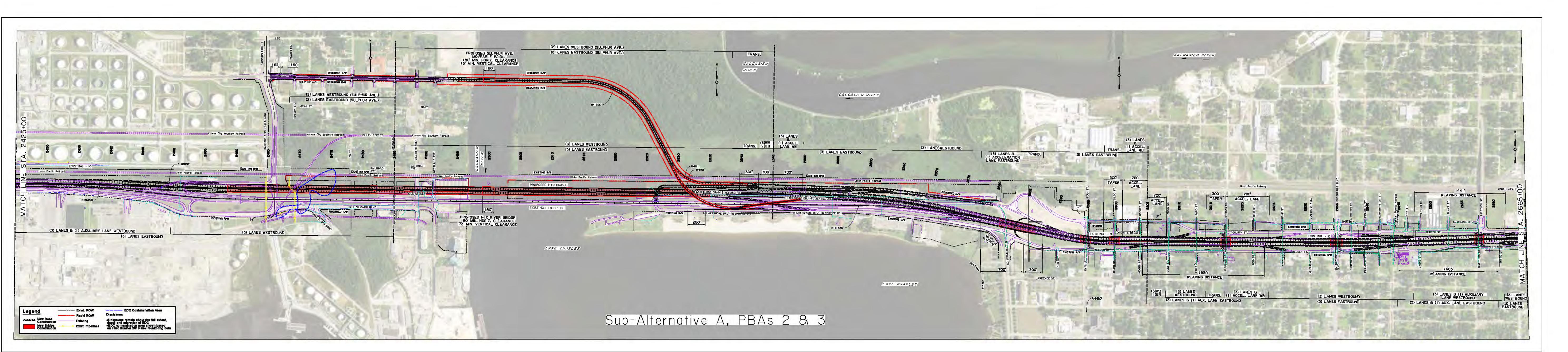
PBA 4

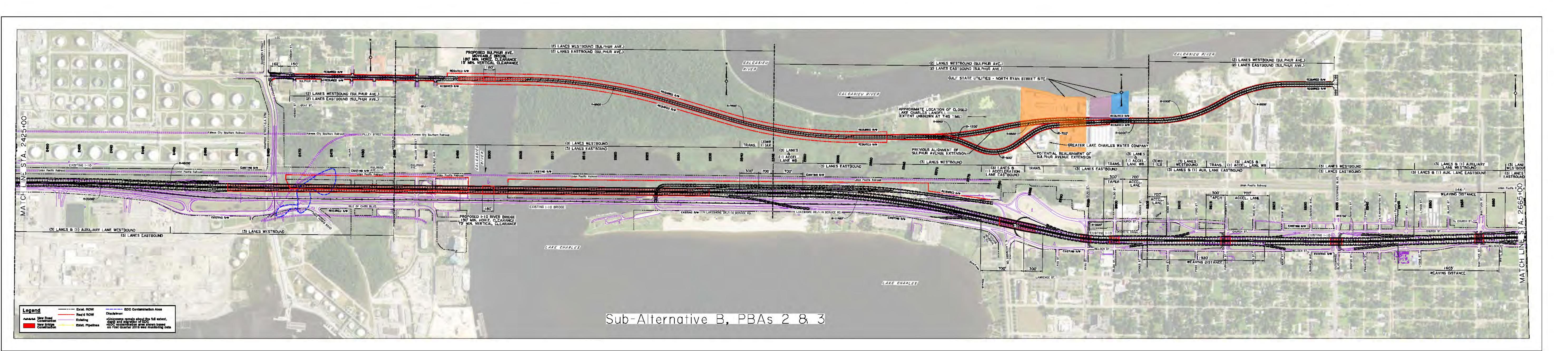
PBA 4 - A

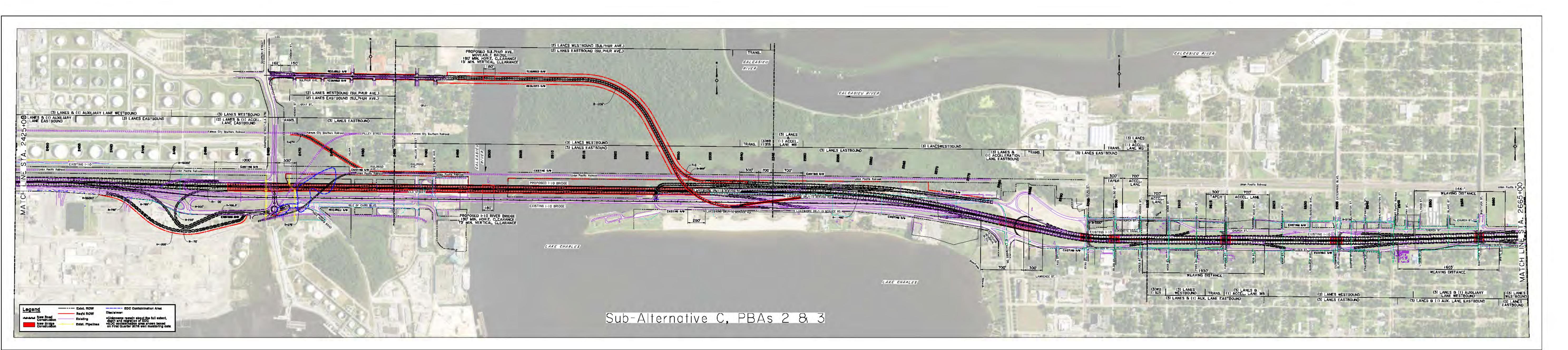
PBA 4 - B

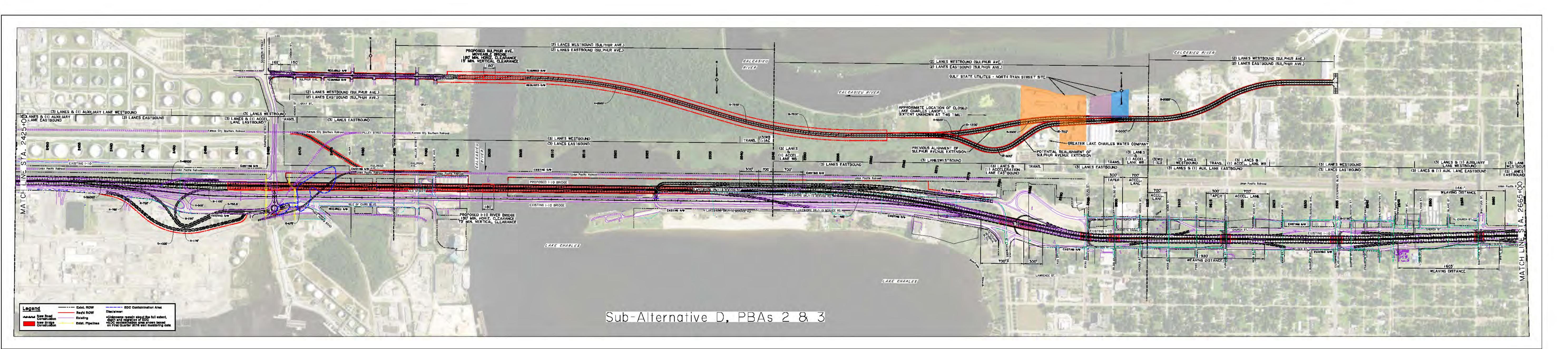
PBA 5

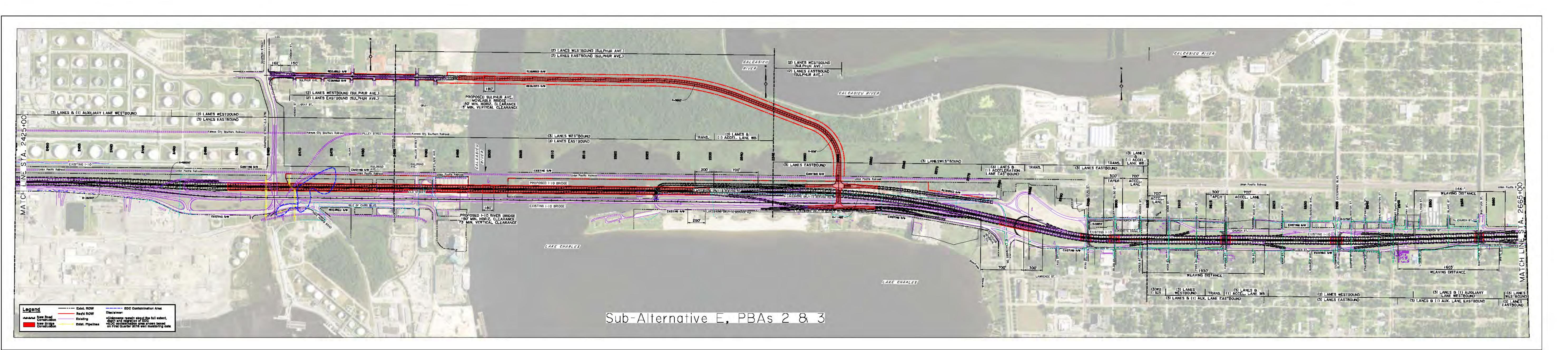
PBA 5-G

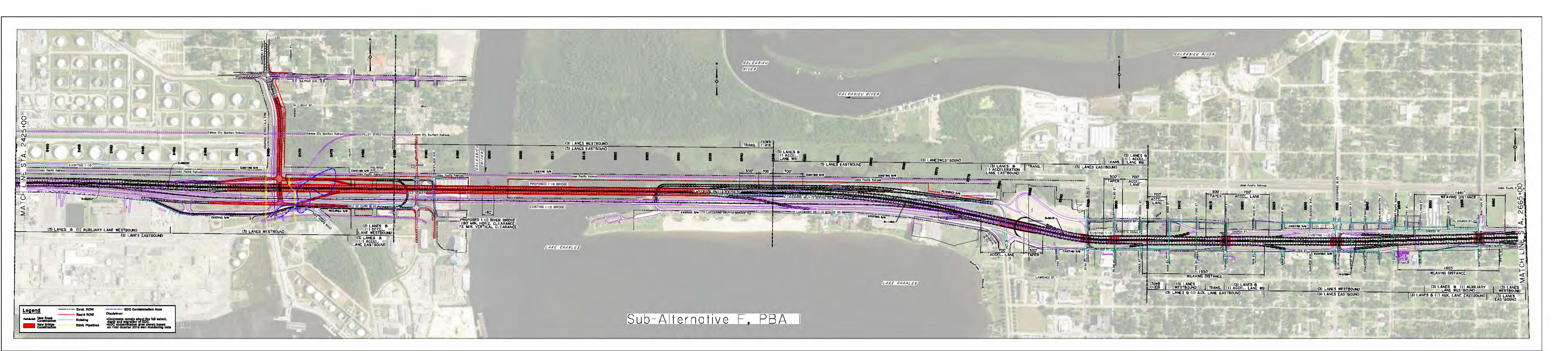


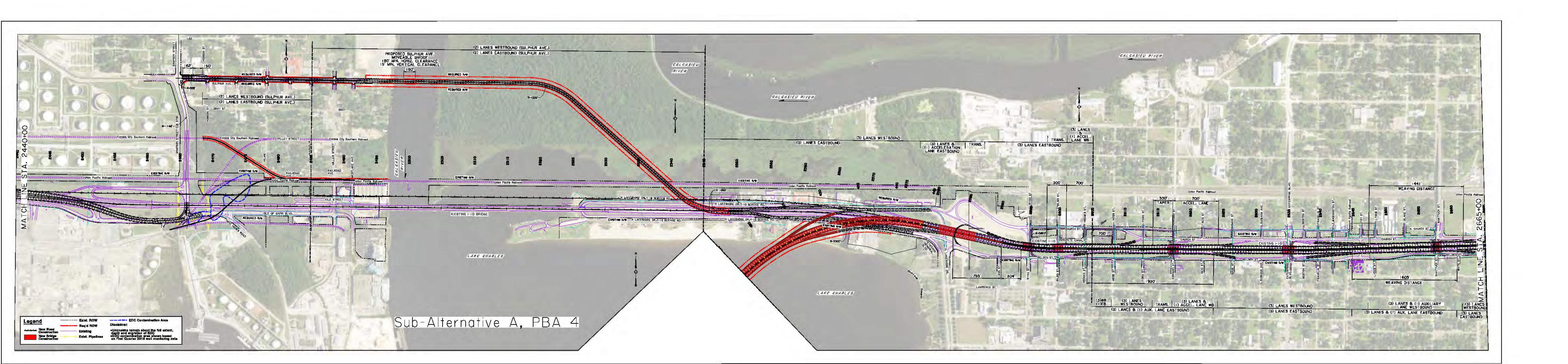


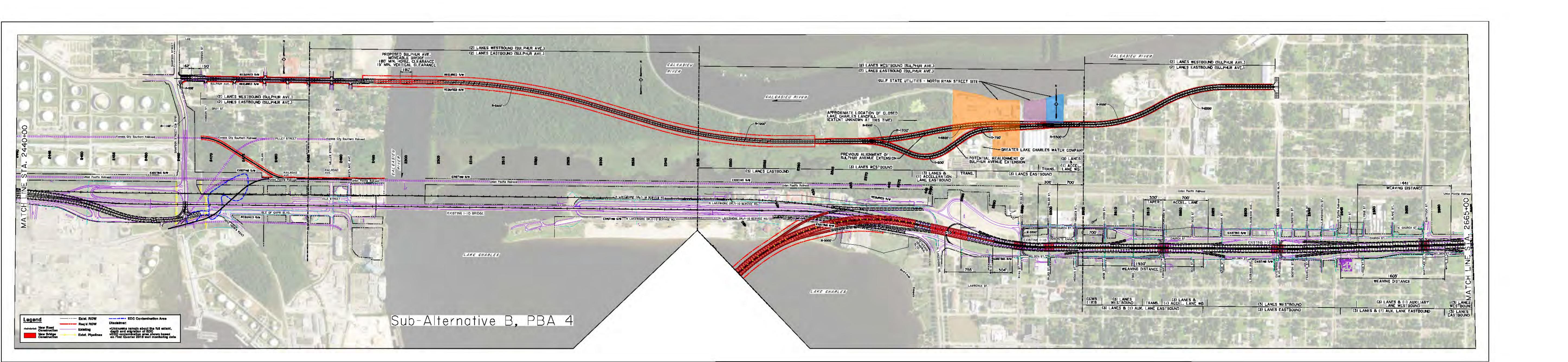


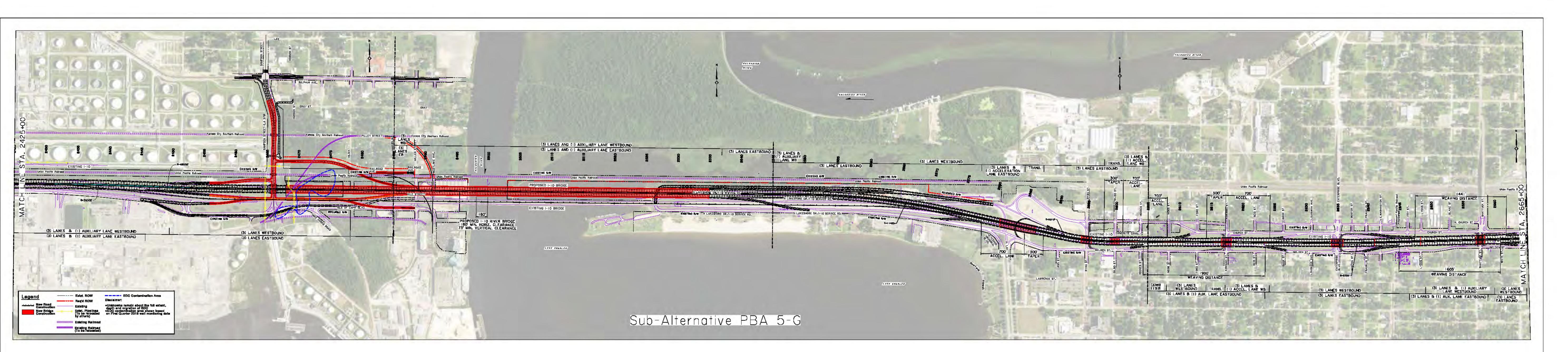


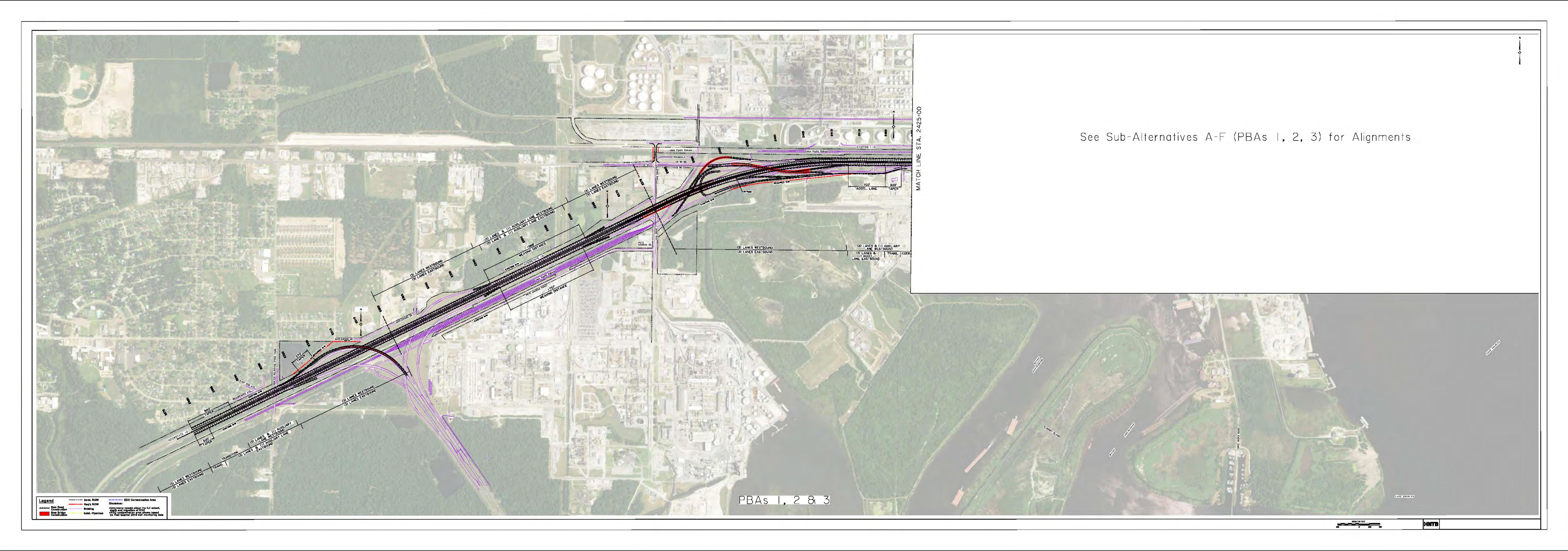


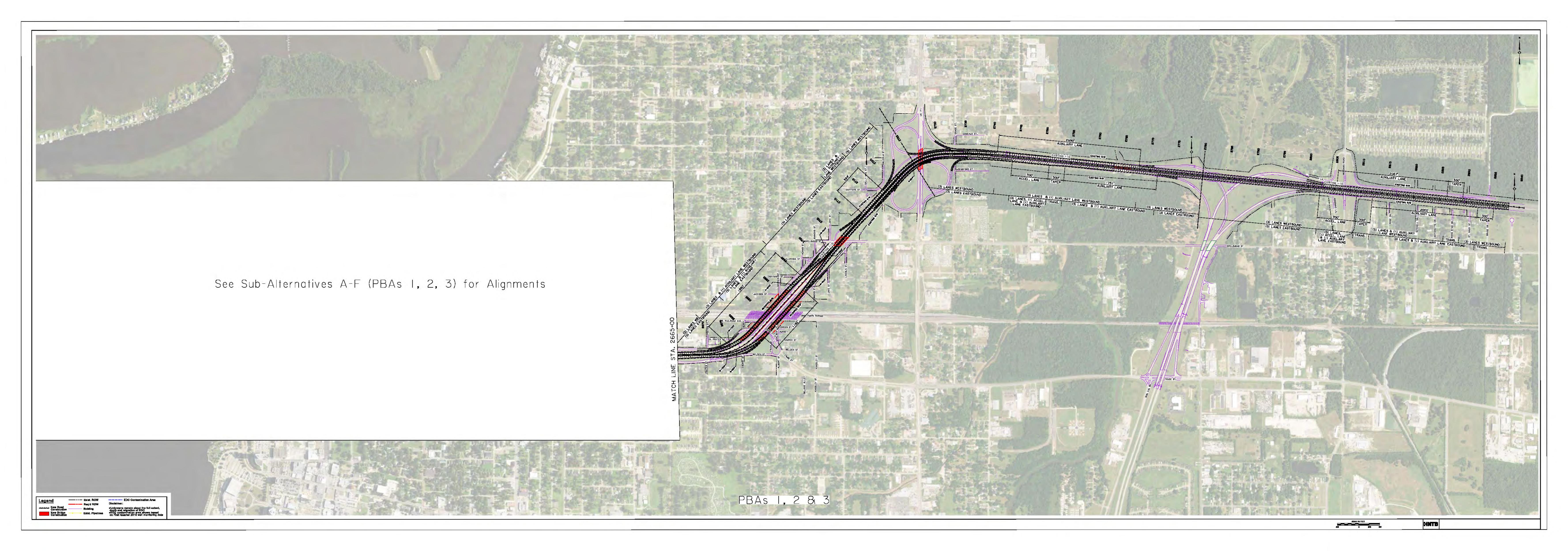


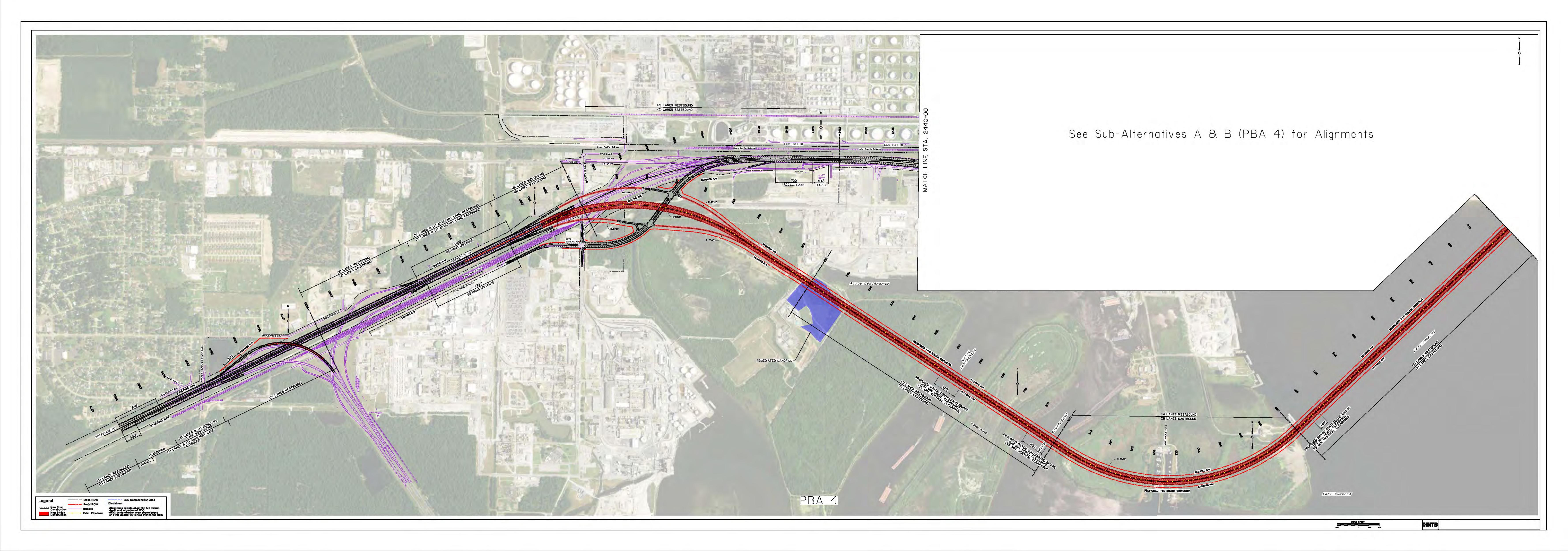


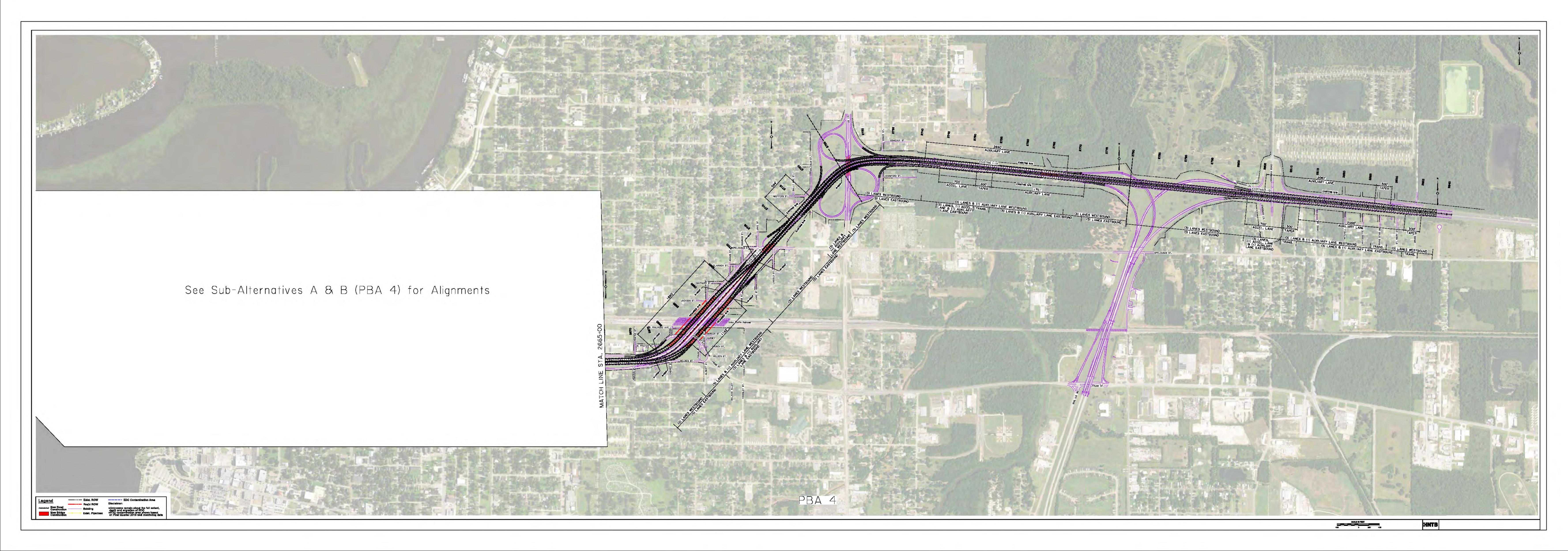


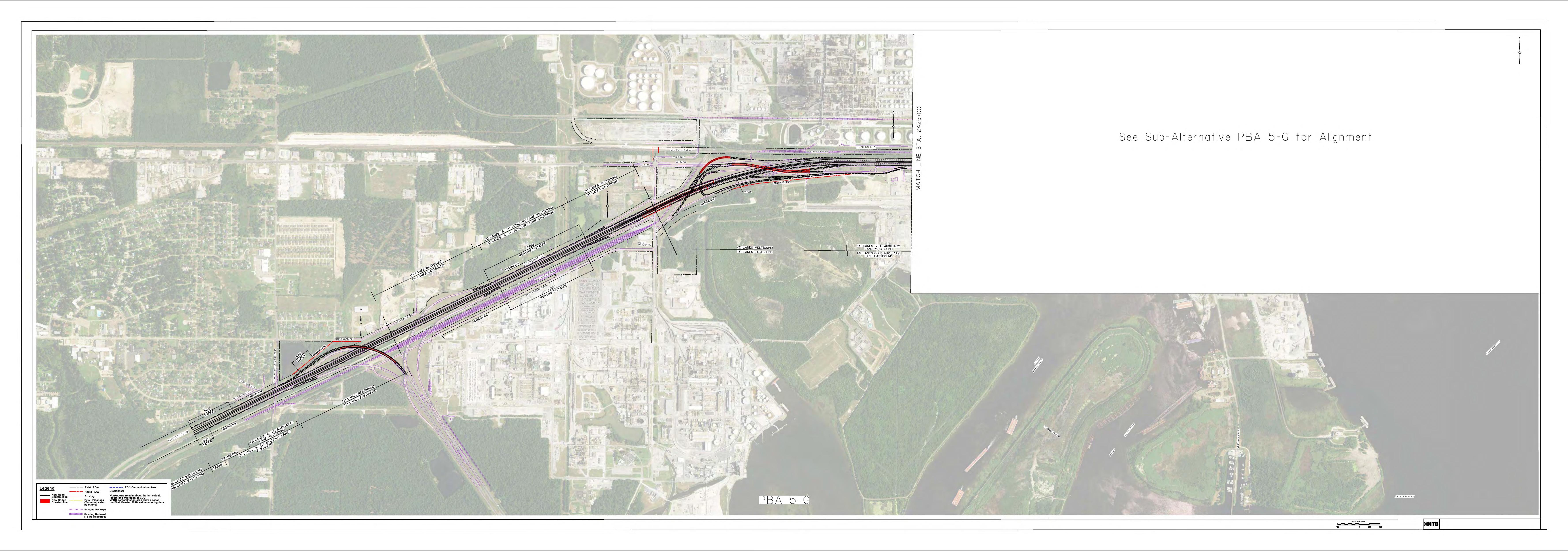


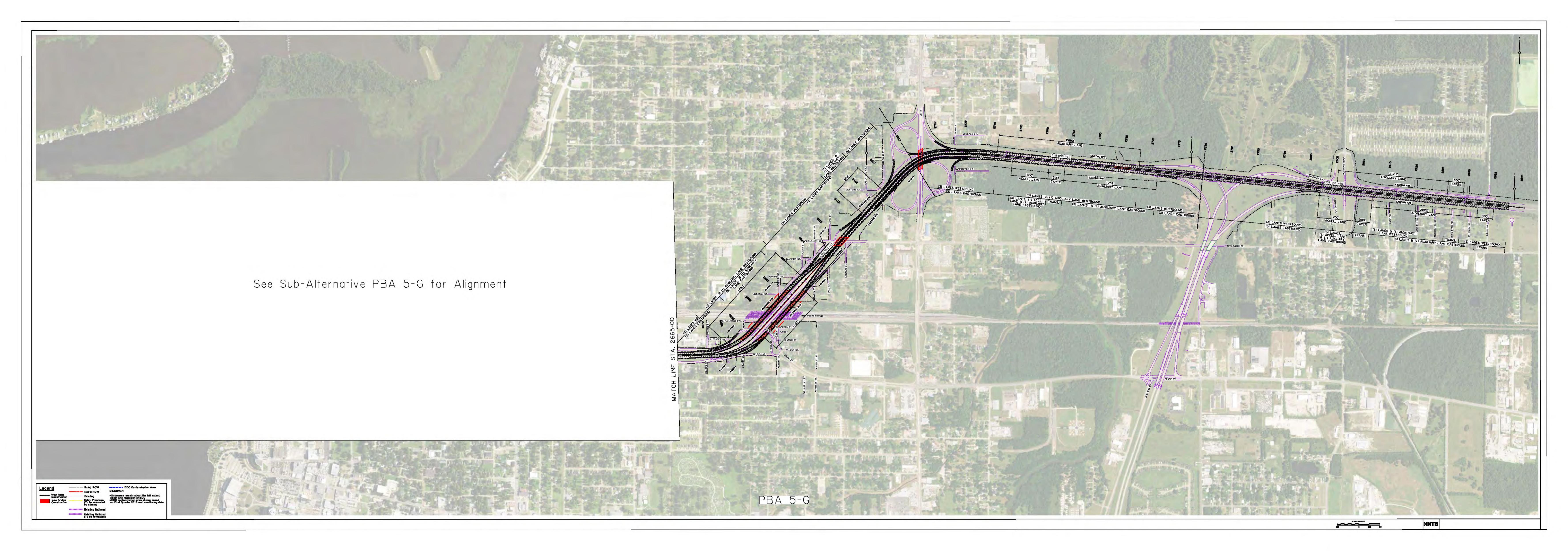


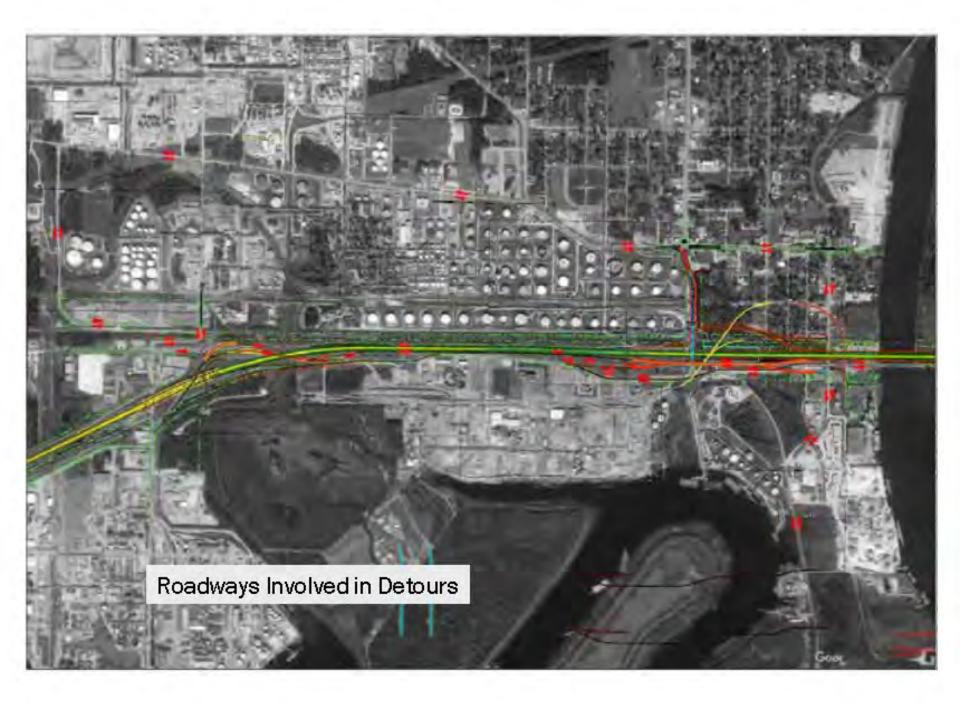








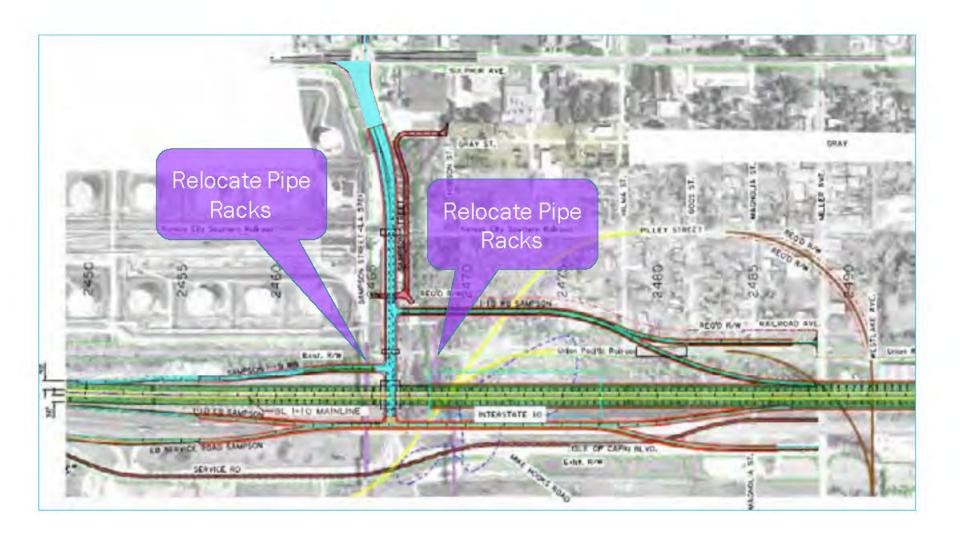


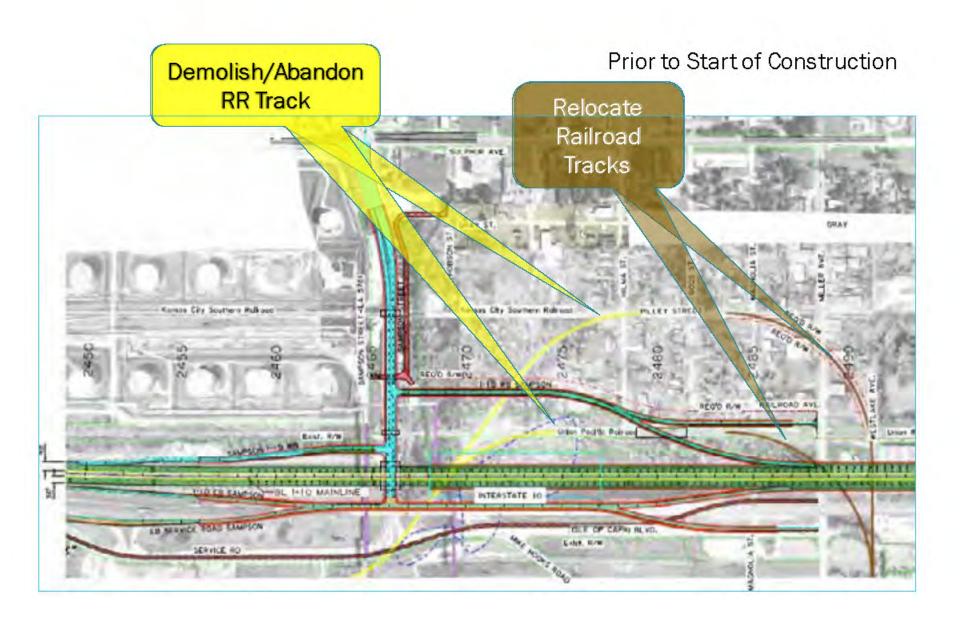






PROPOSED PRELIMINARY CONSTRUCTION SCHEDULE PRIOR TO START OF CONSTRUCTION RELOCATE PIPE RACKS DEMOLISH/ABANDON # ALLROAD TRACKS			
	CONSTRUCTION MONTH 0-30	PHASE I	SAMPSON STREET PYTERCHANCE OFEN IN ALL DIRECTIONS. EXISTING I'TO DRIDGE AND ROADWAY OFEN IN ALL DIRECTIONS. CONSTRUCT I'TO BRIDGE AT CALCASIEU HIVER, WEST BOUND SAMPSON STREET ELEVATED EXIT HAMP, WEST BOUND ELEVATED ENTHANCE RAMP FROM SAMPSON STREET, UROUND LEVEL MAINLINE INTO FROM WEST SIDE BASE OF BRIDGE TO SAMPSON STREET R/W & FROM SAMPSON STREET R/W TO EXISTING GROUND LEVEL RAMPS WEST OF SAMPSON STREET.
	MONTH 18-10	PHATE 2	SAMPSON STREET WEST BOUND EXIT RAMP AND WEST BOUND ENTRANCE RAMP TO 1-10 CLOSED, EAST BOUND EXIT RAMP FROM 1-10 TO SAMPSON STREET MERVICE ROAD AND EAST BOUND ENTRANCE RAMP FROM SAMPSON STREET/SERVICE ROAD TO 1-10 OPEN. CONTINUE PHASE I CONSTRUCTION. CONSTRUCT ELEVATED SAMPSON STREET FROM NORTH OF EXISTING 1-10/CALCASIED RIVER BRIDGE TO SULPHUR AVENUE, ELEVATED WEST BOUND ENTRANCE RAMP TO 1-10 BEYOND PHASE I CONSTRUCTION AND GROUND LEVEL MAINLINE 1-10 IN SAMPSON STREET R/W.
	MONTH SO-48	PHASE 3	MOVE 1-10 TRAFFIC TO NEW BRIDGE AND GROUND LEVEL MARKINE 1-10, OPEN WEST BOUND ELEVATED EXIT RAMP TO SAMPSON STREET, OPEN WEST BOUND ELEVATED ENTRANCE RAMP TO 1-10, OPEN ELEVATED SAMPSON STREET FROM NORTH OF EXISTING BRIDGE TO SULPHUR AVENUE, DEMOURSH EXISTING ELEVATED 1-10 BRIDGE FROM WEST END DOWNSLOPE TO EAST END INCLUDING COLUMNS IS PILE CAPS, CONSTRUCT GROUND LEVEL IS ELEVATED EAST BOUND EXIT RAMP TO SAMPSON STREET AND EAST BOUND ENTRANCE RAMP TO 1-10, EMT RAMP FROM ELEVATED RAMP TO WILLER AVENUE, ENTRANCE RAMP FROM 1-10 DERVICE ROAD TO ELEVATED RAMP.
	MONTH 36-48	PHASE 4	OPEN EAST BOUND ELEVATED HAMP, I-TO EXIT HAMP TO SAMPSON STREET AND EAST BOUND ENTRANCE HAMP TO I-TO MAMPHON STREET INTERCHANCE AT 1-10 OPEN IN ALL DIRECTIONS, CONSTRUCT RELOCATED 1-10 SERVICE ROAD AND OTHER GROUND LEVEL FEATURES.





Construction Phase 1

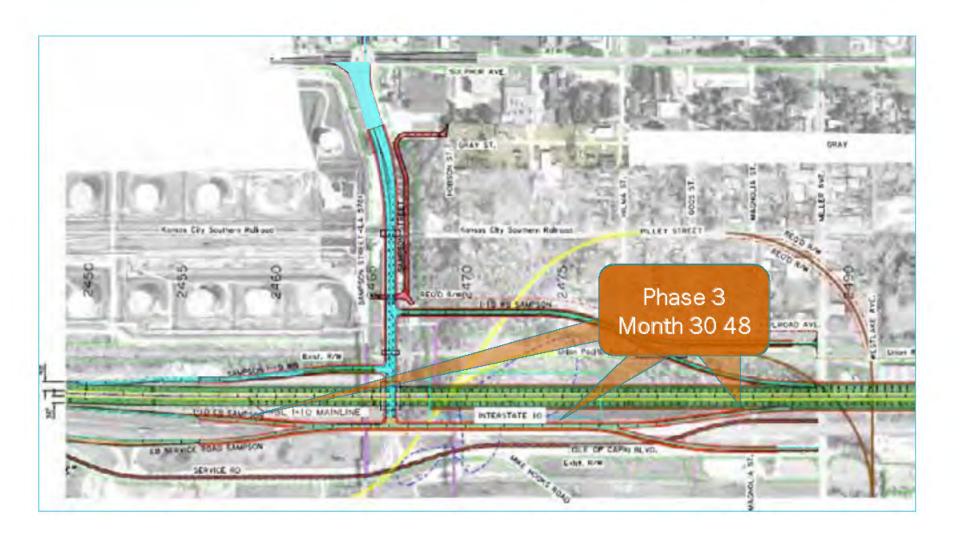


Construction Phase 2

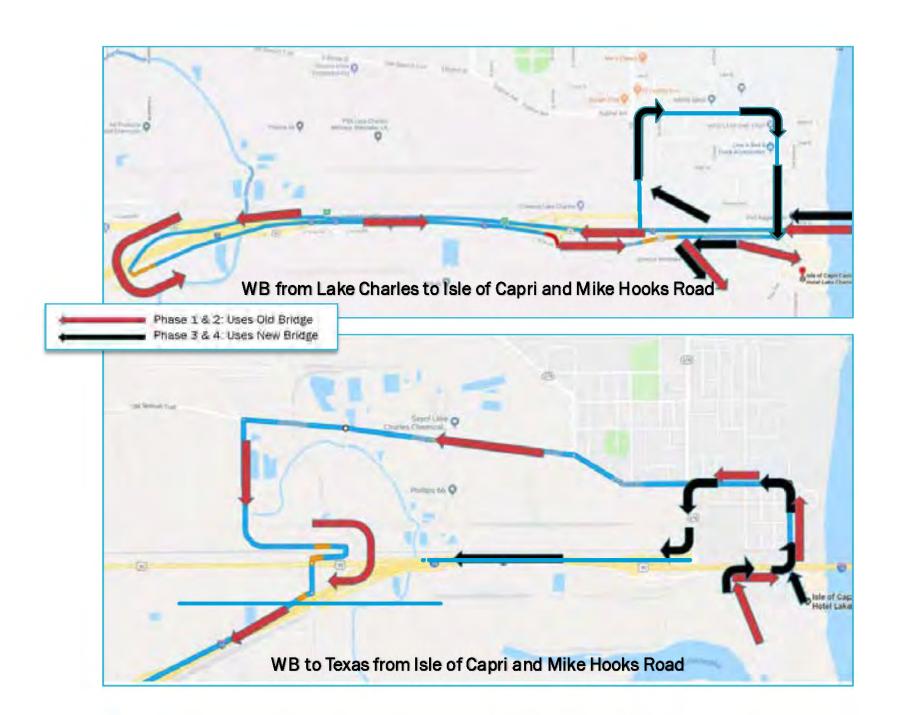


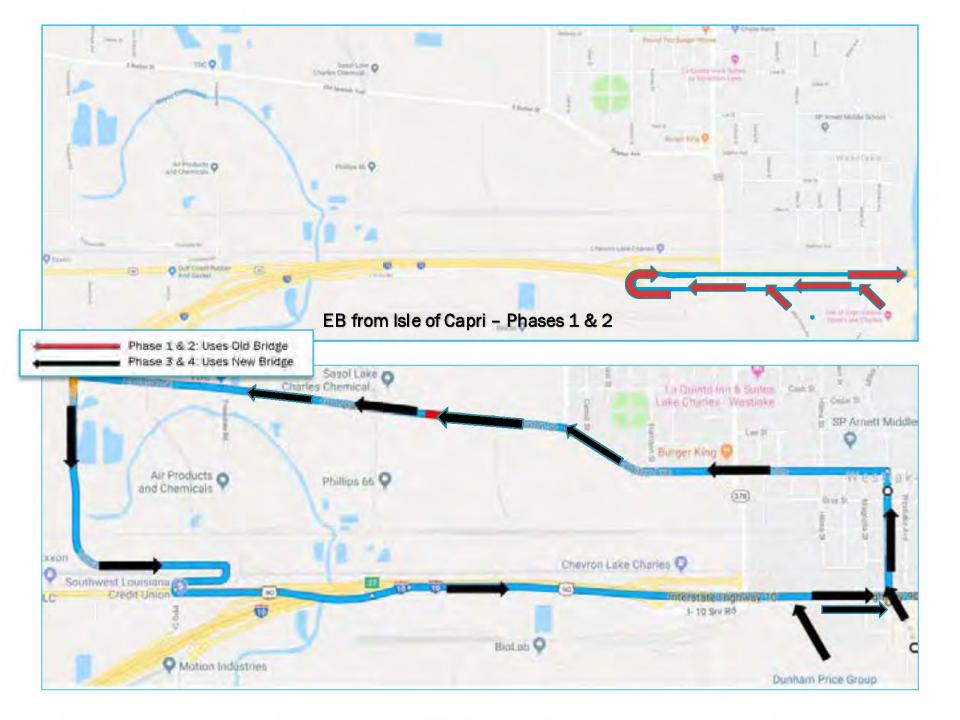
*Sampson Street Closed

Construction Phase 3

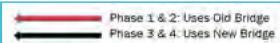


Construction Phase 4 Phase 4 Month 36 48 Karasa City Southern Rulliago Konsas City Southern Refront EB MERICE FOR EARLISON Echt. Row SERVICE RO



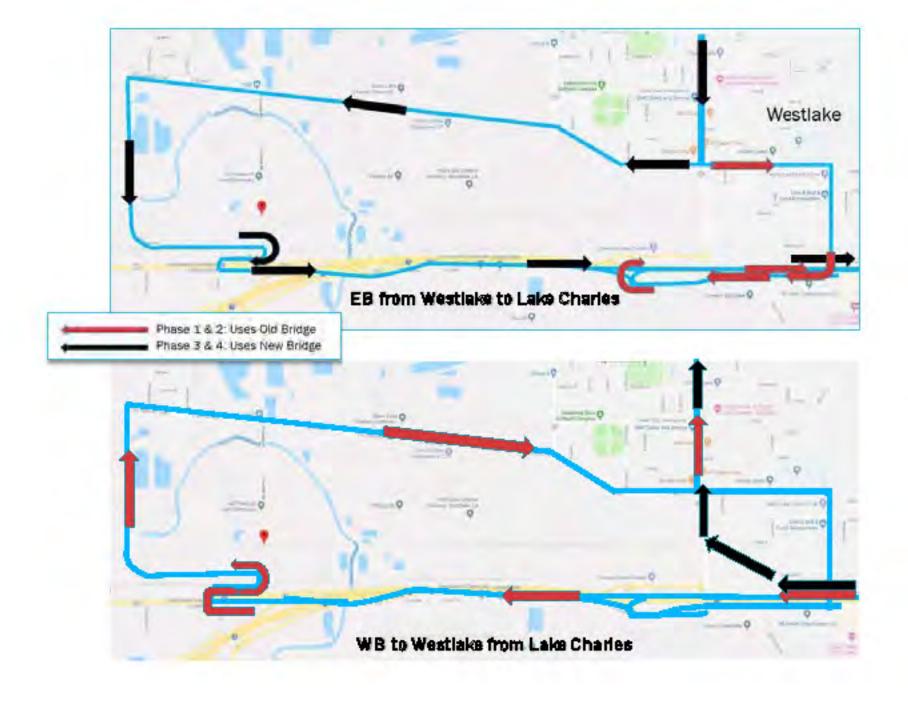






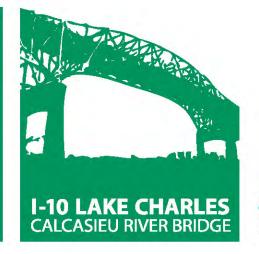
EB to Isle of Capri from Texas





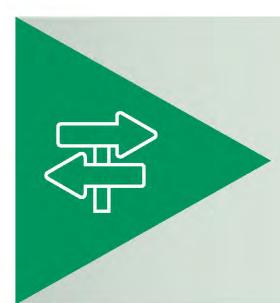
Appendix C-8: Station 8: Screening Results Exhibits

I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE
I-10/1-210 WEST END - I-10/1-210 EAST END
State Project No. H.003931





11 OBJECTIVES | 35 MEASURES



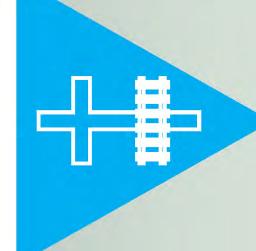
1. Minimize ROW

Acres of New ROW
 Number of Parcels Impacted



7. Avoid/Minimize Impacts
to Cultural and
4(f)/6(f) Resources

18. Number NRHP Listed/Eligible Archaeological Sites Potentially Impacted 19. Number of NRHP Listed/Eligible Historic Structures/Districts Potentially Impacted 20. Number of Identified Section 4(f) and 6(f) Resources Potentially Impacted



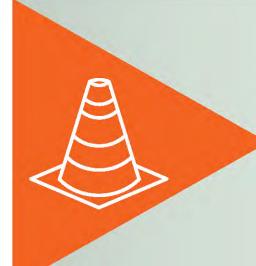
2. Avoid/Minimize Impact to Infrastructure

3. Number of Major Utilities Crossed
4. Railroad Crossing Impacts



8. Avoid/Minimize Impacts to Natural Resources

21. Acres of Mapped Surface Water Features
22. Acres of Mapped Wetland Features by Type
23. Acres of Wetland Soils
24. Acres of Estimated Section 404 Impacts
25. Number of New Navigable Water Crossings
26. Acres of Quality Habitat in ROW
27. Acres of Fish Habitat in ROW



3. Minimize Roadway
Disruptions During
Construction

5. Number of I-10 Full Road Closures6. Number of Arterial Full Road Closures



9. Avoid/Minimize Impacts to Other Resources

28. Number of Sensitive Noise Receivers Immediately Adjacent 29. Potential Visual Impacts from Elevated Structures 30. Number of Potential Substantial Cost Hazardous Materials Sites Impacting Alternatives 31. Potential Impacts to Private Industry Vessels



4. Optimize Cost

7. Estimated Construction Cost
8.Estimated ROW Cost
9. Estimated Operations and
Maintenance Cost



10. Supports/Consistent with Economic Development and Transportation Plans

32. Supports Economic Development
33. Supports Transportation Plans Identified in the MTP and LA STP



5. Minimize Construction Risk in EDC Contamination Area

10. Potential Impacts to Soil Pressure Sampson St. Construction
 11. Potential Impacts to Soil Pressure Calcasieu River Bridge Construction
 12. Potential Impacts to Project Cost and

Schedule



11. Sustain Public and Agency Support

34. Comments Received at Agency Meeting 35. Comments Received at Public Meeting



6. Avoid/Minimize Impacts to Community

- 13. Number of Potential Residential Displacements
- 14. Number of Potential Commercial Displacements
- 15. Number of Potential EJ/LEP Displacements
- 16. Number of Mapped Parks Potentially Impacted
- 17. Number of Public Facilities Potentially Impacted

Acronym Key

ROW = Right-of-Way

EJ = Environmental Justice

LEP = Limited English Proficiency

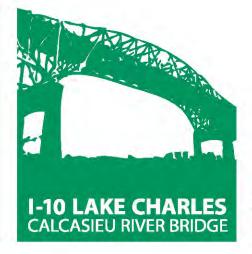
NRHP = National Register of Historic Places

MTP = Metropolitan Transportation Plan

LA STP = Louisiana Statewide Transportation Plan

PRELIMINARY ALTERNATIVES SCREENING

I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE I-10/1-210 WEST END - I-10/1-210 EAST END





Preliminary Alternatives



Tier 1 Purpose and Need Screening

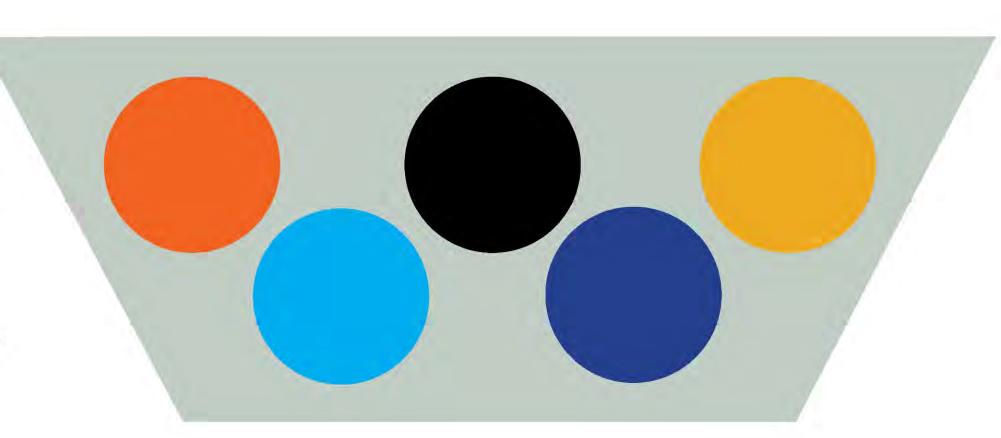
Alternatives Screened Out

TSM & TDM

Existing and planned TSM and TDM measures do not include the physical improvements to provide the magnitude of bene\(\mathbb{Z}\)ts needed to accommodate the needs of the project.

HOV

No foreseeable opportunity for HOV lanes in the Lake Charles area to address the project needs.



Alternatives Passing the Purpose and Need Screening

No Build

Does not meet project needs but serves as baseline condition against which other alternatives are compared.

PBA 1 - PBA 4 (and all Sub-Alts)

Corrects lane imbalance, increases capacity, improves road and bridge de\successciencies and safety.

Tier 2 Project Objectives Screening

Alternatives Screened Out

PBA 1, Sub-Alt F

Driving piles is a potential risk for downward migration of EDC towards aquifer.

PBA 2 and PBA 3, Sub-Alts B & D

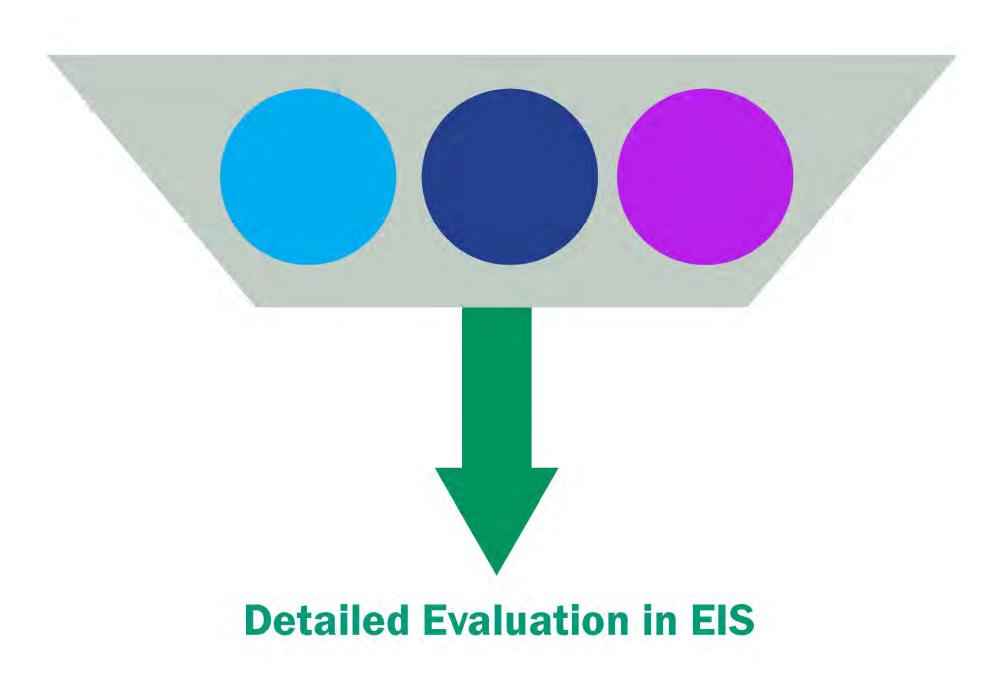
Crossing Entergy Superfund Site not feasible.

PBA 2 and PBA 3, Sub-Alt C

Split Interchange same as A but more costly.

PBA 4, Sub-Alts A & B

Highest cost, impacts to natural resources, and visual impacts.



Recommended Reasonable Alternatives

PBA 2, Sub-Alts A & E

Compensated foundation and Sampson St. Sub-Alternatives avoid/minimize risk of construction in EDC release area.

PBA 3, Sub-Alts A & E

Long-span bridge and Sampson St. Sub-Alternatives designed to avoid/minimize risk of construction in EDC release area.

PBA 5, Sub-Alt G

Long-span bridge and Sampson St. Sub-Alternative as fully directional Sampson Street interchange elevated over I-10 mainline connecting to the bridge on the east and to the at-grade roadway on the west

Recommended Reasonable Alternatives: PBA 2, Sub-Alts A & E PBA 3, Sub-Alts A & E PBA 5, Sub-Alt G

I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE I-10/1-210 WEST END - I-10/1-210 EAST END





OBJECTIVES SCREENING MATRIX - PAGE 1

I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE I-10/1-210 WEST END - I-10/1-210 EAST END State Project No. H.003931



Objectives Screening Evaluation Matrix of Preliminary Alternatives

Objective	(1) Minimize F	ROW Impacts		inimize Impacts to Infrastructure	(3) Minimiz Disruptio Const	ns during		(4) Optimize Co	st	(5) Minimize Cons	truction Risk in EDC Co	ontamination Area	(6) Improve Access at Sampson Street
	A	В	С	D	E	F	G	Н	1	J	К	L	M
Criteria/ Measures	New ROW (acres)	Parcels Impacted (#)	Major Utilities Crossed (#)	Railroad Crossing Impacts (Sampson St.)	I-10 Full Road Closures (#)	Arterial Full Road Closures (#)	Estimated Construction Cost in Millions (M)	Estimated ROW Cost in Millions (M)	Estimated Operations and Maintenance Cost in Millions (M)	Potential Impacts to Soil Pressure - Sampson St Construction	Potential Impacts to Soil Pressure - Calcasieu River Bridge Construction	Potential Impacts to Project Cost and Schedule	Effect on Traffic Movements at Sampson Street
No-Build	0	0	0	HIGH	0	0	\$0	\$0	\$31	LOW	LOW	LOW	LOW
PBA 1-F	61.6	60	102	LOW	48	230	\$599.8	\$28.8	\$0.63 (\$630 K)	HIGH	HIGH	HIGH	LOW
PBA 2-A	84.7	85	100	MEDIUM	48	226	\$770.3	\$34.6	\$1.1	LOW	MEDIUM	MEDIUM	MEDIUM
PBA 2- B	105.3	146	105	MEDIUM	40	214	\$795.2	\$39.4	\$1.1	LOW	MEDIUM	MEDIUM	MEDIUM
PBA 2-C	98.0	110	107	MEDIUM	48	226	\$778.4	\$38.2	\$1.1	LOW	MEDIUM	MEDIUM	MEDIUM
PBA 2-D	122.3	173	112	MEDIUM	40	214	\$803.3	\$41.9	\$1.1	LOW	MEDIUM	MEDIUM	MEDIUM
PBA 2-E	93.5	90	104	MEDIUM	56	222	\$803.6	\$36.7	\$1.1	LOW	MEDIUM	MEDIUM	HIGH
РВА 3-А	85	85	100	MEDIUM	48	226	\$821.0	\$34.6	\$1.1	LOW	LOW	LOW	MEDIUM
РВА 3-В	105	146	105	MEDIUM	40	214	\$845.9	\$39.4	\$1.1	LOW	LOW	LOW	MEDIUM
РВА 3-С	98	110	107	MEDIUM	48	226	\$829.1	\$38.2	\$1.1	LOW	LOW	LOW	MEDIUM
PBA 3-D	122	173	112	MEDIUM	40	214	\$853.9	\$41.9	\$1.1	LOW	LOW	LOW	MEDIUM
РВА 3-Е	93	90	104	MEDIUM	56	222	\$854.2	\$36.7	\$1.1	LOW	LOW	LOW	HIGH
PBA 4-A	174.9	95	131	MEDIUM	64	222	\$990.9	\$27.6	\$1.1	LOW	LOW	LOW	MEDIUM
PBA 4-B	195.0	161	136	MEDIUM	56	214	\$1,012.2	\$31.3	\$1.1	LOW	LOW	LOW	MEDIUM
PBA 5-G	55.6	89	103 1	LOW ²	48	231 ³	\$604.4 1	\$35.5	\$0.75 (\$750 K)	LOW ⁴	MEDIUM ⁵	MEDIUM	LOW
LOW	0-75	0-60	0-99	Eliminates at-grade crossings	0-39	0-100	\$0 - \$450M	\$0 - \$30M	\$0 - \$1M	No construction in EDC area	No construction in EDC area	No additional cost and schedule impacts.	All Movements Directly at Sampson Street
MEDIUM	75-150	61-120	100-120	Reduces vehicular at-grade crossings	40-50	101-200	\$450M - \$900M	\$30M - \$40M	\$1M - \$20M	Foundation concept to equalize or minimize soil pressure	Foundation concept to equalize or minimize soil pressure	Some potential for cost and schedule impacts.	Some Movements Indirectly to/from Sampson Street at a Single Location
HIGH	150 +	121 +	121 +	No reduction in vehicular at-grade crossings	51 +	201 +	\$900M +	\$40M +	\$20M +	Increase in soil pressure	Increase in soil pressure	Increased potential for cost and schedule impacts.	All Movements Indirectly to/from Sampson Street

¹ PBA 5-G would require displacement of 2 pipe racks crossing perpendicular to I-10, one west of Sampson St. and the other east of Sampson St. Note: cost of relocating the pipe racks are not included in the cost estimate above.

² PBA 5-G would elevate Sampson St. over the UPRR and KCS railroad tracks; however, the existing railroad track spur serving the petrochemical facility south of existing I-10 would require relocation due to ground level I-10 construction. The relocation of this railroad track spur would result in two new at-grade crossings of Miller Road – one immediately south of I-10.

³ Includes the full closure of Sampson St. for a minimum of 18-months and partial direction opening of Sampson St. as available. Sampson St. would not be fully opened to traffic until I-10 is relocated.

⁴ PBA 5-G would utilize traditional bridge construction with deep foundations located outside of the EDC.

⁵ PBA 5-G would utilize a shallow foundation with a slab spread footing supported on shallow piles to support light weight fill and MSE walls. Pile depth and frequency of piles would be varied to minimize soil pressures compared to driving deep piles (PBA 1-F). In addition, the I-10 main line lanes would be constructed at an elevated ground level, thereby minimizing soil pressure compared to a multi-level main line.

Objectives Screening Evaluation Matrix of Preliminary Alternatives

						(8) Avoid/Min	imize Impacts to Cult		bjectives S	creening	Evaluation Matri			100				200000	Salar and the salar	
Objective	(7) Avoid/Minimi	ze Impacts t	o Communit	У	(6) Avoid/with	& 4(f)/6(f)	urai Resources			(9) Avoid/Mi	nimize Imp	acts to Natural Res	ources				(10) Avoid/M	Minimize Impacts to 0	Other Resources
Criteria / Measure	Potential Residential Displace- ments (#)	Potential Commercial Displace- ments (#)	Potential EJ/LEP Displace -ments (#)	P Mapped Parks Potentially Impacted (#)	Public Facilities Potentially Impacted (#)	R NRHP Listed/ Eligible Sites Potentially Impacted (#)	NRHP Listed/Eligible Historic Structures & Districts Potentially	T Identified Section 4(f) & 6(f) Resources Potentially	Mapped Surface Water Features (acres)	Mapped (acres)	V Wetland Features (acres by wetland type)	Wetland Soils (acres)	Estimated Section 404 Impacts (acres)	New Crossings of a Navigable Water (#)	Quality Habitat in ROW (acres)	Fish habitat in ROW (acres)	Sensitive Noise Receivers Immediately Adjacent (#)	Potential Visual Impacts from Elev ated Structures	Potential Substantial Cost Hazardous Material Sites Impacting	Potential Impacts to Private Industry Vessels
No-Build	0	0	0	0	0	0	Impacted (#)	Impacted (#)	0	0	Emergent=0 Shrub=0 Forested=0	0	0	0	0	0	0	LOW	Alternatives (#)	LOW
PBA 1-F	3	12	3	2	0	1	1	2	12.87	18.94	Emergent=1.05 Shrub=11.48 Forested=6.40	17.43	Water = 0.22 Wetland = 14.67 Total = 14.89	1	12.97	12.87	173	LOW	1	MEDIUM
PBA 2-A	3	10	3	2	4	1	1	2	18.99	32.12	Emergent=4.55 Shrub=22.32 Forested=5.25	29.75	Water = 0.28 Wetland = 14.70 Total = 14.98	2	26.48	18.99	177	LOW	-1	MEDIUM
PBA 2- B	3	-11	3	2	4	1	1	2	19.65	44.80	Emergent=1.45 Shrub=19.58 Forested=23.76	44.35	Water = 1.80 Wetland = 17.19 Total = 18.99	2	36.77	19.65	196	MEDIUM	3	MEDIUM
PBA 2-C	8	10	3	2	4	1	1	2	16.21	31.31	Emergent=0.53 Shrub=22.85 Forested=7.93	31.32	Water = 0.26 Wetland = 14.72 Total = 14.98	2	27.36	16.21	178	LOW	1	MEDIUM
PBA 2-D	7	10	3	2	4	1	1	2	17.00	44.78	Emergent=2.36 Shrub=35.99 Forested=6.43	45.68	Water = 1.80 Wetland = 17.19 Total = 18.99	2	37.30	17.00	197	MEDIUM	3	MEDIUM
PBA 2-E	3	10	3	2	6	1	1	2	18.14	41.75	Emergent=1.31 Shrub=29.57 Forested=10.87	41.55	Water = 0.23 Wetland = 14.76 Total = 14.99	2	36.63	18.14	177	LOW	1	MEDIUM
PBA 3-A	3	10	3	2	4	1	1	2	18.99	32.12	Emergent=4.55 Shrub=22.32 Forested=5.25	29.75	Water = 0.28 Wetland = 14.70 Total = 14.98	2	26.48	18.99	177	LOW	1	MEDIUM
PBA 3-B	3	11	3	2	4	1	1	2	19.65	44.80	Emergent=1.45 Shrub=19.58 Forested=23.76	44.35	Water = 1.80 Wetland = 17.19 Total = 18.99	2	36.77	19.65	196	MEDIUM	3	MEDIUM
PBA 3-C	8	10	3	2	4	1	1	2	16.21	31.31	Emergent=0.53 Shrub=22.85 Forested=7.93	31.32	Water = 0.26 Wetland = 14.72 Total = 14.98	2	27.36	16.21	178	LOW	1	MEDIUM
PBA 3-D	7	10	3	2	4	1	1	2	17.00	44.78	Emergent=2.36 Shrub=35.99 Forested=6.43	45.68	Water = 1.80 Wetland = 17.19 Total = 18.99	2	37.30	17.00	197	MEDIUM	3	MEDIUM
PBA 3-E	3	10	3	2	6	1	1	2	18.14	41.75	Emergent=1.31 Shrub=29.57 Forested=10.87	41.55	Water = 0.23 Wetland = 14.76 Total = 14.99	2	36.63	18.14	177	LOW	1	MEDIUM
PBA 4-A	5	2	1	2	6	1	14	1	46.00	97.27	Emergent=6.04 Shrub=68.04 Forested=23.19	108.56	Water = 0.20 Wetland = 43.25 Total = 43.45	4	87.75	46.00	179	HIGH	1	HIGH
PBA 4-B	5	3	1	2	6	1	14	1	47.00	105.63	Emergent=7.44 Shrub=81.13 Forested=17.06	117.56	Water = 1.72 Wetland = 45.69 Total = 47.41	4	106.19	47.00	198	HIGH	3	HIGH
PBA 5-G	10	10	3	1	0	1	1	2	16.33	18.88	Emergent=1.05 Shrub=6.36 Forested=11.47	15.28	Water = 0.22 Wetland = 14.67 Total = 14.89	1	18.37	16.33	167	LOW	2	LOW
LOW	0-2	0-8	0	0	0-2	0	0	0	0-10	0	0-30 (Total)	0-25	0-10 (Total)	0	0-20	0-10	0-90	No to Some Slightly Obstructed Views	0	No impact to Friend Ships' vessels; OR no additional bridge crossings
MEDIUM	3-5	9-10	1	1	3-5	1	1-10	1	10-20	3	0-60 (Total)	25-50	10-20 (Total)	1-2	20-40	10-20	91–180	More Obstructed Views	1-2	Vertical clearance impacts to Friend Ships' vessels; additional moveable crossing only
HIGH	6+	11.+	2+	2+	6+	2+	11+	2	20 +	6	60 + (Total)	50 +	20 + (Total)	3+	40 +	20 +	181 +	Most Obstructed Views	3+	Vertical clearance impacts to Friend Ships' vessels; additional moveable crossing & non-moveable crossings



Objective		with Economic Development ortation Plans		istent with Economic Transportation Plans	
	FF	GG	нн	=	
Criteria/ Measures	Supports Economic Development	Supports Transportation Plans Identified in MTP and LA STP	Comments Received at Agency Meeting #2	Comments Received at Public Meeting #2	
	GENERALLY NOT SUPPORTED	GENERALLY NOT SUPPORTED	GENERALLY NOT SUPPORTED	GENERALLY NOT SUPPORTED	
	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	NEUTRAL	
	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	NEUTRAL	
	GENERALLY SUPPORTED	GENERALLY SUPPORTED	NEUTRAL	NEUTRAL	
	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	NEUTRAL	
	GENERALLY SUPPORTED	GENERALLY SUPPORTED	NEUTRAL	NEUTRAL	
	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	NEUTRAL	
	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	GENERALLY SUPPORTED	
	GENERALLY SUPPORTED	GENERALLY SUPPORTED	NEUTRAL	GENERALLY SUPPORTED	NOTE: To be completed
	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	GENERALLY SUPPORTED	Agency and Public Me
	GENERALLY SUPPORTED	GENERALLY SUPPORTED	NEUTRAL	GENERALLY SUPPORTED	
	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	GENERALLY SUPPORTED	
	NEUTRAL	GENERALLY SUPPORTED	GENERALLY NOT	GENERALLY NOT	
	GENERALLY SUPPORTED	GENERALLY SUPPORTED	SUPPORTED GENERALLY NOT SUPPORTED	SUPPORTED GENERALLY NOT SUPPORTED	
	NEUTRAL	GENERALLY SUPPORTED			
GENERALLY SUPPORTED	Improvements generally support established economic development goals	Generally supports/consistent with MTP and STP	Generally positive comments	Generally positive comments	
NEUTRAL	Potential exists for economic dev elopment opportunities	Neutral	No comments received or number of positive and negative comments were generally equal	No comments received or number of positive and negative comments were generally equal	
GENERALLY NOT SUPPORTED	No improvements to support established economic development goals	Does not support/inconsistent with MTP and STP	Generally negative comments	Generally negative comments	

ed following leeting #3



PBA 1 (Driven Piles) Sub-Alt F



PBA 2 (Compensated Foundation) Sub-Alt A PBA 3 (Long Span) Sub-Alt A



PBA 2 (Compensated Foundation) Sub-Alt B PBA 3 (Long Span) Sub-Alt B



PBA 2 (Compensated Foundation) Sub-Alt C PBA 3 (Long Span) Sub-Alt C



PBA 2 (Compensated Foundation) Sub-Alt D PBA 3 (Long Span) Sub-Alt D



PBA4 (Southern Bridge) Sub-Alt A PBA4 (Southern Bridge) Sub-Alt C



PBA 2 (Compensated Foundation) Sub-Alt E PBA 3 (Long Span) Sub-Alt E



PBA5 (Retaining Wall) Sub-Alt G

Appendix C-9: Station 9: We Want to Hear From You! Exhibits

WE WANT TO HEAR FROM YOU!

AT THIS PUBLIC MEETING

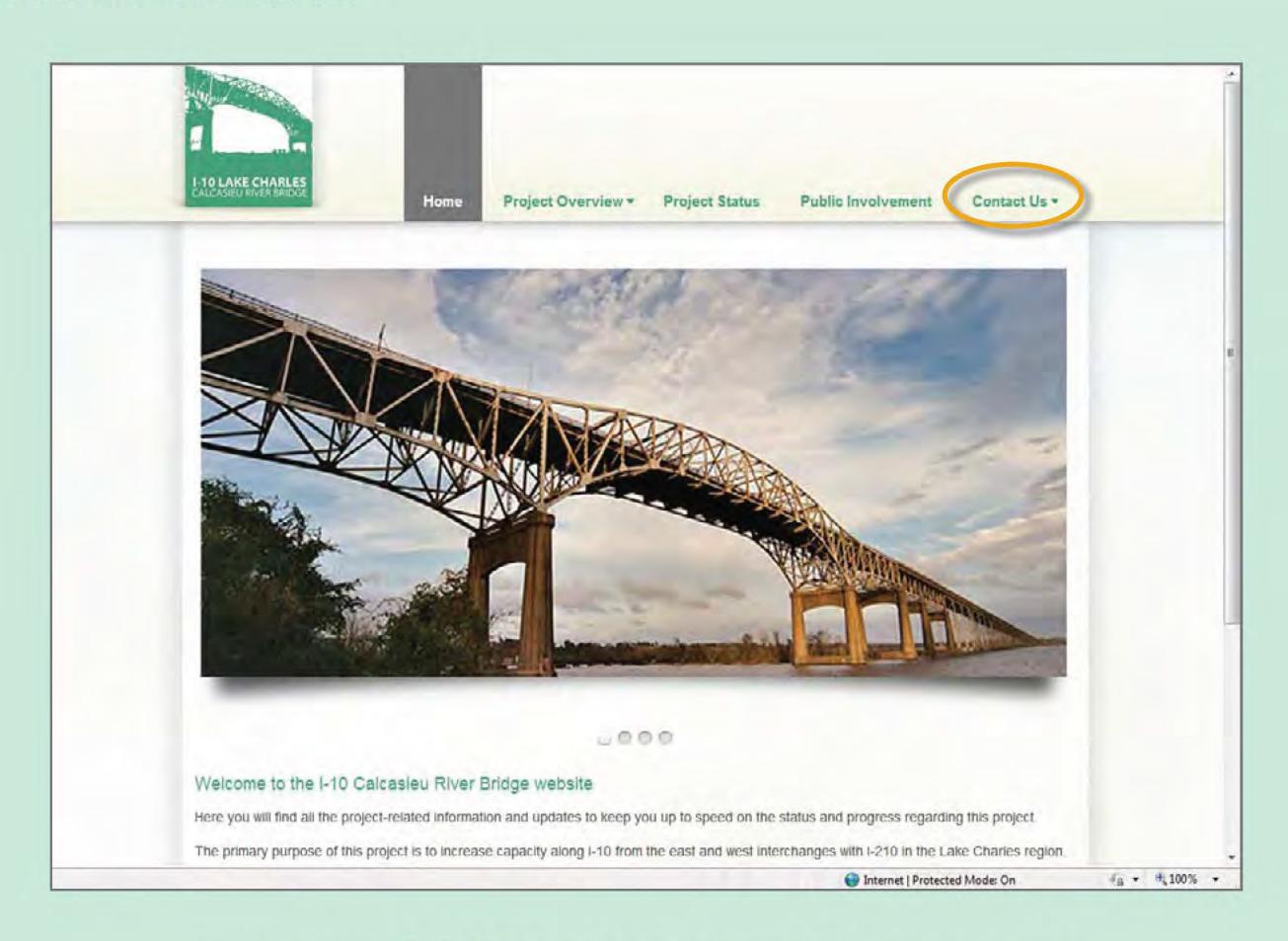
Written Comments will be accepted at this public meeting at the Comment Table or by mailing your comment to the following address:

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 2021 Lakeshore Drive, Suite 230 New Orleans, LA 70122

Verbal Comments will be accepted tonight at the Comment Table.

PROJECT WEBSITE

Written comments will also be accepted by logging on to the project website and selecting the *Contact Us* tab.



www.i10lakecharles.com

Comments on the project will be solicited for 45 days following the public meeting. Comments must be submitted/postmarked within 10 days of the public meeting to become part of the official public meeting record.

Appendix D-1: Comments Received

Richard Harbison Manager Lake Charles Relinery

PHILLIPS 66 2200 Old Spanish Trail Westlake, LA 70669

May 3, 2019

Via U.S. Mail

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe, Suite 640 Baton Rouge, LA 70810

Re

Written Comments to the April 25, 2019 Public Meeting #3 State Project No. H.003931 Calcasieu River Bridge Project

To Whom It May Concern:

I am writing on behalf of Phillips 66 Company to provide comments on the information presented at the April 25, 2019 public meeting for the I-10 Calcasieu River Bridge Project. Consistent with the information provided during that meeting. I ask that this letter and attachments be included in the official public meeting transcript and record. These comments supplement the written comments on the project that we submitted on August 14, 2017 and September 15, 2017.

As you know, Phillips 66 owns and operates facilities that will be directly affected by potential changes to the I-10 Calcasieu River Bridge. The company fully supports thoughtful improvements to the Calcasieu River crossing, which should greatly benefit the public. Phillips 66 appreciates DOTD's invitation to the recent stakeholder meeting; however, we had requested a meeting with DOTD on several occasions to provide DOTD with updated EDC remediation information and current site conditions, as well as to discuss our written comments to the bridge designs presented at the August 3, 2017 public meeting. Unfortunately, the meeting did not take place. We believe DOTD's consideration of the most current data would and should change DOTD's recommended reasonable alternatives to include PBA 1-F, and therefore PBA 1-F should not be screened out at this time.

As discussed in this comment letter. Phillips 66 has continuously remediated the site and monitored the EDC attenuation in the groundwater. Attachment 1 summarizes monitoring results from 2015-2017, and shows the estimated distribution of EDC in groundwater as of the Fourth Quarter 2017. The site conditions improved dramatically from 2015 to 2017, and continue to improve. We disagree with DOTD's reliance on First Quarter 2016 data and its risk assessment of construction in the EDC area to rule out PBA 1-F from being evaluated in detail in the EIS. The most up-to-date data shows that the project can proceed in accordance with traditional bridge design and construction methods. There are no site conditions or environmental impacts that require a re-design to avoid the EDC area.

Even though these issues were raised in Phillips 66's previous comment letters on the project. DOTD is still threatening to withhold from complete analysis in the draft EIS a clearly reasonable alternative, PBA 1-F, based on flawed assumptions on environmental impact, stale and incomplete data, and without conducting the appropriate technical studies necessary to make such a decision. But for the assumptions DOTD has made about the theoretical impact pile foundations would have in the EDC area, PBA 1-F is the most reasonable alternative presented thus far. A decision not to pursue study of PBA 1-F would be an arbitrary and capricious decision, contrary to NEPA's requirements. See, W. Watershed Project v. Abbey, 719 F.3d 1031, 1050 (9th Cir. 2013) ("agency errs when it relies on old data, without showing that the data remains accurate"). A decision not to evaluate PBA 1-F in the draft EIS would deprive the public and the process of the most cost-effective alternative that ultimately could have the least impact on the environment.

I. Comments on Proposed Preliminary Build Alternatives

- It is not acceptable for DOTD to use EDC contamination data from 3 years ago (First Quarter 2016) for the PBA screening and not update it with readily available quarterly monitoring data and estimated plume delineation. The EDC well monitoring results have greatly improved since First Quarter 2016 (see Attachment 1), and DOTD's consideration of the most current data on the EDC site conditions would and should change DOTD's recommended reasonable alternatives to include PBA 1-F
- 2. PBA 1-F is Phillips 66's preferred alternative, and any concern over adverse impacts to the subsurface environment should be studied in the EIS rather than ruling out PBA 1-F at this stage of the NEPA process. The detailed evaluation in the EIS includes evaluation of ways to avoid, minimize, or mitigate potential environmental impacts associated with all proposed reasonable alternatives for the project.
- In 2010, DOTD explained in a report to FHWA that "if the decision is made to allow driven piles above contaminated strata, then it is currently considered feasible to construct the previous proposed foundations for the clevated mainline and north frontage roads. To allow relatively shallow piles which would be required for the south frontage road, soil-cement mixing is recommended to improve the load carrying capacity of the soil above a depth of 25 feet," Particularly because significant attenuation of the EDC has occurred since 2010, DOTD should acknowledge its previous statement that designs with driven piles are engineering solutions that "demonstrate that an acceptable approach is available to implement the project while managing risk associated with the hazardous materials issue." Therefore, PBA 1-F should be studied in the EIS.
- In comments to the public meeting #2 in 2017, LDEQ again approved driving piles in the EDC area as it had since 2009, to "piling depths of 75 feet below current existing

grade or less north of the current I-10 footprint" and not to "exceed a depth of 40 feet below current existing grade south of the current I-10 footprint" with the exception of the area east of the specified eastern boundary measured by CPT boring locations (see EDMS Document ID# 6754900 for reference points). Likewise, the EPA Sole Source Aquifer Office has expressed no objections to the project. In 2013, the SSA Office sent a letter to DOTD that it evaluated "the potential environmental impact which might result from changes to a previously approved project" and "determined that the project, as proposed, should not have an adverse effect on the quality of the ground water underlying the project site." Therefore, PBA 1-F should be studied in the EIS.

- 5. It is not acceptable to rule out PBA 1-F without a comprehensive analysis of likely piling depths in relation to the subsurface depths of EDC, particularly in the areas north of the existing 1-10 bridge where EDC has never been detected in depths from 0 to 55 feet below ground surface and it is unlikely piling depths would exceed 55 feet in this area.
- 6. Regarding DOTD's comment H-1 in the Public Involvement Summary Report, PBA 1-F, DOTD acknowledges that the EDC contamination in relation to the project alternatives will be evaluated in the EIS based on available information in coordination with LDEQ. DOTD has committed that "if a proposed alternative with the potential to encounter EDC is selected as the Preferred Alternative. DOTD would follow the proper procedures to ensure the safety of its employees, contractors, and the public," DOTD's comment H-2 in the same report likewise confirms that the potential impact of EDC on the integrity of soils, along with other mitigating factors, will be considered during the evaluation process of the reasonable alternatives. Therefore, PBA 1-F should be studied in the EIS.

Discussion

A. Stale and incomplete EDC data

The presentation slides at public meeting #3 describing PBA 1 and PBA 2 and other exhibits contain a disclaimer that reads:

Disclaimer:

Unknowns remain about the full extent, depth and migration of EDC

EDC contamination area shown based on First Quarter 2016 well monitoring data

As shown below, PBA 2, which is proposed as a reasonable alternative to be studied in the EIS, "utilizes a compensated foundation in the EDC area... It would be constructed above the known EDC contamination depth." The slide shows only an "Approx. EDC Area."



DOTD is proposing as a reasonable alternative a bridge that will be constructed in the EDC area above the EDC contamination which has yet to be evaluated by DOTD. PBA 2 is included for further evaluation in the EIS and will require an updated review of the EDC contamination, yet PBA 1-F is being screened out without an updated EDC review. According to DOTD's manual for Stage 1 — Planning/Environmental Manual of Standard Practice (January 2018, p. 33), the previous meeting materials should be updated before the preliminary alternatives outreach meetings.

Stakeholder Comment on Honge of Alternatives

th election of the preferringly encountry automatic materials materials present if the previous access meeting is separately upon the information previously previous installation should include

The EDC information has not been updated since the public meeting #2 in 2017, where DOTD erroneously depicted an EDC plume in areas north of 1-10. This representation of a "plume" north of the existing 1-10 bridge is inaccurate. Phillips 66 has installed 12 groundwater monitoring wells in the marsh area between the existing 1-10 bridge and the Union Pacific Railroad tracks located north of the marsh and collected quarterly samples since 2011. While there have been historical detections of EDC in groundwater beneath the marsh area, it is a fact that the concentrations of EDC in samples collected from monitoring wells in the marsh area do not exceed the cleanup standard established by LDEQ. Therefore, there is no regulatory need for any cleanup whatsoever in the marsh area. More crucial to the point, through a combination of Phillips 66's recovery efforts south of 1-10 and natural attenuation over the last 8 years, the EDC concentrations beneath the marsh area have decreased. The data that the DOTD used to depict a "plume" in the path of the proposed bridge is 3 years old, and information collected since then indicates the plume DOTD illustrates is not accurate. (See Attachment 1).

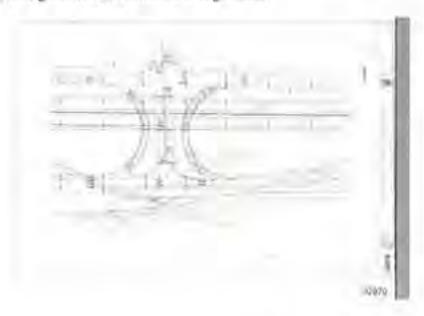
Accordingly, the potential risk for downward migration of EDC towards the aquifer should not be a factor. The piling or ground improvement method of choice will not breach

a low permeability layer (aquitard). The primary aquitard protecting the Chicot aquifer is below the interbedded unit at a depth of approximately 75 feet, and this aquitard would not be breached physically, or from the displacement/pore pressure effects of a driven pile, if a properly designed bridge support system is used. It is unlikely that piling depths would exceed 50-55 feet below ground surface. Therefore, PBA 1-F cannot be ruled out until a more detailed analysis of likely piling depths is performed based on current geotechnical data.

All of the recommended reasonable alternatives require that the EDC contamination area be fully studied and delineated with current data to determine design details for the compensated foundation above the EDC contamination depth (PBA 2), long-span bridge over the EDC contamination area (PBA 3), the bridge ending at the eastern boundary of the EDC contamination area (PBA 5), and interchange retaining wall on top of the EDC contamination area (Station 1, Project Features handout). This updated environmental and geotechnical data is likewise necessary to determine whether or not driven piles will necessarily be driven in EDC contamination (PBA 1), as opposed to above or outside the EDC contamination area, and whether another construction method, such as a drilled shaft method, is a viable alternative to avoid or minimize construction risk in the EDC contamination area.

B. DOTD's recommended feasible design with driven piles in 2010

The feasible design offered by DOTD in 2010 is similar to PBA 1-F. In DOTD's May 26, 2010 status report to FHWA seeking FHWA's concurrence in re-starting the NEPA process. DOTD explained the most likely alternative for the Sampson Street interchange and bridge that had been advancing through the NEPA process that was suspended in 2006. DOTD illustrated the feasible design by superimposing the contamination plume at that time with the proposed geometry for the elevated 1-10 mainline bridge, elevated Sampson Street ramps, and ground level south frontage road.



DOTD explained to FHWA that "if the decision is made to allow driven piles above contaminated strata, then it is currently considered feasible to construct the previous proposed foundations for the elevated mainline and north frontage roads. To allow relatively shallow piles which would be required for the south frontage road, soil-cement mixing is recommended to improve the load carrying capacity of the soil above a depth of 25 feet." (DOTD to FHWA Status Report, p. 9). Alternatively, DOTD stated that structures could be designed to place the foundations outside the contamination boundary. DOTD explained both solutions are feasible and "an acceptable approach is available to implement the project while managing risk associated with the hazardous materials issue,...," DOTD further stated: "A decision on which engineering solution to adopt can be made as part of the NEPA process or the detailed design phase." Id.

The same conclusion should apply here for PBA 1-F, a similar bridge design alignment with driven pile foundation in the EDC area. The decision on which engineering solution to adopt can be made part of the NEPA process or the detailed design phase. Therefore, there would be no environmental risk to using a traditional driven piles design to the LDEQ-approved depths even using DOTD's risk management recommendations based on the environmental data from 2010.

C. LDEQ guidance for driven pile design

LDEQ has issued piling depth guidelines for the areas south of I-10, and the project can safely proceed in accordance with those guidelines. As the project advances and precise piling locations are determined. Phillips 66 will work cooperatively with LDEQ and DOTD to ensure that the bridge construction activities are done safely and without added risk to the Chicot aquifer.

After the Notice of Intent was published in 2013, DOTD issued a Solicitation of Views letter on September 9, 2013, and held its first public meeting in October, 2013. LDEQ's response to the SOV letter on November 5, 2013, reconfirmed its guidance on piling depths provided in its 2009 and 2010 letters to DOTD, pending final determinations of piling locations and depths:

Based on LDEQ's previous involvement in the area potentially impacted by this project, we offer these specific comments:

- In the absence of final determinations of piling locations and LDOTD-proposed depths, it is not
 possible to provide specificity in recommending depths which would be protective of the subsurface
 environment. i.e., avoiding exacerbation of current groundwater conditions.
- As per the correspondence from LDEQ to LDOTD dated November 19, 2009, LDEQ would have no
 objection to piling depths of 75' below corrent existing grade or less north of the current I-10 footprint.
 While pilings may be placed within contamination zones, this depth would not penetrate zones currently
 uncontaminated by EDC.
- South of the current 1-10 footprint, no piling should exceed a depth of 40' below current existing grade with the exception of the following: Using a line drawn from CPT18, CPT7, and a point 50' due

east of 18 as a reference, there would be no depth restrictions to the cast of this line (See EDMS Document ID # 6754900 for reference points).

Please contact Laura LeBouef by phone at (225) 219-3033 or by email at laura lebouefacta gov with any further questions or concerns.

Nov 5, 2013 letter, at http://i10lakecharles.com/images/documents/PublicandAgencyScopingMeetings_October24 2017/Appendix%20A_updated.pdf at pp. 32-33 of 46.

LDEQ again confirmed its position on allowable piling depths in 2017, in official comments to LDOTD's draft Alternatives Screening Methodology (ASM), http://www.i10lakecharles.com/images/documents/OtherPublicandAgencyInvolvement/Alternatives_Screening_Methodology_ASM.pdf_The ASM outlined all Preliminary Build Alternatives and Preliminary Sub-alternatives, including PBA T with "Calcasieu River Bridge construction using piles in the EDC contamination area," Id. at p. 5 of 23. LDEQ's comments included the same guidance on piling depths as in 2013. DOTD responded that it would coordinate with LDEQ if an alternative requiring driving piles is selected as the Preferred Alternative.:

Response 2: Comment mixed. It is DOTD's intention to unitamize the risk to the Chicot Aquifer.
If an alternative requiring driving piles in the EDC area is selected as the Preferred Alternative.
DOTD would coordinate with LDEQ on appropriate depths. DOTD is committed to working with
LDEQ on contamination issues as the project marks forward.

Agency Comments and Responses report at http://www.i10lakecharles.com/images/documents/PublicandAgencyMeetings_August0320/ 17/Attachment%20D.pdf_at p. 5 of 100.

D. EPA preliminary approval of the project alignment and driven pile design

The EPA Sole Source Aquifer (SSA) Office has expressed no objections to the project. The EPA SSA Office issued letters in 2000, 2005, and 2009, stating that it had not identified a hazard to ground water quality as a result of the proposed project based upon project information provided to it at that time. Notably, the 2009 letter was to LDEQ in response to a request from Laura LeBouef for input on what DOTD can and cannot do with respect to the construction and the location of the contamination so that LDEQ could prepare the guidance document for LDOTD referenced above.

After the NEPA restart in 2013, the EPA offered two responses to the SOV letter: (1) a letter from the Office of Planning and Coordination outlining areas to be addressed in the EIS, including whether "the project lies over a SSA and how the project may affect the aquifer" and (2) a letter from the SSA Office. The SSA Office evaluated "the potential environmental impact which might result from changes to a previously approved project" and "determined that the project, as proposed, should not have an adverse effect on the quality of the ground water underlying the project site." [Emphasis supplied]

http://i10lakecharles.com/images/documents/PublicandAgencyScopingMeetings_October24 2017/Appendix%20A_updated.pdf at p. 6 of 46.

Thus, the EPA SSA Office has followed the development of the project since 2000, and has not identified any potential adverse effects to the Chicot Aquifer. Further, the EPA expects that any potential effects on the aquifer will be identified in the EIS along with statements of other environmental impacts required under federal regulations.

E. DOTD responses to public comments in 2017

DOTD published a Public Involvement Summary Report after the 2017 public meeting. DOTD's comment H-1 acknowledges that the EDC contamination in relation to the project alternatives will be evaluated in the EIS based on available information in coordination with DEQ. DOTD has committed that "if a proposed alternative with the potential to encounter EDC is selected as the Preferred Alternative, DOTD would follow the proper procedures to ensure the safety of its employees, contractors, and the public."

- market	General Trinin Addressed	A CONTRACTOR OF THE PARTY OF TH
s-y	£1/iS contamination	The EDC contamination in milition in the proposed project will be evaluated in the EIS based on available information and to the extent practicable in coordination with Louissanta Department of Environmental Quality (LDEQ). Assessment and remediation of the EDC spill is the responsible for its release. Philipsob LDEQ is working with Primpality responsible for its release. Philipsob LDEQ is working with Primpality in the responsibility of the EDC contamination. With monitoring lindings are available to the public through the LDEQ Clicchool Document Management System (EDMS). If a proposed afternative with the primital to encounter EDC is selected as the Preferred Attemption, (ICF) is would follow the prime procedure. In ensure the safety of its improved contractors, and the public.

See DOTD Public Involvement Summary Report (Full report at http://i10lakecharles.com/images/documents/PublicandAgencyMeetings_August032017/Summary%20Report.pdf). It is not acceptable to rule out PBA 1-F without a comprehensive analysis of likely piling depths in relation to the subsurface depths of EDC, particularly in the areas north of the existing 1-10 bridge where EDC has never been detected in depths from 0 to 55 feet below ground surface and it is unlikely piling depths would exceed 55 feet in this area.

Likewise, DOTD's comment H-2 in the Public Involvement Summary Report confirms that the potential impact of EDC on the integrity of soils, along with other mitigating factors, will be considered during the evaluation process of the reasonable alternatives.

Westake municipal water wells just north
of the rairroad seem to be drawing the
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EDC would destroy the soil beneath
Sub-Arts A-E and PBAs 2 and 3.

The potential impact of EDC on the integrity of soils, along with other misgating factors, will be considered during the evaluation process of the Reasonable Alternatives and beyond regardless to which Alternative is selected as "Preferred".

See DOTD Public Involvement Summary Report (Full report at http://i10lakecharles.com/images/documents/PublicandAgencyMeetings_August032017/Summary%20Report.pdf)

Thus, there is simply no reason to screen out PBA 1-F where the single risk category identified by DOTD in the matrix relative to the other alternatives is "potential impacts to soil pressure" of the bridge and interchange, and associated "potential impacts to project cost and schedule" - concerns that DOTD considered manageable, feasible, and acceptable in 2010. Subsurface EDC conditions have significantly improved in the subsequent 9 years.

11. Comments on the Alternatives Screening Methodology and Results

- 7: The public has been provided an illustrative matrix showing categories of objectives for screening the PBA's and a draft Preliminary Alternatives Screening Methodology. No environmental site assessments, geotechnical investigations, or similar reports used in the alternatives development and analysis were presented or made available. Based on the limited information provided, Phillips 66 has serious concerns and questions for several of the designs for the bridge and interchange chosen by DOTD for evaluation as "recommended reasonable alternatives."
- Phillips 66 requests that the following information be made available to the public to show the data relied on for the PBA screening analyses:
 - I-10 Calcasieu River Bridge Preliminary Alternatives Screening Methodology (final version)
 - Alternatives Analysis Technical Report and the data and sources used to complete the matrix section "Construction Risk in EDC Contamination Area"
 - Technical reports for the Project, including but not limited to wetland impacts, cultural resources, air quality, traffic noise, hazardous materials, water quality impacts, Phase 1 Environmental Site Assessment, Section 4(f)/6(f) resources, and threatened or endangered species.
- 9. DOTD did not present any detail on the current EDC delineation, remediation efforts, risk assessment, risk management principles, geotechnical studies, likely piling depths and locations for PBA 1-F, or other basis for avoiding the EDC contamination other than the blanket statement that for PBA 1-F, "driving piles is a potential risk for downward migration of EDC towards aquifer." This information should be made publicly available.
- In relation to the other proposed reasonable alternatives, the only area of high risk assigned by DOTD to PBA 1-F out of 12 project objectives categories was as to category 5 "Minimize Construction Risk in EDC Contamination Area." Yet DOTD disclaimed knowledge "about the full extent, depth and migration of EDC" in its

presentation materials, and pledged to fully evaluate the EDC contamination in relation to the project alternatives in the EIS. Using updated EDC data would change the relative risk rating on this objective favorably for PBA 1-F.

11. An adequate NEPA analysis allows an agency and the public to assess and compare the potential environmental impacts of reasonable alternatives. In this way, better decisions can be made to ensure that possible adverse impacts be avoided or mitigated, and that the investment of public infrastructure dollars is done wisely. By narrowing down a clearly reasonable alternative such as PBA 1-F before detailed study. DOTD has curtailed the opportunity to compare the impact and costs of that option, which otherwise has the most favorable results in the matrix, along with PBA 5-G, for the 12 objectives DOTD identified for the project.

Discussion

DOTD has published a new Objectives Screening Evaluation Matrix of Preliminary Alternatives added new PBA 5-G. According to the meeting presentation, PBA 5-G was developed "to address comments received during the previous public meeting about an interchange at Sampson Street." In addition, category 6 objective "Improved Access at Sampson Street" was added to the screening matrix. https://il0lakecharles.com/images/documents/Virtual%20Meeting/Station8c_AlternativesScreeningMatrices.pdf

DOTD announced that it is recommending PBAs 2A, 2E, 3A, 3E, and 5-G as the reasonable alternatives to proceed with detailed evaluation in the EIS. We have isolated those PBAs, along with PBA 1-F, in the reducted matrix in Attachment 2 for easier comparison of the PBAs.

According to DOTD's presentation on the Tier 2 objectives screening, "In general, alternatives with the lowest potential impacts are considered more favorable than alternatives with the highest potential impacts." The table below is a summary of the screening results for the selected PBAs. A "<" means the alternative had the most favorable result for that objective category, and an "X" means the alternative had the highest potential impact in that category. Comparing these 6 PBAs reveals that PBA 1-F and PBA 5-G are clearly more favorable alternatives than PBAs 2 and 3 based on the Tier 2 screening results.

Recommended Reasonable Alternatives and PBA 1-F Results of Objectives Screening Evaluation

	OBJECTIVE	PBA 1-F	PBA 2-A	PBA 2-E	PBA 3-A	PBA 3-E	PBA 5-G
1	Minimize ROW Impacts	1	-			-	1
2	Avoid/Minimize Impacts to Existing Infrastructure	*					Å)
3	Minimize Roadway Disruptions During Construction			X		X	
4	Optimize Cost	*					*
5	Minimize Construction Risk in EDC Contamination Area	Х			~	Y	
6	Improve Access of Sampson Street	*		X		X	*
7	Avoid/Minimize Impacts to Community	V2.					
8	Avoid/Minimize Impacts to Cultural Resources & 4(D/6(f)			Com	parable		
9	Avoid/Minimize Impacts to Natural Resources	V.					1
10	Avoid Minimize Impacts to Other Resources						
11	Supports/Consistent with Economic Development and Transportation Plans			Com	parable		
12	Supports/Consistent with Economic Development and Transportation Plans				*	4	N/A

The results of the project objective screening analysis clearing warrant and require PBA 1-F to be a reasonable alternative. PBA 1-F has a similar alignment and access to and from Sampson Street to PBA 5-G and similar positive screening results. PBA 5 "MSE Wall over EDC area" and sub-alternative G "utilizes a retaining wall and shallow foundations for the construction of the I-10 mainline and the Sampson Street interchange in the vicinity of the EDC. It also utilizes existing I-10 foundation piles for construction of some ramps to the interstate." However, PBA 5 includes the relocation of two sections of railroad tracks and relocation of the pipe racks crossing over the interstate. According to the presentation, it also would require partial closure of the existing Sampson Street during construction

PBA 5-G would require displacement of 2 pipe racks crossing I-10. Cost of relocating the pipe racks are not included in the cost estimate. PBA 5-G would also require relocation of 2 railroad track spurs.

PBA 1-1 had 20 potential impacts/displacements compared to 22 and 24 for the other PBAs.

which is projected to last for 48 months. Full closure of Sampson Street would last for 18-24 months. These impacts are footnoted on the matrix but are not, or do not appear to be considered in evaluating the objectives for (a) Major Utilities Crossed, (b) Railroad Crossing Impacts (Sampson Street), or (c) Estimated Construction Cost. Considering these impacts along with the construction planned above the EDC contamination, there is no logic in recommending PBA 5-G as a reasonable alternative and not recommending PBA 1-F.

PBAs 2 and 3-A and E require a new movable bridge-a low bridge with a "movable" deck—for the extension of Sulphur Avenue over the Calcasieu River. There is a high potential of risk associated with the operations of a movable bridge, as well as ongoing significant operational and maintenance costs. Curiously, in the screening matrix under New Crossings of a Navigable Water, DOTD assigned medium risk impact to PBA 2 and 3's two bridges (one movable) across the Calcasieu River, as well as PBA 1-F and 5-G's one non-movable bridge across the river. This "equal" rating is particularly egregious when one considers the movable bridge will severely restrict river traffic when the bridge is down or being raised. PBAs 1-F and 5-G should have received a lower impact rating than the other sub-alternatives for this objective.

Also, PBAs 2 and 3 involve significantly more impacts to natural resources (category 9) than PBAs 1 and 5. The impacts to each feature or habitat are measured in acreage, While PBAs 1 and 5 would impact 90 and 100 acres of natural resources, respectively. PBAs 2 and 3-A and E would impact considerably more — 141 acres and 171 acres. That's an environmental impact 56% to 90% more on our natural resources than PBA 1-F would have. This is a significant factor that weighs in favor of evaluating PBA 1-F further.

Similarly, DOTD measured the Estimated Construction Cost for these alternatives at equal impact—all medium impact. However, the estimated costs of PBAs 2 and 3 are significantly higher than PBAs 1 and 5, and should have a corresponding higher impact rating. PBAs 2 and 3 have an estimated construction cost approximately 28%-42% higher than PBA 1-F (a difference of \$170MM-\$254MM). This is also a significant factor that weighs in favor of evaluating PBA 1-F further.

DOTD's presentations states the "Recommendation of Reasonable Alternatives based on professional judgement with consideration given to ALL project objectives." The exercise of professional judgment here based on the results of the project objective analysis clearing warrants and requires PBA 1-F to be a reasonable alternative, and DOTD should analyze the current EDC site conditions in the EIS to allow a complete comparison of reasonable alternatives.

111. Comments on Recommended Reasonable Alternatives for further evaluation in the Environmental Impact Statement

12. PBA 1-F must be chosen as a reasonable alternative to be studied in detail in the EIS, and is Phillips 66's preferred alternative. Under NEPA, an agency may not rule out feasible alternatives that meet the stated project needs. DOTD's proposed elimination

of a reasonable and feasible project alternative, PBA 1-F, is arbitrary and capricious because it is based on outdated and incorrect information on potential environmental impacts, it is without consideration of relevant information from LDEQ and Phillips 66 regarding site conditions, and it is without sufficient study of potential environmental impacts in relation to the proposed design. DOTD's failure to objectively evaluate all reasonable and feasible project alternatives taints the NEPA analysis.

- Of the PBAs that have been recommended to proceed for detailed evaluation in the EIS, Phillips 66 accepts PBA 5-G as a second choice behind PBA 1-F.
- Phillips 66 is not in favor of PBAs 2A. 2E. 3A or 3E, and they should not be reasonable alternatives evaluated in the EIS for the reasons stated in these comments.

By eliminating at this stage PBA 1-F a feasible design that meets the Purpose and Need of the project, DOTD is preventing the assessment and comparison of environmental impacts of PBA 1-F with current EDC data, which an EIS is designed to facilitate, while advancing more expensive alternatives with greater potential impacts to natural resources, infrastructure and road conditions during construction, and that fail the new important objective of having an interchange at Sampson Street. PBA 1-F has a similar alignment to PBA 5-G and similar screening results, and should likewise be studied in the EIS.

NEPA, 42 U.S.C. §§ 4321, et seq., requires federal agencies to take a "hard look" at the environmental consequences of its actions, and that decision-making is subject to full public disclosure and participation. The EIS must "[r]igorously explore and objectively evaluate all reasonable alternatives." including a "no action" alternative. 40 C.F.R. § 1502.14; see 23 C.F.R. § 771.123(c). Indeed, the mandate to assess all reasonable and feasible alternatives to a proposed action has been called "the heart of an EIS." DOTD is prohibited by federal regulation from prejudicing the process at any time by making a predetermined selection of the preferred alternative. That includes an explicit regulatory prohibition from "committing resources" in a manner that would "prejudic[e] [the] selection of alternatives before making a final decision." 40 C.F.R. § 1502.2(f).

Ultimately, the engineering solution for the Calcasieu River Bridge should be driven by an objective analysis conducted under well-established NEPA procedures. DOTD's role as the agency partner with FHWA is to rigorously explore and objectively evaluate all reasonable alternatives, taking into account the potential environmental impacts, and economic and technical considerations, 40 CFR §§1501.2(a)-(c) and §1502.14; 23 CFR §771.123(c). At a minimum, preservation of a feasible build alternative that has the potential to save the state's taxpayers millions of dollars is in the best interest of the project. Phillips 66 most strongly supports inclusion of the PBA 1-F alternative in the draft EIS. We remain confident that if this option is considered fairly and subject to full public participation, its feasibility will be evident.

In accordance with the NEPA process, Phillips 66 requests that its input be considered and DOTD's recommendations modified based on these comments. Phillips 66 specifically requests DOTD's responses to the 13 numbered comments listed in this letter, as well as the project information identified in comment 8 for review by the public and project stakeholders.

Very truly yours,

Richard G. Harbison

ATTACHMENT 1



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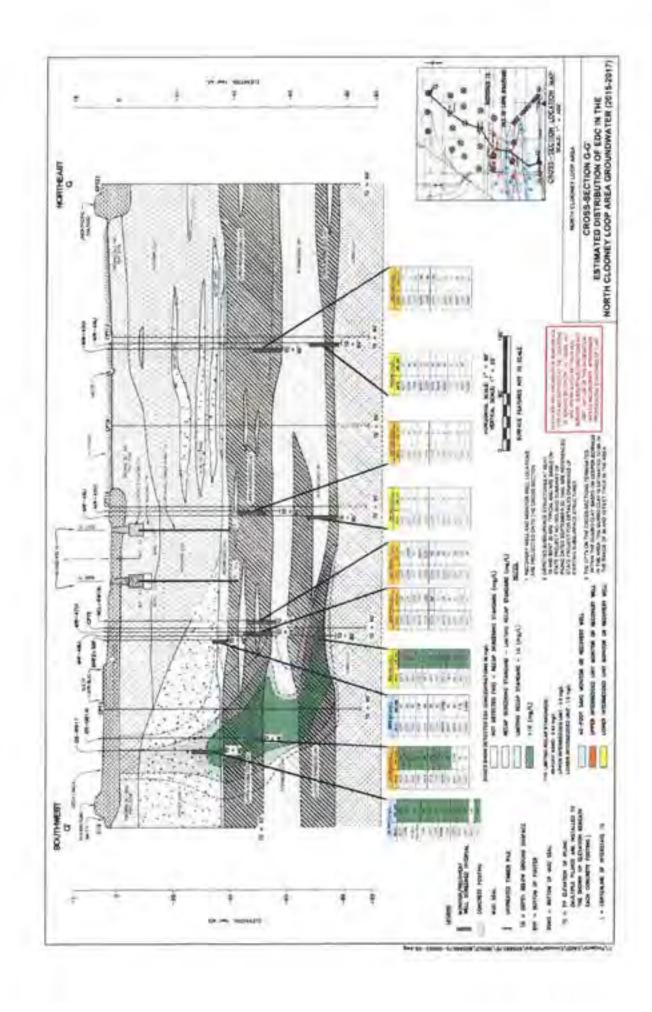
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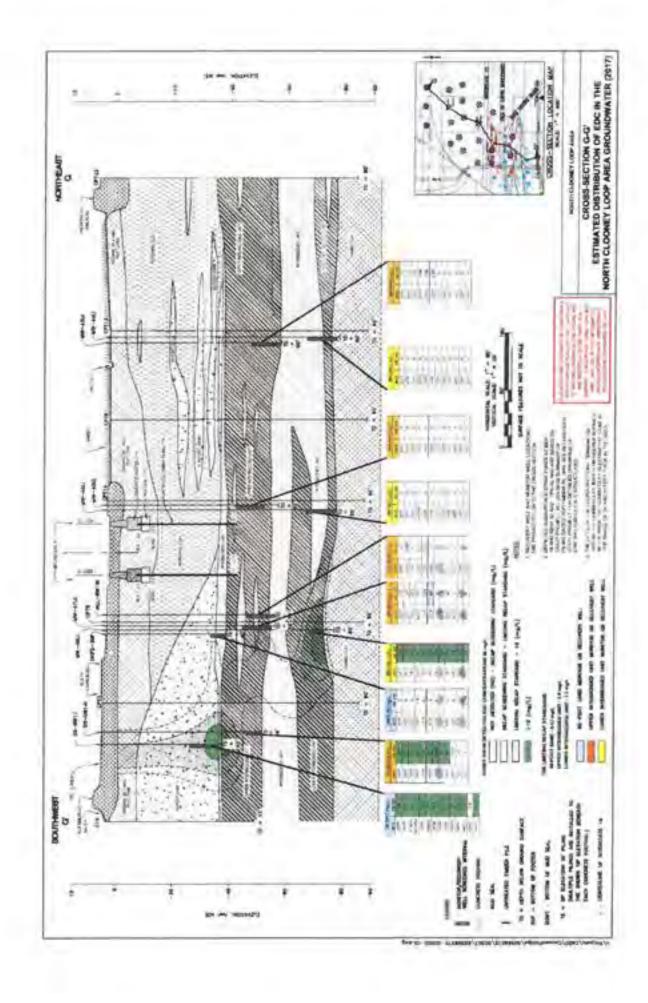
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ATTACHMENT 2

OBJECTIVES SCREENING MATRIX - PAGE 1

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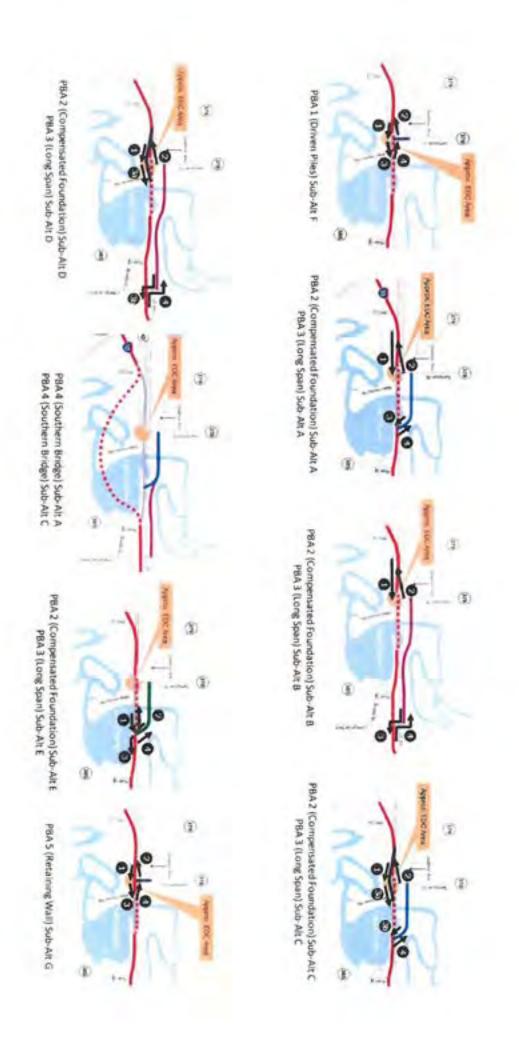
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C/O HWTB Compositation I-10 Calcavieur River Bridge Project Basson Rouge, 2/A 10810 10000 Perkins Rome, Switch 640

To: <u>Ria Doshi; Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 9

Date: Friday, April 26, 2019 1:08:00 AM

User Name: Danielle Richardson

Enter your email:

Subject: 400 Kile Street, westlake, LA

Comments: HI! I met you at the Westlake Meeting. My sister(Susan Rogers) lives at 400 Kile Street almost under the I10 bridge. Please mail information about her residence dealing with the new bridge. My address is P.O. Box 777, Westlake, LA 70669. Do you know when they will buy property if this bridge happens?

I think the bridge to the north is the best option. I actually think a new bridge there and another bridge at the end of Mike Hooks road ending at the port area would also help traffic congestion. There was a bridge there years ago and I think it is a short span to put a bridge or a ferry there also.

Put my vote in as YES on building a new bridge. We dont mind paying tolls for it. Thanks so much and it was great meeting you. My cell number is 337-936-7842

Ria Doshi; Lynn Maloney-Mujica To:

Subject: I-10 Calcasieu Bridge New Submission # 11

Date: Friday, April 26, 2019 6:31:46 AM

User Name: Ryan partin Enter your email:

Subject: I10 bridge

Comments: Close the entrance ramp from westlake. It is to close to the bridge.

To: <u>Ria Doshi</u>; <u>Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 12

Date: Friday, April 26, 2019 7:56:17 AM

User Name: Timothy Gothreaux

Enter your email: Subject: I-10 Bridge

Comments: It is no secret that the I-10 bridge in Lake Charles is way past it's prime. All when hearing is talk about talk of replacing it. Rumor has it Shell offshore forbids trucking companies to cross it with thier freight. We can talk about it for the next 20 years, or wait until it collapses and then wonder why and how we going to get funds to rebuild it. I understand it's an immense project, but personally I have heard enough talk. Seek advice from our neighbors Texas. They know how to get one planned and built in record time.

To: <u>Ria Doshi</u>; <u>Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 13

Date: Friday, April 26, 2019 8:39:20 AM

User Name: Laura Cangelose

Enter your email:

Subject: I-10 comment

Comments: We desperately need relief in this area. A temporary inconvenience is worth it for

a long term solution. Always plan for the long term.

To: <u>Ria Doshi</u>; <u>Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 14

Date: Friday, April 26, 2019 10:29:13 AM

User Name: Melissa Woods

Enter your email:

Subject: Westlake Sampson Street

Comments: Why cause more problems than already exist? Traffic leaving the plants already have difficulty and more is coming with John Stine Road expansion causing more alternate route problems. All I notice is more headaches with trying to get on I-10 even worse. TOLL BRIDGE IDEA IS A DEFINITE "NO!" FROM ME. We do not have money now to give for that.

To: <u>Ria Doshi</u>; <u>Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 15

Date: Friday, April 26, 2019 11:05:39 AM

User Name: Andew Steiner

Enter your email:

Subject: Do Not Build Over the Lake

Comments: The alternative proposal - PBA 4 - would be a disaster for Lake Charles, both the City and the Lake. No one wants to see a large bridge going across the Lake like that. I understand the need to avoid the contaminated area, but that proposal to go across the Lake is ridiculous. It seems it would be more expensive as two additional bridges would be needed to cross Contraband Bayou.

The alternatives that included an additional moveable bridge for Sulphur Avenue seemed like a worth pursuit. Whenever there are car accidents on the bridge, then drivers could use that as an alternative. Although I can see a future advantage of connecting that new roadway to Enterprise Boulevard, let's face the truth that that area of Lake Charles will remain poor, crime-ridden, and uneducated for quite some time to come. The proposed Sulphur Avenue bridge should have a connector to I-10, even if it is decided that it should connect to Enterprise Boulevard. Any crossings over the railroads should have overpasses - there is no reason to not include overpasses for what should be an easy connection from Lake Charles to those places west of the Calcasieu River.

Thank you

To: <u>Ria Doshi; Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 16

Date: Friday, April 26, 2019 12:42:37 PM

User Name: Blaine Young

Enter your email: Subject: My Opinion

Comments: Build the movable bridge connecting I-10 and Sampson St.. That should be the least obstructive to the current infrastructure and gives alternative means for I-10 E and Westlake to cross the river and train tracks. You will also have to build the overpass in Westlake for Westbound traffic, but that can be built after. Another option for that could be build the overpass/interstate entrance off of Sulphur Ave. That way while its being built Sampson wouldn't be shut down.

To: <u>Ria Doshi</u>; <u>Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 17

Date: Friday, April 26, 2019 1:07:18 PM

User Name: Mary Hale

Enter your email:

Subject: I-10 new bridge

Comments: I love the ideas you guys are throwing out. Including the idea of making sulphur avenue across the lake. I recently bought my first house off of Goos street that is off of sulphur avenue, and have always regretted buying a house in westlake because of the traffic and trains. I 100% fully support any ideas you have for the traffic problems. I know I am fortune enough to say that I would be willing to pay a toll for a new bridge. Anything it would take to get the traffic down and avoid the trains. Thank you so much for listening to me.

To: <u>Ria Doshi; Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 19

Date: Friday, April 26, 2019 3:06:48 PM

User Name: Jeremy Benoit

Enter your email:

Subject: I-10 Bridge Chaos.

Comments: I feel as if we are paying several police officers to be present along the foot of the I-10 bridge especially westbound at the Lakeshore Dr. feeder road, and entrance ramp, they should at least be implementing the zipper method to keep traffic flowing. Most of the problem lies in the fact that 18 wheelers merge to the left lanes, and can't climb the bridge well. Signs need to be replaced, and/or added to ensure they stay in right lane on bridge.

To: Ria Doshi; Lynn Maloney-Mujica

Subject: I-10 Calcasieu Bridge New Submission # 32 Date: Saturday, April 27, 2019 4:57:59 PM

User Name: Rebekah

Enter your email:

Subject: Bridge

Comments: Residents of Calcasieu parish should not have to pay these tolls. Or possibly only

pay one flat fee a year at an extremely reduced rate.

To: Ria Doshi; Lynn Maloney-Mujica

Subject: I-10 Calcasieu Bridge New Submission # 24

Date: Saturday, April 27, 2019 8:33:49 AM

User Name: Dave cain

Enter your email:

Subject: Tolls

Comments: We pay a higher Parrish tax than average on top of just recently raised property tax. Now we are going to have to pay a toll to use a bridge that should have been built years ago? What have you done with the money you've been given? You need to be better stewards, and get our house in order!

To: Ria Doshi; Lynn Maloney-Mujica

Subject: I-10 Calcasieu Bridge New Submission # 33

Date: Saturday, April 27, 2019 7:39:50 PM

User Name: Reagan Enter your email: Subject: 110 bridge

Comments: I think all of these new ideas are ridiculous. We shouldn't have to pay a toll whenever we use the road every single day. We've paid way too much already for road improvements, yet nothing ever changes. Whoever comes up with these ideas must not cross the bridge every single day. Things will be so much worse if these ideas keep being passed on.

To: <u>Ria Doshi</u>; <u>Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 39
Date: Sunday, April 28, 2019 10:12:38 AM

User Name: Kelvin Ellender

Enter your email:

Subject: I-10 Corridor Improvement

Comments: Dear Mr./Mrs. Calcasieu River-I-10 Bridge Proposer,

I would like to commend you on a great presentation, to update the public on your plans concerning the Calcasieu River-I-10 Bridge. I am not so impressed that this study has been going since 2001, with no definitive plan yet. Now, we have moved away from LaDOTD doing their job, that our taxes pay for, to a more CRM type plan. For you folks who don't know that acronym, it is "Construction Risk Management". Companies are beginning to do more and more of this to lock in projects. They design, propose, receive approval to construct then get paid on the back-end instead of invoicing throughout the project like most small companies would have to do.

As I read through your document, I was drawn to one thing. This plan is to improve the I-10 corridor from I-210 to I-210 exits. Now, that is a great plan for Lake Charles, but it is VERY short sighted for those of us west of the west most I-210 exit. Although you plan would help Lake Charles residents exit I-10 onto the northern streets of Lake Charles, your plan does nothing about the three exchanges that must handle the full force of construction workers of all industries except for SASOL and about half of Phillips 66. I know I sound like a broken record when I say this, but once again, Sulphur is getting screwed!".

Those of us who live west of the river must deal with the traffic on Hwy 27, Hwy 90, Cities Service and Old Spanish Trail. All of these are paths that the workers take to their jobs, that are over congested and are the real traffic issue. Yes, we need another, wider, I-10 bridge. Yes, we need a wider I-210 bridge. But the roads that the workers are traveling all lead there. Now, I'm pointing this out because the only way we can alleviate that congestion is by having larger bridges, which one is in the plans. But the money that you are planning on spending to improve the corridor from I-210 exit to I-210 exit, would be MUCH better spent widening the I-10 corridor where the must traffic congestion is.

In closing, I ask that you consider correcting the real issues in our area, instead of correcting the issues that the money of our area is telling you needs to be done.

Sulphur has a great history of being a good neighbor and I would like to see that history continue, but now it's time Lake Charles returned the favor.

To: <u>Ria Doshi</u>; <u>Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 40 Date: Sunday, April 28, 2019 11:06:14 AM

User Name: Joe Woodside

Enter your email: | Subject: Bridge

Comments: I personally feel as if this is just another way for our parish to rip us off. We already have one of the highest taxes in the state and now you want to add a toll bridge for \$2.50 a toll. For one that's ridiculous especially when you consider it's a minimum of \$5 for the average person. Did you consider the others that live in the area that might cross it several times a day, because I cross that bridge some days upwards of 8 times a day. So you're telling me it can cost me \$25 some days to do my job? That's crazy and to be honest some bs because I've been a resident of Westlake for over 15 years and the citizens have been promised several things and they are all broken promise. Hell we can't even get a way around the train tracks that always runs during prime time for traffic and it's even stopped backing traffic up the bridge. The worse part is I've called and complained several times and get told the same thing every time and that's there isn't enough complaints about it or the officials don't find it suitable.

To: <u>Ria Doshi; Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 42 Date: Sunday, April 28, 2019 11:14:30 PM

User Name: Lynn Bengston

Enter your email: Subject: I 10 Bridge

Comments: As a Westlake resident, it is very important that residents of Westlake not have to pay every time we leave our home. That would put an unfair burden on us since we have to be able to get out of Westlake. To have to use I 210 every time we leave home to avoid the toll is not really a viable option as traffic will probably be quite heavy. It seems to me, that the toll bridge should be something that is optional such as beltway eight in Houston, not the primary roadway. I hope y'all can come up with something better than making us pay constantly to leave our house. Driving through Moss Bluff is not a very good option either. It's already backed up horribly because of workers leaving Sasol and also does trying to avoid the bridge.

To: <u>Ria Doshi</u>; <u>Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 45

Date: Monday, April 29, 2019 7:44:08 AM

User Name: Matthew J. Duhon

Enter your email:

Subject: I-10 Calcasieu River "Toll" Bridge

Comments: I am all for building a new I-10 bridge but I am not willing to pay a toll. I have paid LA state income & sales tax for 19 years. I feel that LA state officials should have planed for this responsibly by setting aside a small amount of tax money each year starting in the late 90's. Instead, nothing was done and now we are in what feels like a crisis. A small toll (\$0.5) would not kill me but I am not willing to send the message that poor planing can be mitigated by passing the burden on to the public directly in the form of a toll in addition to LA income and sales tax.

To: <u>Ria Doshi</u>; <u>Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 46

Date: Monday, April 29, 2019 8:12:53 AM

User Name: Howard Duhjon

Enter your email:

Subject: New I 10 Bridge

Comments: I am against a toll. the state has seen now for decades the bride is getting old and will be in need of repair. There has been no planning for this day as there should have been. Now the day is arriving and the lack of planning on the part of those responsible is attempting to be passed along to others. A toll is inappropriate.

To: <u>Ria Doshi</u>; <u>Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 48

Date: Monday, April 29, 2019 9:46:20 AM

User Name: Daniel DeVore

Enter your email:

Subject: "No" To the Toll

Comments: I don't like the idea of passing the buck of the state's poor planning on to the citizen's through a form of yet another tax. Besides, I have never seen a toll on a major

interstate like I-10. Is this even going to be approved by the Feds?

To: <u>Ria Doshi</u>; <u>Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 54

Date: Friday, May 3, 2019 10:28:47 AM

User Name: Patrick Landry

Enter your email:

Subject: On-Line Public Meeting Comments

Comments: Generally support moving forward with evaluation of PBA 2-C, PBA 3-A, and PBA 5-G. Other recommendations include: Elevate I-10 over US-171 to help improve I-10 geometry and interchange connectivity w/ minimum impact; I-10 Near Ryan Street - Remove WB Off Ramp to LS Drive to reduce restricted movement controls, Remove EB On Ramp from LS Drive to improve ramp spacing, and add connection from I-10 WB SR to I-10 EB SR.to improve Ryan St access; Include I-10 geometric improvements at RR overpass near Opelousas St.; Add 3 thru lanes at all 2 lane sections in study area including I-210 interchanges between ramp connections.

To: <u>Ria Doshi</u>; <u>Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 55

Date: Friday, May 3, 2019 5:11:27 PM

User Name: Daniel A. Kramer

Enter your email:

Subject: Comments regarding meeting on April 15, 2019

Comments: The minimum under-span height of the proposed Interstate 10 Calcasieu River Bridge (in all proposed configurations) is 73'. The current vertical clearance is 135'. Louisiana Scrap Metals Recycling Lake Charles, L.L.C. and its landlord JK Holdings Calcasieu, L.L.C. have a facility located on the Calcasieu River, north of the bridge, that includes a deep water port facility which makes use of tug boats and other vessels with height requirements (known as "air draft") that exceed the proposed 73' minimum under-span height (62' lower than the current bridge). For example, the tug Beaufort Sea has an air draft of 90', and the tug McKinley Sea has an air draft of 86'. Both of these tugs have been used, and will likely continue to be used, in Louisiana Scrap Metals Recycling Lake Charles, L.L.C.'s marine recycling business. The air draft of each tug boat is different, and often exceeds 73'. Louisiana Scrap Metals Recycling Lake Charles, L.L.C.'s marine recycling business operations include the purchase and breaking up of vessels for scrap; many of the vessels are large, with air drafts exceeding 73'. Reduction of the vertical clearance of the bridge will greatly reduce the amount of floating inventory available for Louisiana Scrap Metals Recycling Lake Charles, L.L.C. to bid upon, which will have a large negative impact on the business. Louisiana Scrap Metals Recycling Lake Charles, L.L.C. has 43 direct employees in Lake Charles as well as 25-30 contract laborers at any given time. Reduction of the bridge clearance will endanger all of those jobs.

Louisiana Scrap Metals Recycling Lake Charles, L.L.C. is not the only marine-related business that will be adversely affected by the lower proposed bridge height. One of its neighbors, Park West Children's Fund, Inc. (also known as "Friend Ships") is a non-profit organization that ships disaster-relief supplies world-wide through its deep water port facility on the Calcasieu River north of the Interstate 10 Calcasieu River Bridge. Friend Ship's own ship will not be able to dock at its port facilities if a 73' vertical clearance bridge is constructed.

Consideration should be extended to established businesses such as Louisiana Scrap Metals Recycling Lake Charles, L.L.C., JK Holdings Calcasieu, L.L.C., and Friend Ships, all of whom make commercial use of port facilities north of the Interstate 10 Calcasieu River Bridge. Such consideration should include assistance with locating and acquiring commercial deepwater port facilities south of the proposed Interstate 10 Calcasieu River Bridge which will allow established businesses currently located north of the Bridge to continue their maritime operations after the 135' vertical clearance ceases to exist. Government authorities involved in the bridge project should include within their budget projections the cost necessary to make these arrangements, including the cost of acquisition and development of suitable port facilities for affected businesses. The cost should be included in the budgets because the public or public/private construction of a bridge which prevents the current use of the navigable waterway would constituted a compensable administrative taking of affected businesses' property.

- Daniel A. Kramer, counsel for Louisiana Scrap Metals Recycling Lake Charles, L.L.C.

To: <u>Ria Doshi</u>; <u>Lynn Maloney-Mujica</u>

Subject: I-10 Calcasieu Bridge New Submission # 56

Date: Sunday, May 5, 2019 2:27:33 PM

User Name: David Balmos

Enter your email:

Subject: Comments about I-10 PBA 5G

Comments: I have two suggestions regarding PBA 5-G.

1) It was stated at the public meeting that Sampson Street will be closed completely for up to 18 months, and partially closed for up to 48 months in order to construct the elevated interchange over I-10. If the alignment of Sampson Street north of I-10 were shifted to the east, still within existing ROW, the elevated interchange could be constructed while traffic remains on the current at-grade pavement. The shift eastward would essentially fall on the same location as the proposed new at-grade access road was shown at the public meeting. The at-grade access could be provided by the current pavement. Basically switch the location of the proposed elevated pavement and the proposed at-grade pavement. Some partial closure of Sampson Street would still be required at the tie-in at the northern limit of the new-location alignment. But this approach would likely eliminate the need to fully close the Sampson Street intersection during construction.

2) PBA 5-G showed a westbound exit ramp from I-10 to Sampson Street that flies over the railroad and connects to Sampson Street several hundred feet north of the westbound entrance ramp from Sampson Street to I-10. It was stated that the reason the two intersections do not line up is to avoid the EDC plume area. It was also acknowledged by several project team representatives that the traffic operations of this offset intersection are less than desirable.

I suggest that the westbound exit ramp from I-10 to Sampson Street be aligned with the westbound entrance ramp to I-10. There are multiple solutions to construct the elevated ramp across the EDC plume including shallow foundations (short spans with driven piles of a depth of 25' +/- that would be well above the depth of the EDC contaminates), or compensated foundations that are recommended as on one the feasible solutions being carried forward by the project team, or fill sections with MSE retaining walls. Any of these solutions could be evaluated further to improve the operations of the future Sampson Street interchange.



(I-10/I-210 West End to I-10/I-210 East End) State Project No. H.003931

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- Preliminary Build Alternative 5-G

Please return this completed form to Station 9 or to a Project Team member.

You can also submit comments online at www.i10lakecharles.com or by U.S. mail to the following address:

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe Suite 640 Baton Rouge, LA 70810

Comments on the project will be accepted for 10 days following this public meeting.

	Please Print
	Name: LEONARD K. KNAPP JR.
	Address:_
	Email:
	Agency (if applicable): NA
	Would you like to receive future updates on the project? (res) or No (circle one)
	Comments:
1.	Thank you for opportunity to comment.
Ó	All PBA's propose only six lawer of travel over bridge and
1	between East of West I-210 june from. Very short sighted. 12 year
	day (Completion 2028 to 2040) for traffic. We need Boyear plan - or
	(Continued on Back) (40 year) &

Minimum of bridge and understructure to support 8 lang (4 each
way, plus space for on-off ramp stowdowns. Current bridge
Approaching 50 years of service - will we be less relimit on
vehicles in future. Perhaps - but many more persons to serve.
Need to have as an Alternative - wider bridge - even if not
utilized immediately. Also expand I-10 - excess mond to
encompass larger volume. Also could then have separate have - HOV
3) PBA - 4 -> Problem - visual from east shore, but what are
We seeing now - industry. Perhaps would be improvement if
beautifully designed. Problem: ship channel is used in Area
depoted. It does not cross Contraband Bayon in 2 placer. That
Area is a loop of Calcasien Rover. Probably would need to maintain
height because of industry in north Area of loop for tanger
longer distance as shipr go around at least a part of that
old loop. Need further work up to identify destrability + problems.
PB5- probably too steep to bring down without effect on
EDC area. Then would need to spAN EDC Area at grade.
ANY WAY.
PBA-1: object to going through EDC to create prothways to drivking
I APTEC
PBA 2+3 - probably of if EDC Area is not breached.
4.) Sampron Street Sub-Alternatives
God should be for young both east and west from Samporn Vo
Access I-10 while going over RR tracks Above or below
And Avoiding Any disturbance of EDC contaminated Area.
I Savor Subalternative E joining I-10 west of Ryanst.



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Name: Walt of Council
Address:_
Email: 1
Agency (if applicable): IMCAL
Would you like to receive future updates on the project? Yes or No (Ericle one)
Comments:
IMIAL/LCMPO, the agency where the man Lake Charles
MEtropolitan Planning Organization is housed has a
Complete Streets Advisory Committee. We have mentioned
in our last meeting in sanyay 2019, about the
Continued on Backii

halina MEL duesnit IMBAC heduling. on the alreadu Would guent ulce Charles are invitasingly Enhance quali Occavenic STOP " Calvasiew Apri Casino, development of really a and miles don't have a resolution in support uset, has mentioned this idea to enainters and that made LIMPO shared use paths on a I-10 Bridge alternative Share Use Paths Alony or Near Freeways and Brende on France quidelines on) www.thwa.dot.gov/environment/bicucle_pidestrian Journal of FreeWays . Etm that bike/pod must be consid especially where 710 Gerald Domand Bridge in lit of Loughyir



Please Print

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Name: Address: _	alter Council	
Email:		
Agency (If a	licable): IMCAL	
Would you	to receive future updates on the project? Yes or No (circle one)	
Comments	Charles Bile Pedestrian Master Plan as were for connecting the bridge.	
a	were for connecting the bridge.	
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Please Print

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c/o HNTB Corporation
10000 Perkins Rowe
Suite 640
Baton Rouge, LA 70810

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Name Lella Hoffpauir
Address:
Agency (If applicable); Council City WL
Would you like to receive future updates on the project? (es or No (circle one)
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(I-10/I-210 West End to I-10/I-210 East End) State Project No. H.003931

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I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe Suite 640 Baton Rouge, LA 70810

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NOTE: All comments received at the public meeting, through the virtual meeting link, and written comments mailed to the address below and post-marked by May 5, 2018, will become part of the meeting record.

Please Print Name Mike Johnson Address Email: Agency (if applicable): Would you like to receive future updates on the project? Yes or No (circle one) Comments: lease use All Federal tax dollars and



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Please Print

Name ROB PIZICE JIZ: PIZES. DUNHAM PIZICE GROUP LLC

Address:

Email: _

Agency (If applicable):

Would you like to receive future updates on the project? Frey or No (circle one)

Comments:

CONCERNED ABOUT PROPOSED TIZUCK ACCESS IN &

OUT OF MIKE HOOKS ROAD

(Continued on Back)



(I-10/I-210 West End to I-10/I-210 East End) State Project No. H.003931

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Please Print	II A	0			
Name:	FRED	KAN	Α		
Address:					
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Would you like	se to receive futur	e updates on the pro	ject? Yes or No	o (cycle one)	
Comments:					
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(I-10/I-210 West End to I-10/I-210 East End) State Project No. H.003931

Public Meeting Comment Form

Please provide your comments on the following items:

- 1. Preliminary Build Alternative (PBA) 5-G and other Proposed Preliminary Build Alternatives
- 2. Alternatives Screening Methodology and Results
- 3. Recommended Reasonable Alternatives for further evaluation in the Environmental Impact Statement

The Recommended Reasonable Alternatives are as follows:

- · Preliminary Build Alternatives 2-A & 2-E
- Preliminary Build Alternatives 3-A & 3-E
- Preliminary Bulld Alternative 5-G

Desired

Please return this completed form to Station 9 or to a Project Team member.

You can also submit comments online at www.i10lakecharles.com or by U.S. mail to the following address:

I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe Suite 640 Baton Rouge, LA 70810

Comments on the project will be accepted for 10 days following this public meeting.

Please Print	
Name Denise Kau	
Address:	
Email:	
Agency (if applicable);	
Would you like to receive future updates on the project? Yes or No (circle one)	
Ine only truly feasible alternative is 5	Gr.
Please do what you can to expedite the E	
and Bulld our Bridge!	



(I-10/I-210 West End to I-10/I-210 East End) State Project No. H.003931

Public Meeting Comment Form

Please provide your comments on the following items:

- 1. Preliminary Build Alternative (PBA) 5-G and other Proposed Preliminary Build Alternatives
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- Preliminary Build Alternatives 3-A & 3-E
- Preliminary Build Alternative 5-G.

Please return this completed form to Station 9 or to a Project Team member.

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I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe Suite 640 Baton Rouge, LA 70810

Comments on the project will be accepted for 10 days following this public meeting.

Please Print				
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 From:
 Meredith Taylor

 To:
 Lynn Maloney-Mujica

Subject: FW: I-10 Calcasieu River Bridge Project - PUBLIC MEETING REMINDER

Date: Wednesday, April 17, 2019 1:36:21 PM

Attachments: image001.png

image002.png image003.png image004.png

Lynn, Please see email response below.

Meredith Taylor

Urban Designer & Planner Transportation

Tel (225) 368-2811 Cell (985) 312-9437 Email metaylor@hntb.com

HNTB CORPORATION

10000 Perkins Rowe, Ste. 640 | Baton Rouge, LA 70810 | hntb.com

100+ YEARS OF INFRASTRUCTURE SOLUTIONS



From: Houston Jones

Sent: Wednesday, April 17, 2019 12:20 PM **To:** Meredith Taylor <metaylor@HNTB.com>

Subject: Re: I-10 Calcasieu River Bridge Project - PUBLIC MEETING REMINDER

Just curious, I don't see the Public Private Partnership project on the agenda. Will that also be discussed since it seems to be much farther along than anything the state could do on its own? Thanks, Sam Houston Jones II

Appendix D-2: Response to Phillips 66 Comments

Response to Phillips 66 Comments on the April 25, 2019 Public Meeting #3

- Comment 1: It is not acceptable for DOTD to use EDC contamination data from 3 years ago for the PBA screening and not update it with readily available quarterly monitoring data and estimated plume delineation. The EDC well monitoring results have greatly improved since first quarter 2016, and DOTD's consideration of the most current data on the EDC site conditions would change DOTD's recommended reasonable alternatives to include PBA 1-F.
- Response 1: The schematics utilized during the Public Meeting #3 presentation, do include the following disclaimers "Unknowns remain about the full extent, depth and migration of EDC" and "EDC contamination area shown based on First Quarter 2016 well monitoring data". Current and historical monitoring well data shows that EDC is present north of I-10, spanning through the DOTD right-of-way and migrating towards, and now past, the original northern perimeter wells. Additionally, and despite low level test results routinely obtained from groundwater samples collected since the installation of the monitor wells north of I-10, significantly high EDC test results, obtained from soil samples collected during the installation of these wells and also from the CPT borings in this same area, have not been accounted for or reconciled. The absence of such test data correlation and accountability between these sampling events are of reasonable concern and, thus, continue to support DOTD's decisions and the subsequent development of PBA 5-G as a Reasonable Alternative.
- Comment 2: PBA 1-F is Phillip 66s preferred alternative, and any concern over adverse impacts to the subsurface environment should be studied in the EIS rather than ruling out PBA 1-F at this stage of the NEPA process. The detailed evaluation in the EIS includes evaluation of ways to avoid, minimize, or mitigate potential environmental impacts associated with all proposed reasonable alternatives.
- Response 2: The detailed screening matrices showing how each screening objective was evaluated and rated for each Preliminary Build Alternative were also available for review at the public meeting. To encourage further transparency and public input, all meeting materials have been made available on the project website. In accordance with the NEPA process, public and agency input solicited will be considered and materials modified based on this input as determined practicable. Note that the Reasonable Alternatives presented at the public meeting are recommendations only, and the finalization of those recommendations will not occur until after public and agency input obtained are incorporated into the screening analysis. Ultimately, the final identification of Reasonable Alternatives will be made by DOTD based on professional judgement with consideration given to all project objectives, including environmental issues, cost, engineering issues, and public and agency input.
- Comment 3: In 2010, DOTD explained in a report to FHWA that "if the decision is made to allow driven piles above contaminated strata, then it is currently considered feasible to construct the previous proposed foundations for the elevated mainline and north frontage roads. To allow relatively shallow piles which

would be required for the south frontage road, soil-cement mixing is recommended to improve the load carrying capacity of the soil above a depth of 25 feet." Particularly because significant attenuation of the EDC has occurred since 2010, DOTD should acknowledge its previous statement that design with driven piles are engineering solutions that "demonstrate that an acceptable approach is available to implement the project while managing risk associated with the hazardous materials issue." Therefore, PBA 1-F should be studied in the EIS.

- Response 3: The final identification of Reasonable Alternatives will be made by DOTD based on professional judgement with consideration given to all project objectives, including environmental issues, cost, engineering issues, and public and agency input. The Reasonable Alternatives in the EIS will evaluate acceptable approaches for engineering solutions while managing risk associated with the hazardous materials issue.
- Comment 4: In comments to the public meeting #2 in 2017, LDEQ again approved driving piles in the EDC area as it had since 2009, to "piling depths of 75 feet below current existing grade or less north of the current I-10 footprint" and not to "exceed a depth of 40 feet below current existing grade south of the current I-10 footprint" with the exception of the area east of the specified eastern boundary measured by CPT boring locations. Likewise, the EPA Sole Source Aquifer Office has expressed no objections to the project. In 2013, the SSA Office sent a letter to DOTD that it evaluated "the potential environmental impact which might result from changes to a previously approved project" and "determined that the project, as proposed, should not have an adverse effect on the quality of the ground water underlying the project site". Therefore, PBA 1-F should be studied in the EIS.
- Response 4: The detailed screening matrices showing how each screening objective was evaluated and rated for each Preliminary Build Alternative were also available for review at the public meeting. To encourage further transparency and public input, all meeting materials have been made available on the project website. In accordance with the NEPA process, public and agency input solicited will be considered and materials modified based on this input as determined practicable. Note that the Reasonable Alternatives presented at the public meeting are recommendations only, and the finalization of those recommendations will not occur until after public and agency input obtained are incorporated into the screening analysis. Ultimately, the final identification of Reasonable Alternatives will be made by DOTD based on professional judgement with consideration given to all project objectives, including environmental issues, cost, engineering issues, and public and agency input.
- Comment 5: It is not acceptable to rule out PBA 1-F without a comprehensive analysis of likely piling depths in relation to the subsurface depths of EDC, particularly in the areas north of the existing I-10 bridge where EDC has never been detected in depths from 0 to 55 feet below ground surface and it is unlikely piling depths would exceed 55 feet in this area.
- Response 5: Current and historical monitoring well data shows that EDC is present north of I-10, spanning through the DOTD right-of-way and migrating

towards, and now past, the original northern perimeter wells. Given that unknowns remain about the full extent, depth and migration of the EDC; PBA 1-F was recommended for elimination. However, PBA 5-G, a modification of PBA 1-F, was recommended at the April 25, 2019 Public Meeting #3 as a Reasonable Alternative.

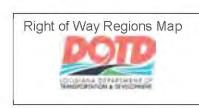
- Comment 6: Regarding DOTD's comment H-1 in the Public Involvement Summary Report, PBA 1-F, DOTD acknowledges that the EDC contamination in relation to the project alternatives will be evaluated in the EIS based on available information in coordination with LDEQ. DOTD has committed that "if a proposed alternative with the potential to encounter EDC is selected as the Preferred Alternative, DOTD would follow the proper procedures to ensure the safety of its employees, contractors, and the public." DOTD's comment H-2 in the same report likewise confirms that the potential impact of EDC on the integrity of soils, along with other mitigating factors, will be considered during the evaluation process of the reasonable alternatives. Therefore, PBA 1-F should be studied in the EIS.
- Response 6: The detailed screening matrices showing how each screening objective was evaluated and rated for each Preliminary Build Alternative were also available for review at the public meeting. Note that the Reasonable Alternatives presented at the public meeting are recommendations only, and the finalization of those recommendations will not occur until after public and agency input obtained are incorporated into the screening analysis. Ultimately, the final identification of Reasonable Alternatives will be made by DOTD based on professional judgement with consideration given to all project objectives, including environmental issues, cost, engineering issues, and public and agency input.

Appendix F: LADOTD Acquisition of Right of Way and Relocation Assistance Brochure



ACQUISITION OF RIGHT OF WAY AND RELOCATION ASSISTANCE

This fifth printing (1000 copies) of this public document is published at a total cost of \$410.00. 2000 copies of this public document were published in the fourth printing at a cost of \$820.00. 2000 copies of this public document were published in the third printing at a cost of \$820.00. 2000 copies of this public document were published in the second printing at a cost of \$820. 1000 copies of this public document were published in the first printing at a cost of \$410.00. The total cost of all printings of this document including reprints is \$3,280.00. This document was published by the Louisiana Department of Transportation and Development Reproduction Unit, 1201 Capitol Access Road, Baton Rouge, LA 70804, to inform the public of the Acquisition of Right of Way and Relocation Assistance process under the authority of the Uniform Act. This material was printed in accordance with standards for printing by State Agencies in R.S. 43:31.





Region 1 - Debra Milstead

Monroe – (318) 342-0250 Bossier – (318) 549-8455

Region 2 - Ronald Powell

Alexandria – (318) 561-5250 Chase – (318) 561-5250

Region 3 - Robert Richard

Lafayette – (337) 262-6251 Lake Charles – (337) 437-9250

Region 4 - Kiawasha White

Baton Rouge – (225) 242-4593

Region 5 - Erin Roussel

Kenner – (504) 465-3468 Hammond – (985) 375-0250

Statewide Relocation

<u>Assistance – Stephanie Black</u>

Baton Rouge - (225) 242-4536

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INTRODUCTION

Government programs designed to benefit the public as a whole often result in acquisition of private property and, sometimes, in the displacement of people from their residences, businesses or farms. Acquisition of this kind has long been recognized as a right of government and is known as the power of eminent domain. The Fifth Amendment of the Constitution requires that private property shall not be taken for public use without payment of just compensation.

To provide uniform and equitable treatment for persons whose property is acquired for public use, Congress passed the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, and amended it in 1987. This law, called the Uniform Act, is the foundation for the information discussed in this brochure. This brochure explains your rights under the Uniform Act as an owner of real property that is being acquired for a state or federally funded project. It also provides information about Relocation Assistance benefits and advisory services that are available for displaced residences, businesses, farms, and nonprofit organizations.

If you are required to move as a result of a state or federally funded project, a representative of the acquiring Agency will contact you. The representative will answer your specific questions and provide any additional information you may need. If you have a disability that prevents you from reading or understanding this brochure, you will be provided appropriate assistance. You should notify the sponsoring Agency if you have special requirements for assistance.

*******NOTICE******

RELOCATION BENEFITS CANNOT BE PAID UNTIL THE PROPERTY IS ACQUIRED BY THE DEPARTMENT. IF YOU MOVE OR PURCHASE REPLACEMENT HOUSING BEFORE YOU HAVE BEEN AUTHORIZED TO DO SO BY THE DEPARTMENT, YOU COULD LOSE ALL POSSIBLE BENEFITS PROVIDED BY THE RELOCATION ASSISTANCE PROGRAM

Attachment E: Meeting Photos

Agency Presentation







IMPORTANT TERMS USED IN THIS BROCHURE

Acquisition Acquisition is the process of acquiring real property (real estate) or some

interest therein.

Agency An agency can be a government organization (Federal, State, or local), a

non-government organization (such as a utility company), or a private person using Federal financial assistance for a program or project that

acquires real property or displaces a person.

Alien Not Lawfully The law provides that if a displaced person is an alien not lawfully Present present in the United States such person is not eligible for re

present in the United States such person is not eligible for relocation payments under the Uniform Act, unless ineligibility would result in exceptional and extremely unusual hardship to the alien's spouse, parent or child, and such spouse, parent or child is a citizen or an alien lawfully

admitted for permanent residence.

Appraisal An appraisal is a written statement independently and impartially prepared by a qualified appraiser setting forth an opinion of the value of an

adequately described property as of a specific date, supported by the

presentation and analysis of relevant market information.

Business Any lawful activity, with the exception of a farm operation, conducted

primarily for the purchase, sale, lease, and rental of personal or real property; or for the manufacture, processing, and/or marketing of products, commodities, or any other personal property; or for the sale of services to the public; or solely for the purpose of the Uniform Relocation Assistance Act, an outdoor advertising display or displays, when the display(s) must

be moved as a result of the project.

Displaced Person Any person (individual, family, partnership, association or corporation) who

moves from real property, or moves personal property from real property as a direct result of (1) the acquisition of the real property, in whole or in part, (2) a written notice from the Agency of its intent to acquire, (3) the initiation of negotiations for the purchase of the real property by the Agency, or (4) a written notice requiring a person to vacate real property for the purpose of rehabilitation or demolition of improvements, provided the displacement is permanent and the property is needed for a Federal or

federally assisted program or project.

Eminent Domain Eminent domain is the right of government to take private property for

public use. In the United States, just compensation must be paid for private

property acquired for federally-funded projects

Expropriation Expropriation is the legal process of acquiring private property for public

use or purpose through the Agency's power of eminent domain. Expropriation is usually not used until all attempts to reach a mutually satisfactory agreement through negotiations have failed. An agency then

goes to court to acquire the needed property.

IMPORTANT TERMS USED IN THIS BROCHURE (continued)

Farm Any activity conducted solely or primarily for the production of one or more

agricultural products or commodities, including timber, for sale and home use, and customarily producing such products or commodities in sufficient quantity to be capable of contributing materially to the operator's support.

Interest An interest is a right, title, or legal share in something. People who share

in ownership of real property have an interest in the property.

Just Compensation Just compensation is the price an agency must pay to acquire real

property. An agency official must make the estimate of just compensation to be offered to you for the property needed. That amount may not be less than the amount established in the approved appraisal report as the value for your property. If you and the agency cannot agree on the amount to be paid for the property needed, and it becomes necessary for the agency to use the expropriation process, the amount determined by the court will be

the just compensation for your property.

Lien A lien is a charge against a property in which the property is the security

for payment of a debt. A mortgage is a lien. So are taxes. Customarily,

liens must be paid in full when the property is sold.

Market Value Market value is the sale price that a willing and informed seller and a willing

and informed buyer agree to for a particular property.

Negotiation Negotiation is the process used by an agency to reach an amicable

agreement with a property owner for the acquisition of needed property. An offer is made for the purchase of property in person, or by mail, and the

offer is discussed with the owner.

Nonprofit A public or private entity that has established its nonprofit status under

Organization applicable Federal or State law.

Person A person is an individual, partnership, corporation, or association.

Personal Property In general, personal property is property that can be moved. It is not

permanently attached to, or a part of, the real property. Personal property

is not included or valued in the appraisal.

Program or Project A program or project is any activity or series of activities undertaken by an

agency where Federal financial assistance is used in any phase of the

activity.

Servitude In general, a servitude is the right of one person to use all or part of the

property of another person for some specific purpose. A servitude can be

permanent or temporary.

IMPORTANT TERMS USED IN THIS BROCHURE (continued)

Small Business

A business having not more than 500 employees working at a site which is the location of economic activity and which will be acquired for a program or project, or is displaced by a program or project. A site occupied solely by an outdoor advertising sign(s) does not qualify for purposes of the reestablishment expense benefit.

ACQUISITION PROCESS

An agency determines what specific property needs to be acquired for a public program or project after the project has been planned and government requirements are met.

If your property, or a portion of it, needs to be acquired, you will be notified as soon as possible of (1) the agency's interest in acquiring your property, (2) the agency's obligation to secure any necessary appraisals, and (3) any other useful information. When an agency begins the acquisition process, the first personal contact with you, the property owner, should be no later than during the appraisal of the property.

PROPERTY APPRAISAL

An appraiser will contact you to make an appointment to inspect your property. The appraiser is responsible for determining the initial value of the property. The agency will have a review appraiser study and accept the appraisal report to establish the just compensation to be offered to you for the property needed.

You, or a representative that you designate, will be invited to accompany the appraiser when the appraiser inspects your property. You can point out any unusual or hidden features of the property that the appraiser could overlook. At this time, you should advise the appraiser if any of these conditions exist:

- There are other persons who have ownership or interest in the property.
- There are tenants on the property.
- Items of real or personal property that belong to others located on your property.
- The presence of hazardous material, underground storage or utilities.

This is your opportunity to tell the appraiser about anything relevant to your property, including other properties in your area that have recently sold. The appraiser will inspect your property and note its physical characteristics. He or she will review sales of properties similar to yours to compare the facts of those sales with the facts about your property. The appraiser will analyze all elements that affect value. The appraiser must consider normal depreciation and physical deterioration that has taken place. By law, the appraiser must disregard the influence of the future public project on the value of the property. The appraisal report will describe your property and the agency will determine a value based on the condition of the property on the day that the appraiser last inspected it.

If you elect to donate the property and formally release the agency from the obligation of performing an appraisal, the appraisal requirement will be waived and an appraisal will not be performed.

JUST COMPENSATION

Once the appraisal is complete, a review appraiser will review the report(s) to ensure that all applicable appraisal standards and requirements are met. When they are, the review appraiser will give the agency the approved appraisal to use in determining the amount of just compensation to be offered for your real property. This amount will never be less than the value established by the approved appraisal.

If the agency is only acquiring a part of your property, there may be damages or benefits to your remaining property. Any allowable damages or benefits will be reflected in the just compensation amount. The agency will prepare a written offer of just compensation for you when negotiations begin.

Buildings, Structures and Improvements

Sometimes buildings, structures, or other improvements are located on the property to be acquired. If they are real property, the agency must offer to acquire at least an equal interest in them if they must be removed or if the agency determines that the improvements will be adversely affected by the public program or project. An improvement will be valued as real property regardless of who owns it.

Tenant-Owned Buildings, Structures and Improvements

Sometimes tenants lease real property and build or add improvements for their use. Frequently, they have the right or obligation to remove the improvements at the expiration of the lease term. If, under State law, the improvements are considered to be real property, the agency must make an offer to the tenants to acquire these improvements as real property. In order to be paid for these improvements, the tenant-owner must assign, transfer, and release to the agency all right, title, and interest in the improvements. Also, the owner of the real property on which the improvements are located must disclaim all interest in the improvements.

For an improvement, just compensation is the amount that the improvement contributes to the value of the whole property, or its value for removal from the property (salvage value), whichever amount is greater.

A tenant-owner can reject payment for the tenant-owned improvements and obtain payment for his or her property interests in accordance with other applicable laws. The agency cannot pay for tenant-owned improvements if such payment would result in the duplication of any other compensation otherwise authorized by law.

If improvements are considered personal property under State law, the tenant-owner may be reimbursed for moving them via the relocation assistance program. The agency will contact the tenant-owner of improvements to explain the procedures to be followed. All payments must be in accordance with Federal rules and applicable State laws.

THE WRITTEN OFFER

After the agency approves the just compensation offer the agency will begin negotiations with you or your designated representative by delivering the written offer of just compensation for the purchase of the real property. If practical, this offer will be delivered in person by a representative of the agency. Otherwise, the offer will be made by mail and followed up with a contact in person or by telephone. All owners of the property with known addresses will be contacted unless they collectively have designated one person to represent their interests.

An agency representative will explain agency acquisition policies and procedures in writing, either by use of an informational brochure, such as this one, or in person.

The agency's written offer will consist of a written summary statement that includes all of the following information:

- The amount offered as just compensation.
- The description and location of the property and the interest to be acquired.
- The identification of the buildings and other improvements that are considered to be part of the real property.

The offer may list items of real property that you may retain and remove from the property and their retention values. If you decide to retain any or all of these items, the offer will be reduced by the value of the items retained. You are responsible for removing the items from the property in a timely manner. The agency may elect to withhold a portion of the remaining offer until the retained items are removed.

Any separately held ownership interests in the property, such as tenant-owned improvements, will be identified by the agency. The agency may negotiate with each person who holds a separate ownership interest, or, may negotiate with the primary owner and prepare a check payable jointly to all owners.

The agency will give you a reasonable amount of time to consider the written offer and ask questions or seek clarification of anything that is not understood. If you believe that all relevant material was not considered during the appraisal, you may present such information at this time. Modifications in the proposed terms and conditions of the purchase may be requested. The agency will consider any reasonable requests that are made during negotiations.

Partial Acquisition

Often an agency does not need all the property you own. The agency will usually purchase only what it needs. If the agency intends to acquire a portion of the property, the agency must state the amount to be paid for the part to be acquired. An amount will be stated separately for damages, if any, to the portion of the property you will keep.

If the agency determines that the remainder property will have little or no value or use to you, the agency will consider this remainder to be an uneconomic remnant and will offer to purchase

it. You have the option of accepting the offer for purchase of the uneconomic remnant or keeping the property.

Agreement Between You and The Agency

When you reach agreement with the agency on the offer, you will be asked to sign a deed prepared by the agency. Your signature will affirm that you and the agency are in agreement concerning the acquisition of the property, including terms and conditions.

If you do not reach an agreement with the agency because of some important point connected with the acquisition offer, the agency may suggest mediation as a means of coming to agreement. If the agency thinks that a settlement cannot be reached, it will initiate expropriation proceedings.

The agency may not take any action to force you into accepting its offer. Prohibited actions include:

- Advancing the expropriation process.
- Deferring negotiations.
- Deferring expropriation.
- Delaying the deposit of funds with the court when expropriation is initiated.
- Any other coercive action designed to force an agreement regarding the price to be paid for your property.

ACQUISITIONS WHERE EXPROPRIATION WILL NOT BE USED

An agency may not possess the power of eminent domain. Or an agency elects not to use eminent domain for a program or project. If this is the case, you will be informed in writing, before negotiations begin, that the agency will not expropriate your property if you and the agency fail to reach agreement. Before making you an offer, the agency will inform you, in writing, of what it believes to be the value for the property it would like to acquire. An owner, in this situation, is not eligible for relocation assistance benefits. Tenants on the property may be eligible for relocation benefits.

PAYMENT

The next step in the acquisition process is payment for your property. As soon as all the necessary paperwork is completed for transferring title of the property, the agency will pay any liens that exist against the property and pay your equity to you. Your incidental expenses will also be paid or reimbursed. Incidental expenses are reasonable expenses incurred as a result of transferring title to the agency, such as:

- Recording fees and transfer taxes.
- Documentary stamps.
- Evidence of title, however, the agency is not required to pay costs solely to perfect your title or to assure that title to the real property is without defect.

- Surveys and legal descriptions of the real property.
- Other similar expenses necessary to convey the property to the agency.

Penalty costs and other charges for prepaying preexisting recorded mortgages entered into in good faith encumbering the real property will be reimbursed. If possible, the agency will pay these costs directly so that you will not need to pay the costs and then claim reimbursement. Property taxes will be pro-rated to the time when the agency obtains title to the property or takes possession of it.

POSSESSION

The agency may not take possession of your property unless:

- You have been paid the agreed purchase price, or
- In the case of expropriation, the agency has deposited with the court an amount for your benefit and use that is at least the amount of the agency's approved appraisal of the value of your property, or
- The agency has paid the amount of the court award of compensation in the expropriation proceeding.

If the agency takes possession while persons still occupy the property:

- All persons occupying the property must receive a written notice to move at least 30 days in advance of the required date to move. In this context, the term person includes residential occupants, homeowners, tenants, businesses, non-profit organizations, and farms.
- An occupant of a residence cannot be required to move until at least 90 days after a
 comparable replacement dwelling has been made available for occupancy. Only in
 unusual circumstances, such as when continued occupancy would constitute a
 substantial danger to the health or safety of the occupants, can vacation of the property
 be required in less than 90 days.

<u>SETTLEMENT</u>

The agency will make every effort to reach agreement with you during negotiations. You may provide additional information, and make reasonable counter offers for the agency to consider. When it is in the public interest, most agencies use the information provided as a basis for administrative or legal settlements, as appropriate.

EXPROPRIATION

If an agreement cannot be reached, the agency can acquire the property by exercising its power of eminent domain. It will do this by instituting formal expropriation proceedings with the appropriate State court and the procedures will follow State law. The court will set the final amount of just compensation after it has heard all arguments.

Litigation Expense

Normally, the agency does not reimburse you for costs you incur as a result of expropriation proceedings. The agency will reimburse you, however, under any of the following conditions:

- The court determines that the agency cannot acquire your property by expropriation.
- The expropriation proceedings are abandoned by the agency without an agreed-upon settlement.
- You initiate an inverse expropriation action and the court agrees with you that the agency
 has taken your real property rights without the payment of just compensation, or the
 agency elects to settle the case without further legal action.
- The agency is subject to State laws that require reimbursement for these or other expropriation costs.
- If ordered by the Court to pay these expenses.

RELOCATION ASSISTANCE BENEFITS

SECTION 1 - RELOCATION ADVISORY SERVICES

Any individual, family, business or farm displaced by a Federal or federally assisted program shall be offered relocation assistance services for the purpose of locating a suitable replacement property. Relocation services are provided by qualified personnel employed by the Agency. It is their goal and desire to be of service to you, and assist in any way possible to help you successfully relocate. Remember, the Agency's representative is there to **help** and **advise** you, so please be sure to make full use of their services. Ask questions and be sure you understand all your rights and benefits.

An individual with a disability will be provided the assistance needed to locate and move to a replacement dwelling or site. The individual should notify the Agency of any special requirements for assistance.

Residential Assistance

An agency representative will contact and interview you to find out your needs. Relocation services and payments will be explained in accordance with your eligibility. During the initial interview your housing needs and desires will be determined as well as your need for assistance.

Later, the agency representative will offer assistance and provide a current listing of comparable properties. You will be provided a written determination of the amount of replacement housing payment for which you qualify. The agency representative can supply information on other Federal and State programs in your area. Transportation will be offered to inspect housing referrals. The Agency will provide counseling or help you get assistance from other sources as a means of minimizing hardships in adjusting to your new location.

You cannot be required to move unless at least one comparable decent, safe, and sanitary (DSS) replacement dwelling is made available to you. Please let the agency representative

know if you locate a replacement dwelling so that it can be inspected to assure that it meets DSS standards.

Business, Farm, and Nonprofit Organization Assistance

An agency representative will contact and interview you to find out your needs and replacement site requirements and estimate the time needed to accomplish the move. Relocation services and payments will be explained in accordance with your eligibility. It is important to explain to the agency representative any anticipated problems. During the initial interview the agency representative will ask many questions to determine your financial ability to accomplish the move, including lease terms and other obligations.

The agency representative will help determine the need for outside specialists to plan, move, and reinstall personal property. The agency representative will identify and resolve any issues regarding what is real estate and what is personal property to be relocated. The agency representative will explore and provide advice as to possible sources of funding and assistance from other local, State, and Federal agencies. In addition, as needed, the agency representative will maintain listings of commercial properties and farms. The goal is to achieve a successful relocation back into the community.

Social Services Provided By Other Agencies

The agency representative will be familiar with the services provided by other public and private agencies in your community. If you have special problems, the agency representative will make every effort to secure the services of those agencies with trained personnel who have the expertise to help you. Make your needs known in order that you may receive the help you need.

SECTION 2 - INDIVIDUALS AND FAMILIES Moving Costs

If you qualify as a displaced person, you are entitled to reimbursement of your moving costs and certain related moving expenses. Displaced individuals and families may choose to be paid either on the basis of actual, reasonable, and necessary moving costs and related expenses, or according to a fixed moving cost schedule. If you elect to be moved by a professional mover, the agency will secure bids and provide you with an eligibility letter for the amount of the selected bid.

Actual, Reasonable Moving Costs

You may be paid for your actual, reasonable moving costs by a professional mover plus related expenses, **or** you may move yourself. Reimbursement will be limited to a 50-mile distance in most cases. Related expenses involved in the move may include:

- Packing and unpacking personal property.
- Disconnecting and reconnecting household appliances.
- Temporary storage of personal property.
- Insurance while property is in storage or transit.
- Transfer of telephone service and other similar utility reconnections.
- Other expenses considered eligible by the Agency.

Remember, all expenses must be approved and considered necessary and reasonable by the Agency and supported by paid receipts or other evidence of expenses incurred.

Moving Costs For Mobile Homes

If you are the owner of a displaced mobile home, you may be entitled to a payment for the cost of moving the mobile home to a replacement site on an actual cost basis. Displaced mobile home occupants may also be eligible for a payment for moving personal property from the mobile home such as furniture, appliances and clothing on an actual cost basis, or on the basis of a moving cost schedule. For a complete explanation of all moving cost options involving a mobile home, please discuss the matter with the agency representative.

LOUISIANA RESIDENTIAL MOVING COST SCHEDULE

A. UNFURNISHED UNITS (Furniture Owned by Occupant)

1	2	3	4	5	6	7	8	Each
Room	Rooms	Rooms	Rooms	Rooms	Rooms	Rooms	Rooms	Extra
\$600	\$800	\$1000	\$1200	\$1300	\$1550	\$1700	\$1900	\$300

B. FURNISHED UNITS (Furniture Not Owned by Occupant)

1	2	3	4	5	6	7	8	Each
Room	Rooms	Extra						
\$400	\$470	\$610	\$750	\$820	\$890	\$960	\$1030	\$70

EXCEPTIONS:

- a. A person displaced from a residential dwelling, including a mobile home, is eligible for a moving payment regardless of whether they move into DSS or NON-DSS housing.
- b. Payment for moving expenses shall be processed in accordance with Section 4.19.
- c. The payment to a person with minimal personal possessions who's in occupancy of a seasonal residence, dormitory style room, or a person whose residential move is performed by an Agency at no cost to them shall be limited to the amount stated in the Fixed Residential Moving Cost Schedule, Section B.
- d. Move of Mobile Home: Actual costs, plus a payment for packing and securing personal property on the basis of \$80.00 for the first room and \$40.00 for each additional room.

Replacement Housing

There are three types of replacement housing payments: purchase supplement, rental assistance, and down payment. To understand replacement housing payments, you first need to become familiar with the terms Comparable; Financial Means; Decent, Safe, and Sanitary (DSS); and Last Resort Housing.

Comparable

A **comparable** replacement dwelling must be DSS and functionally equivalent to your present dwelling. While not necessarily identical to your present dwelling, a comparable replacement dwelling should provide the same utility and function as the dwelling from which you are displaced. In addition, a comparable replacement dwelling should be:

- Adequate in size to accommodate the occupants (e.g., you and your family).
- Located in an area that is not subject to unreasonable adverse environmental conditions.
- Located in an area that is not less desirable than your present location with respect to public utilities and commercial and public facilities.
- Reasonably accessible to your place of employment.
- Located on a site that is typical in size for residential development with normal site improvements.
- Currently available on the private market.
- Within your financial means.

Financial Means

For a homeowner, if a purchase supplement is needed and provided, in addition to the acquisition price for your dwelling, then the replacement dwelling is considered to be within your financial means.

For a tenant, the monthly rent and estimated average monthly utility (electricity, gas, other heating and cooking fuels, water and sewer) cost for a comparable replacement dwelling is considered to be within financial means if, after receiving rental assistance, this amount does not exceed the base monthly rent (including average monthly utility cost) for the dwelling from which the tenant is displaced. The Agency may need to calculate the base monthly rent using 30% of the displaced tenant's total monthly gross household income, if that income qualifies as low income in accordance with established low income amounts determined by the U.S. Department of Housing and Urban Development (HUD). The Agency will also evaluate the amounts designated for shelter and utilities for a tenant that receives government assistance.

The rental assistance payment will be computed using the lesser of the three (rent and average monthly utility cost; 30% of the total monthly gross household income for a qualified low income tenant; or the total amount designated for shelter and utilities for a tenant receiving government assistance). To ensure the maximum benefit, it is important to provide the Agency appropriate evidence of total monthly household income when asked. There are some amounts that are not included as monthly household income, including income earned by dependents. The Agency will explain this procedure in greater detail.

Decent, Safe, and Sanitary

The DSS standard means the replacement dwelling meets the minimum requirements established by Federal regulations and conforms to applicable local housing and occupancy codes. The dwelling shall:

- Be structurally sound, weather tight, and in good repair.
- Contain a safe electrical wiring system adequate for lighting and other devices.
- Contain a heating system capable of sustaining a healthful temperature (approximately 70 degrees Fahrenheit) except in those areas where local climatic conditions do not require such a system.
- Be adequate in size with respect to the number of rooms and area of living space to accommodate the displaced person.
- Contain a well-lighted and ventilated bathroom providing privacy to the user and containing a sink, bathtub or shower stall, and a toilet, all in good working order and properly connected to appropriate sources of water and sewage drainage system.
- Contain a kitchen area with a fully usable sink, properly connected to potable hot and cold water and to a sewage drainage system, with adequate space and utility connections for a stove and refrigerator.
- Have unobstructed egress to safe, open space at ground level.
- Be free of any barriers which prevent reasonable ingress, egress or, in the case of a handicapped displaced person, use of the dwelling.

IMPORTANT NOTICE

Please understand that the replacement dwelling inspection for decent, safe, and sanitary requirements is conducted by the agency representative for the sole purpose of determining your eligibility for a relocation payment. Therefore, you must not interpret the Agency's approval of a dwelling to provide any assurance or guarantee that there are no deficiencies in the dwelling or in its fixtures and equipment that may be discovered at a later date. It is your responsibility to protect your best interest and investment in the purchase or rental of your replacement property and you must clearly understand that the Agency will assume no responsibility if structural, mechanical, legal, or other unforeseen problems are discovered after the inspection has been conducted.

Last Resort Housing

The term Last Resort Housing is an administrative procedure authorized by law to address those times when comparable replacement housing is not available under statutory limits specified in law. The law and regulation allow the Agency to provide a replacement housing payment in excess of the statutory maximums of \$7,200 and \$31,000. Because this provision is commonly used, the statutory maximums will not be restated throughout this brochure.

The Agency must provide comparable replacement housing, that is DSS and within your financial means, before you are required to move. The Agency may provide the necessary housing in a number of ways, such as:

- Making a replacement housing payment in excess of the maximum \$7,200 or \$31,000 statutory limits.
- Purchasing an existing comparable residential dwelling and making it available to you in exchange for your dwelling.
- Moving and rehabilitating a dwelling and making it available to you in exchange for your property.
- Purchasing, rehabilitating or reconstructing an existing dwelling to make it comparable to your property.
- Purchasing land and constructing a new replacement dwelling comparable to your dwelling when comparables are not otherwise available.
- Purchasing an existing dwelling, removing barriers or rehabilitating the structure to accommodate a handicapped displaced person when a suitable comparable replacement dwelling is not available.
- Providing a direct loan which will enable you to construct or contract for the construction of a decent, safe, and sanitary replacement dwelling.

Freedom of Choice

All eligible displaced persons have the freedom of choice in the selection of a replacement dwelling. The Agency will not require you, without your written consent, to accept a replacement dwelling provided by the Agency. If you decide not to accept the replacement housing offered

by the Agency, you may secure a replacement dwelling of your choice but it must meet the DSS standard. If you are eligible for Last Resort Housing, the agency representative will thoroughly explain the program to you.

Length of Occupancy - Basic Occupancy Requirements

The type of payment you are eligible for depends on whether you are an owner or a tenant, and how long you have lived in the property being acquired prior to the initiation of negotiations. "Length of occupancy" simply means counting the number of days that you occupied the dwelling before the date of initiation of negotiations by the Agency for the purchase of the property.

The term "initiation of negotiations" is usually the date the Agency makes the first personal contact with the owner of real property, or his/her representative, to provide a written offer to purchase the property being acquired.

Owners who were in occupancy 90 days or more prior to the initiation of negotiations may be eligible for a purchase supplement or a rental assistance payment.

Tenants who were in occupancy 90 days or more prior to the initiation of negotiations may be eligible for a rental assistance payment or a down payment.

Owners who were in occupancy less than 90 days prior to the initiation of negotiations, may be eligible for a rental assistance payment or a down payment, however, the down payment cannot exceed the amount you would have received if you had been a 90 -day owner.

If you were in occupancy at the time of the initiation of negotiations, but less than 90 days prior to that date, you are considered a displaced person entitled to relocation assistance advisory services and moving payments. You may be entitled to a rental assistance payment if comparable replacement rental housing is not available within your financial means. The Agency will use the financial means test described earlier in this brochure. You should meet with the agency representative for an explanation of the relocation benefits that you may be eligible to receive.

Replacement Housing - Purchase Supplement For Owner Occupants of 90 Days or More

If you are an owner and occupied your home for 90 days or more immediately prior to the initiation of negotiations for your property, you may be eligible – in addition to the value of your property – for a supplemental payment for costs necessary to purchase a comparable DSS replacement dwelling. The Agency will compute the maximum payment you are eligible to receive. You must purchase and occupy a DSS replacement dwelling within one year. A purchase supplement has three components: a price differential, an amount for increased mortgage interest and incidental expenses. The purchase supplement is in addition to the acquisition price paid for your property.

The price differential payment is the amount by which the cost of a replacement dwelling exceeds the acquisition cost of the displacement dwelling. You may also be reimbursed for increased mortgage interest costs if the interest rate on your new mortgage exceeds that of

your present mortgage. To be eligible your acquired dwelling must have been encumbered by a bona fide mortgage which was a valid lien for at least 90 days prior to the initiation of negotiations. Finally, you may be reimbursed for other expenses such as reasonable costs incurred for title search, recording fees, and certain other closing costs, but not for prepaid expenses such as real estate taxes and property insurance.

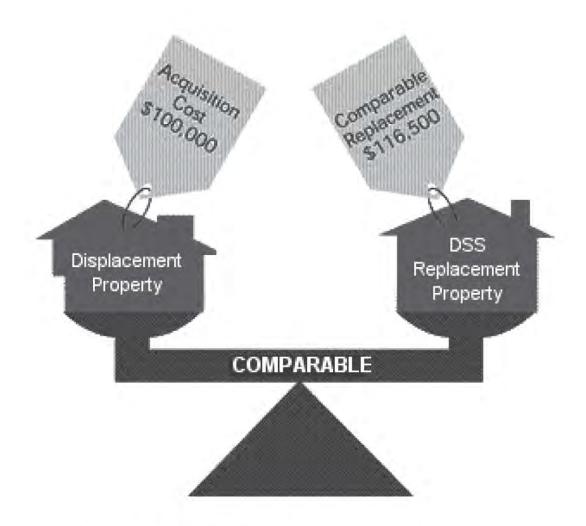
Example of a Price Differential Computation

Example A: Assume the Agency purchases your property for \$100,000. After a thorough study of available comparable residential properties on the open market, the Agency determines that a comparable replacement property will cost \$116,500. If you purchase a DSS replacement property for \$116,500, you will be eligible for a price differential payment of \$16,500.

Example B: If you purchase a DSS replacement property costing more than \$116,500, you pay the difference as shown in Example B.

Example C: If your purchase price is less than \$116,500, the price differential payment will be based on your actual cost.

Agency Computation of Maximum Price Differential Payment	Cost of Comparable Replacement Acquisition Price of Your Property Maximum Price Differential Payment	\$ 116,500 - 100,000 \$ 16,500
Example A	Actual Cost of Replacement Property (Same Purchase Price as Comparable) Acquisition Price of Your Property Price Differential Payment	\$116,500 - 100,000 \$ 16,500
Example B	Actual Cost of Replacement Property Acquisition Price of Your Property Difference Price Differential Payment You Are Responsible for This Amount	\$ 125,000 - 100,000 \$ 25,000 \$ 16,500 \$ 8,500
Example C	Actual Cost of Replacement Property Acquisition Price of Your Property Price Differential Payment Payment is Based on Actual Cost	\$ 114,000 - 100,000 \$ 14,000



Replacement Housing - Rental Assistance 90-Day Owners Who Elect to Rent

A rental computation will be computed based on a determination of the fair market rent for the acquired dwelling compared to a comparable rental dwelling available on the market. The difference will be multiplied by 42. In no instance will the rental assistance payment exceed the amount the owner would have received as a price differential.

For Owner Occupants and Tenants of 90 Days or More

Owner occupants and tenants of 90 days or more may be eligible for a rental assistance payment. To be eligible for a rental assistance payment, tenants and owners must have been in occupancy at least 90 days immediately preceding initiation of negotiations for the property. This payment is designed to enable you to rent a comparable DS&S replacement dwelling for a 42-month period. If you choose to rent a replacement dwelling and the cost of rent and utilities are higher than you were paying, you may be eligible for a rental assistance payment. The Agency will determine the maximum payment you may be eligible to receive in accordance with established procedures. The rental assistance payment is paid in a lump sum unless the Agency determines that the payment should be in installments. You must rent and occupy a DSS replacement dwelling within one year to be eligible.

Example: Assume you have been paying \$500 per month rent for the dwelling unit occupied by you and purchased by the Agency. You also pay \$150 per month for utilities (electricity, gas, other heating and cooking fuels, water, and sewer). The rental assistance payment computation always includes the cost of basic utilities (electricity, gas, other heating and cooking fuels, water, and sewer), as well as the cost of rent. If rent includes utilities, a separate computation is not necessary. After a study of the rental market, the Agency determines that a replacement rental unit, that is DSS and comparable to your unit, is available for \$645 per month. It is estimated that average monthly utility costs for the replacement unit will be \$175 per month. The maximum rental assistance payment you can receive is \$170 per month for a 42-month period, or a total of \$7,140.



Example A: If you select a DSS replacement dwelling unit that rents for \$695 per month plus \$175 for utilities, despite the availability of comparable DSS replacement rental units that rent for \$645 per month plus \$175 for utilities, you will receive the maximum amount computed by the Agency, or \$7,140. You will be required to pay the additional \$50 per month yourself.

Example B: If you select a DSS replacement dwelling unit that rents for more than your present unit, but less than the amount determined by the Agency as necessary to rent a comparable unit, your payment will be based on actual cost. For example, assume you select a replacement dwelling unit that rents for \$575 per month plus \$165 for utilities. On the basis of actual cost, you will be eligible for a payment of \$90 per month for 42 months, or \$3,780.

Agency Computation of Maximum Rental Assistance Payment	Rent You are Currently Paying Plus Cost for Utilities You are Paying	\$ 500 + 150 \$ 650
	Rent for a Comparable DSS Dwelling Estimated Cost for Utilities	\$ 645 + 175 \$ 820
	Difference (\$820-650=\$170) x 42 months Maximum Rental Assistance Payment	\$ 7140 \$ 7140
Example A	Actual Rent for DSS Replacement Property Plus Estimated Cost for Utilities	\$ 695 + 175 \$ 870
	Difference (\$870-650=\$220) x 42 months Rental Assistance Payment	\$ 9240 \$ 7140
Example B	Actual Rent for DSS Replacement Property Plus Estimated Cost for Utilities	\$ 575 + 165 \$ 740
	Difference (\$740-650=\$90) x 42 months Rental Assistance Payment	\$ 3780 \$ 3780

Replacement Housing - Down Payment Owner Occupants of 90 Days and Tenants of 90 Days

Owner occupants of 90 days and tenants of 90 days may be eligible for a down payment and incidental expenses. The Agency will determine the maximum down payment you may be eligible to receive based on its computation for a rental assistance payment. However, the payment for a displaced owner occupant shall not exceed the amount they would receive as a 90-day owner for the same property.

To be eligible for the full amount of the down payment assistance payment, the entire payment must be used to purchase a DSS replacement dwelling. The payment may be utilized for a down payment toward the purchase price and/or eligible incidental expenses. Incidental expenses include the reasonable costs of title search, recording fees, and certain other closing costs but do not include prepaid expenses such as real estate taxes and property insurance. You may be eligible for the reimbursement of loan origination or loan assumption fees if such fees are normal to real estate transactions in your area and do not represent prepaid interest. The combined amount of the down payment and incidental expenses cannot exceed the amount the Agency computed as your maximum rental assistance payment. The agency representative will explain how the Agency determines the maximum down payment assistance payment.

DSS REMINDER

It is very important to remember that the replacement dwelling you select must meet the basic DSS standard. Do not execute a sales contract or a lease agreement until a representative from the Agency has inspected and certified in writing that the dwelling you propose to purchase or rent meets the DSS standard. Please do not jeopardize your replacement housing payment by moving into a substandard dwelling.

Fair Housing Laws

Title VI of the Civil Rights Act of 1964 and Title VIII of the Civil Rights Act of 1968 set forth the policy of the United States to provide, within constitutional limitations, for fair housing throughout the United States. These Acts and Executive Order 11063 make discriminatory practices in the purchase and rental of residential units illegal if based on race, color, religion, sex, or national origin. Whenever possible, a minority person shall be given reasonable opportunity to relocate to a DSS replacement dwelling which is not located in an area of minority concentration that is within their financial means. This does not require an Agency to provide a displaced person with a larger payment than is necessary to enable the person to relocate to a comparable replacement dwelling.

<u>SECTION 3 - BUSINESS, FARMS, AND NONPROFIT ORGANIZATIONS</u> <u>Moving Cost Reimbursement</u>

Owners or tenants may be paid on the basis of actual, reasonable moving costs and related expenses or, under certain circumstances, a fixed payment. Actual, reasonable moving expenses may be paid when the move is performed by a professional mover or if you move yourself. Related expenses, such as personal property losses, expenses in finding a replacement site, and reestablishment expenses may also be reimbursable.

You must provide the Agency with an inventory of the personal property to be moved and advance notice of the approximate date of the move, unless the Agency specifically tells you these notices are not necessary. If you elect to be moved by a professional mover, the agency will secure bids and provide you with an eligibility letter for the amount of the selected bid. The Agency has the right to inspect the personal property at the displacement and replacement sites, and to monitor the move.

Actual Cost Move

You may be paid the actual, reasonable and necessary cost of your move when the move is performed by a professional mover or when you elect to move yourself, however, all your moving costs must be supported by paid receipts or other evidence of expenses incurred. In addition to the transportation costs of your personal property, certain other expenses may be reimbursable, such as packing, crating, unpacking and uncrating, and the disconnecting, dismantling, removing, reassembling, and reinstalling relocated machinery, equipment and other personal property. Other expenses such as professional services necessary for planning and carrying out the move, temporary storage costs, and the cost of licenses, permits and

certifications may be reimbursable. The agency representative will provide you with a complete explanation of reimbursable expenses.

Estimated Cost Move

If you agree to take full responsibility for all or part of the move of your operation, the Agency may approve a payment not to exceed the lower of two acceptable bids or estimates obtained by the Agency from qualified moving firms, moving consultants, or a qualified Agency staff employee. A low cost or uncomplicated move may be based on a single bid or estimate at the Agency's discretion. The advantage of this moving option is that it relieves you from documenting all moving expenses because the payment is limited to the amount of the lowest acceptable bid or estimate.

Direct Loss of Tangible Personal Property

Displaced businesses, farms, and nonprofit organizations may be eligible for a payment for the actual direct loss of tangible personal property which is incurred as a result of the move or discontinuance of the operation. This payment is based on the lesser of the value of the item for continued use at the displacement site less the proceeds from its sale, or the estimated cost of moving the item. The agency representative will explain this procedure in detail if this is a consideration for you.

Low Value High Bulk Property

If an Agency considers a personal property item to be of low value and high bulk, and moving costs are disproportionate to its value (such as minerals, metals, rock, or topsoil), the allowable moving cost payment shall not exceed the lesser of the amount which would be received if the property were sold at the site, or, the replacement cost of a comparable quantity delivered to the new business location.

Searching Expenses for Replacement Property

Displaced businesses, farms, and nonprofit organizations are entitled to reimbursement for actual, reasonable expenses incurred in searching for a replacement property, not to exceed \$2,500. Expenses may include transportation, meals, and lodging when away from home; the reasonable value of the time spent during the search; and other expenses determined to be reasonable and necessary by the Agency.

Fees paid to real estate agents or brokers to locate a replacement site may be reimbursed, exclusive of any commissions or fees related to the purchase of the site. Commissions and fees related to the purchase of a replacement site are not eligible relocation expenses and will not be reimbursed.

Related Eligible Expenses

In addition to the moving expenses listed above, costs for these items may be reimbursed if the Agency determines they are actual, reasonable, and necessary

- Connection to available nearby utilities from the right-of-way to improvements at the replacement site.
- Professional services to determine a sites' suitability for the displaced person's operation.
- Impact fees or one time assessments for heavy utility usage as determined necessary by the Agency.

Please discuss this with your agency representative before incurring these costs to assure that they are reimbursable.

Reestablishment Expenses

A small business, farm, or nonprofit organization may be eligible for a payment, not to exceed \$25,000, for expenses actually incurred in relocating and reestablishing the enterprise at a replacement site. To qualify, the business, farm, or nonprofit organization must not have more than 500 employees working at the site who will be displaced by a program or project. Reestablishment expenses may include, but are not limited to:

- Repairs or improvements to the replacement real property required by Federal, State, and local laws, codes or ordinances.
- Modifications to the replacement real property to make the structure(s) suitable for the operation.
- Construction and installation costs of exterior advertising signs.
- Redecoration or replacement such as painting, wallpapering, paneling, and carpeting when required by the condition of the replacement site.
- Advertising the replacement location.
- Estimated increased costs of operation at the replacement site during the first two years for items such as: lease or rental charges; personal or real property taxes; insurance premiums; utility charges (excluding impact fees).
- Other items that the Agency considers essential for reestablishment.

Fixed Payment For Actual Moving Expenses (In Lieu Payment)

Displaced businesses, farms, and nonprofit organizations may be eligible for a fixed payment in lieu of (in place of) actual moving expenses, personal property losses, searching expense, and reestablishment expenses. The fixed payment may not be less than \$1,000 nor more than \$40,000. For a business to be eligible for a fixed payment, the Agency must determine the following:

- Business owns or rents personal property that must be moved due to the displacement.
- Business cannot be relocated without a substantial loss of its existing patronage.
- Business is not part of a commercial enterprise having more than three other businesses
 engaged in the same or similar activity which are under the same ownership and are not
 being displaced by the Agency.

 Business contributed materially to the income of the displaced business operator during the two taxable years prior to displacement.

Any business operation that is engaged solely in the rental of space to others is not eligible for a fixed payment. This includes the rental of space for residential or business purposes.

Eligibility requirements for farms and nonprofit organizations are slightly different than business requirements. The computation for nonprofit organizations differs in that the payment is computed on the basis of average annual gross revenues less administrative expenses for the two year period specified. If you are interested in a fixed payment, please consult your agency representative for additional information.

Computation of Your Fixed Payment

The fixed payment for a displaced business or farm is based upon the average annual net earnings of the operation for the two taxable years immediately preceding the taxable year in which it was displaced, or a two-year period deemed more representative by the Agency. You must provide the Agency with proof of net earnings to support your claim. Proof of net earnings can be documented by income tax returns, certified financial statements, or other reasonable evidence acceptable to the Agency.

Fixed Payment Example

2012	2013	2014	
Annual Net Earnings \$16,500	Annual Net Earnings \$18,500	Year Displaced	
Average annual net e \$16,500 + \$18,500 = Fixed Payment = \$17	\$35,000 / 2 = \$17,50	0	

Project Office

The Agency may establish a relocation office near the project. Project relocation offices are usually open during hours convenient to persons being displaced, including evening hours when necessary. If the Agency opens a project office, the staff will be happy to assist you, answer questions, and will maintain various types of information.

Relocation Payments Are Not Considered To Be Income

No relocation payment received will be considered as income for the purpose of the Internal Revenue Code. No relocation payment received will be considered income for the purposes of determining eligibility or the extent of eligibility of any person for assistance under the Social Security Act or any other Federal law (except for any Federal law providing low-income housing assistance).

Right To Appeal

Any aggrieved person may file a written appeal with the head of the Agency if the person believes the Agency has failed to properly determine his or her eligibility for relocation assistance advisory services, or the amount of a relocation payment. If you have a grievance, you will be given a prompt and full opportunity to be heard. You will also have the right to be represented by legal counsel or other representative in connection with the appeal, but solely at your own expense.

The Agency will promptly review your appeal and consider all pertinent justification and information available to ensure a fair and full review. The Agency will provide you with a written determination as well as an explanation of the decision. If you are still dissatisfied with the relief granted, the Agency will advise you of your right to seek judicial review of the Agency decision.

An alien not lawfully present in the United States shall not be eligible to receive relocation payments or any other assistance provided under 49 CFR Part 24.

The information is provided to assist you in understanding the requirements that must be met by agencies, and your rights and obligations. If you have any questions, contact your agency representative.

NOTICE: Relocation Assistance payments cannot be made until the property is acquired by the Agency.*

^{*}In rare cases a Notice of Intent to Acquire may be issued which would allow payment of relocation benefits in advance of acquisition.

Title VI Plan

Notification of Rights

The Louisiana Department of Transportation and Development (LADOTD) assures that no person shall on the grounds of race, color, or national origin as provided by Title VI of the Civil Rights Act of 1964, and the Civil Rights Restoration Act of 1987 (P.L. 100.259) be excluded from participation in, be denied the benefits of, or be otherwise subjected to discrimination under any programs or activities. LADOTD assures every effort will be made to ensure nondiscrimination in all of its programs and activities, whether those programs and activities are federally funded or not (inclusive of additional Title VI Authorities and citations).

The Civil Rights Restoration Act of 1987, broadened the scope of Title VI coverage by expanding the definition of terms "programs or activities" to include all programs or activities of Federal Aid recipients, sub-recipients, and contractor/consultants, whether such programs and activities are federally assisted or not (Public Law 100259 [5.557] March 22, 1988.)

LADOTD will be responsible for initiating and monitoring Title VI activities, preparing required reports and other LADOTD responsibilities as required by 23 Code of Federal Regulation, (CFR) 200 and 49 Code of Federal Regulation 21.

in the event the LADOTD distributes federal aid funds to sub-recipient, the recipient will include Title VI language in all written agreements/contracts and will monitor for compliance.

Shawn D. Wilson, Ph.D.

Secretary, Louisiana Department of

Transportation and Development

Any individual, group of individuals or entity that believes they have been discriminated against on the basis of race, color or national origin by the Louisiana Department of Transportation and Development may file a Title VI complaint by submitting the agency's Title VI Complaint Form.

For all Title VI matters, please contact

Compliance Programs Director P.O. Box 94245 Baton Rouge, LA 70804-9245

Telephone Number: (225)379-1382 Fax Number: (225)379-1865

Authorities

Title VI of the 1964 Civil Rights Act provides that no person in the United States shall, on the grounds of race, color, or national origin be excluded from participation in, be denied the benefits of or be otherwise subjected to discrimination under any programs or activity receiving federal financial assistance (please refer to 23 CFR 200.9 and 49 CFR 21).

Civil Rights Restoration Act of 1987 broadened the scope of Title VI coverage by expanding the definition of terms "programs or activities" to include all programs or activities of Federal Aid recipients, subrecipients, and contractors, whether such programs and activities are federally assisted or not (Public Law 100259 [S. 557] March 22, 1988).

Environmental Justice (EJ) (Executive Order 12898) addresses disproportionate adverse environmental, social and economic impacts that may exist in communities, specifically minority and low-income populations.

Limited English Proficiency (LEP) (Executive Order 13166) addresses access to services for persons whose primary language is not English and who have limited ability to read, write, speak or understand English.

The 1970 Uniform Act (42 U.S.C. 4601) prohibits unfair and inequitable treatment of persons displaced or whose property will be acquired as a result of Federal financially assisted programs or activities.

Section 504 of the Rehabilitation Act of 1973 (29 U.S.C. 790) prohibits discrimination based on a handicap/disability.

The Federal Aid Highway Act of 1973 (23 U.S.C. 324) prohibits discrimination based on gender.

The Age Discrimination Act of 1975 (42 U.S.C. 6101) prohibits discrimination based on age.

Additional Authorities and Citations Include:

Title VI of the Civil Rights Act of 1964, 42 United States Code 2000d to 2000-4; 42 United States Code 4601 to 4655; 23 United States Code 109(h); 23 United States Code 324; Department of Transportation Order 1050.2; Executive Order 12250; Executive Order 12898; 28 Code of Federal Regulations 50.3

LADOTD Title VI Notice to Public

LDOTD hereby gives public notice that it is the policy of the department to assure full compliance with Title VI of the Civil Rights Act of 1964, the Civil Rights Restoration Act of 1987, Executive Order 12898 on Environmental Justice, and related statutes and regulations in all programs and activities. Title VI requires that no person in the United States of America shall, on the grounds of race, color, sex, national origin or disability/handicap be excluded from the participation in, be denied the benefits of, or be otherwise subjected to discrimination under any program or activity for which LDOTD receives federal financial assistance.

Any person who believes they have been aggrieved by an unlawful discriminatory practice under Title VI has a right to file a formal complaint with the LDOTD. Any such complaint must be in writing and filed with the LDOTD Title VI Coordinator within one hundred eighty (180) days following the date of the alleged discriminatory occurrence. Title VI Discrimination Complaint Forms may be obtained from the Compliance Programs Office by calling (225) 379-1382

Non-discrimination Complaint Procedures for Federally Assisted Programs or Activities

These procedures cover all complaints filed under Title VI of the Civil Rights Act of 1964, Section 504 of the Rehabilitation Act of 1973, Civil Rights Restoration Act of 1987, and the Americans with Disabilities Act of 1990, relating to any program or activity administered by LDOTD as to sub-recipients, consultants, and contractors.

Intimidation or retaliation of any kind is prohibited by law. The procedures do not deny the right of the complainant to file formal complaints with other state or federal agencies or to seek private counsel for complaints alleging discrimination.

Every effort will be made to obtain early resolution of complaints at the lowest level possible. The option of informal mediation meeting(s) between the affected parties and the Title VI Specialist may be utilized for resolution.

Procedure

- Any individual, group of individuals or entity that believes they have been subjected to discrimination prohibited by Title VI nondiscrimination provisions may file a written complaint with the LDOTD's Compliance Programs Office. A formal complaint must be filed within 180 calendar days of the alleged occurrence.
- 2. Upon receipt of the complaint, CPO will determine its jurisdiction, acceptability, need for additional information, and investigative merit of the complaint. In cases where the complaint is against one of LDOTD's sub-recipients of federal highway funds, the Department will assume the jurisdiction and will investigate and adjudicate the case.
- 3. Once CPO decides to accept the complaint for investigation, the complainant and the respondent will be notified in writing of such determination within five calendar days. The complaint will then be logged in CPO's records identifying its basis, the race, color, national origin and gender of the complainant.

- 4. In cases where LDOTD assumes the investigation of the complaint, CPO will provide the respondent with the opportunity to respond to the allegations in writing. The respondent will have 10 calendar days to furnish CPO his/her response to the allegations.
- 5. Within 50 calendar days of receipt of the complaint, the LDOTD's investigator* will prepare an investigative report for the Compliance Programs Director. The report shall include a narrative description of the incident, identification of persons interviewed, findings and recommendations for disposition. *This can be the Program Area Title VI Liaison or LDOTD's Title VI Specialist.
- 6. Once LDOTD investigative report becomes final, the parties will be properly notified of the outcome and appeal rights.
- 7. LDOTD's investigative report and a copy of the complaint will be forwarded to FHWA, within 60 calendar days of the receipt of the complaint.
- 8. If the complainant is not satisfied with the results of the investigation, s/he shall be advised of their rights to appeal LDOTD's determination to the FHWA Louisiana Regional Office, USDOT or USDOJ. Appeals must be filed within 180 days after LDOTD's final resolution. Unless new facts not previously considered come to light, reconsideration of LDOTD's determination will not be available.
- LDOTD will serve as appealing forum to a complainant that is not satisfied with the outcome
 of an investigation conducted by a LDOTD sub-recipient. LDOTD will analyze the facts of
 the case and will issue its conclusion to the appellant within 60 days of the receipt of the
 appeal.

QUESTIONS FREQUENTLY ASKED ABOUT RELOCATION ADVISORY SERVICES

1. Who is eligible to receive relocation advisory services?

Relocation Assistance Advisory Services shall be offered to:

- All persons occupying property to be acquired.
- All persons occupying property adjacent to the real property acquired when the Department determines that such persons are caused substantial economic injury because of the acquisition.
- All persons who, because of the acquisition of real property used for a business or farm operation, move from other real property used for a dwelling or move their personal property from such other real property.

2. As a "displaced person", what relocation assistance advisory services will be offered to me?

The Department's Real Estate Representative assigned to the project will:

- Give you a Real Estate Brochure;
- Determine your need, if any, for relocation assistance;
- Discuss and explain the services available, relocation payments and the eligibility requirements, and assist you in completing any applications or other forms required;
- Provide current information on the availability, prices, and rentals of comparable, decent, safe, and sanitary housing, and of comparable commercial properties and locations for displaced businesses;
- Assist you, if your business or farm is displaced, in obtaining and becoming established in a suitable replacement location;
- Supply information concerning Federal and State housing programs, disaster loan programs, and other Federal or State programs offering assistance to displaced persons;
- Provide advisory services in order to minimize hardships on adjusting to a new location

QUESTIONS FREQUENTLY ASKED ABOUT REPLACEMENT HOUSING PAYMENTS

1. If I own my home, can I elect to rent replacement housing? Yes.

QUESTIONS FREQUENTLY ASKED ABOUT MOVING EXPENSE PAYMENTS

NOTES

Louisiana Department of Transportation and Development

Title VI Discrimination Complaint Form

Name	Phone	Name of Person Discriminated	
Address (Street No., P.O. Box, E	Etc.)	Location and Position of Person (If known)	
City, State, Zip		City, State, Zip)
Discrimination Because Of: Race/ColorSexNational OriginRetaliation	Disabilit	yAge	Date of Alleged Incident
discrimination. Be sure to include you. Attach any written material	was involved persons were t	and witnessed the reated differently than	
Signature			Date

Please return this form to: **Heather Huval**

Title VI/ADA

LADOTD - Compliance Programs Office

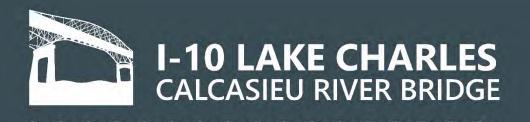
P.O. Box 94245

Baton Rouge, LA 70804-9245

Telephone Number: (225) 379-1923 Fax Number: (225) 379-1385



Agency/Public Officials Briefing and Public Meeting #4 Summary Report March 2021



PUBLIC INVOLVEMENT SUMMARY REPORT Elected Officials / Agency Briefing Public Meeting #4 and Other Outreach

I-10 Calcasieu River Bridge Improvements (I-10/I-210 West End to I-10/I-210 East End) Calcasieu Parish, Louisiana

State Project Number: H.003931



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ATTACHMENTS

Attachment A: Proof of Publication of Meeting Notice Advertisement

Attachment B: LADOTD Public Involvement Email Blast

Attachment C Press Release

Attachment D: Public Meeting #4 Attendees Lists

Attachment E: List of Public Meeting #4 Panelists

Attachment F: Copy of Recorded Presentation Slides and Script

Attachment G: Copy of Winter 2020 Newsletter

Attachment H: Copy of Website Survey Questions

Attachment I: Comments and Response Matrix

Attachment J: List of TAC Meeting Attendees and a Copy of Calcasieu River Bike/Ped Crossing

Recommendation

Attachment K: Outreach to Westlake Neighborhood Potentially Impacted by Railroad

Relocations

1.0 INTRODUCTION

The Louisiana Department of Transportation and Development (LADOTD), in coordination with the Federal Highway Administration (FHWA), are preparing an Environmental Impact Statement (EIS) for the I-10 Calcasieu River Bridge and Improvements Project. The proposed project is approximately 9 miles in length and includes alternatives for improvements to I-10 in the Lake Charles region between the I-210 interchanges, including the Calcasieu River Bridge as shown on **Figure 1**.

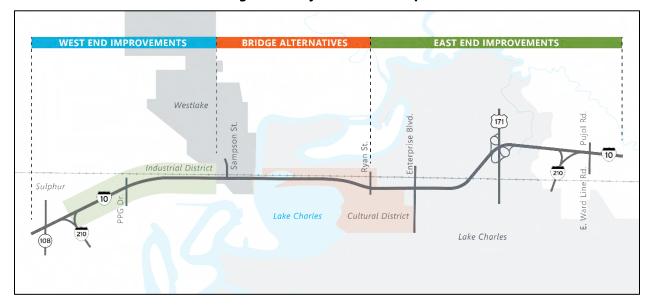


Figure 1: Project Location Map

This document summarizes the information provided and input obtained as part of the fourth round of public involvement activities for the EIS. Outreach to elected officials, agency representatives, stakeholders, and interested members of the public was performed to gather input about the reasonable alternatives being evaluated in the EIS and to solicit comments regarding implementation strategies such as tolling and public-private partnerships (P3). Besides being given the opportunity to comment on these items as well as other aspects of the project, people were encouraged to identify the alternative that they preferred, if they had one, and the reason for that preference.

2.0 AGENCY AND PUBLIC OFFICIALS BRIEFING

An Agency and Public Officials Briefing was held on Thursday, March 25, 2021, from 3:00 P.M. to 4:00 P.M., online via ZOOM, a video conferencing platform available free for download with both audio and video capabilities. A link to the ZOOM meeting was provided in the invitation to the briefing.

2.1 Meeting Invitations and Attendance

Meeting invitations were emailed to local agency representatives and public officials. A copy of

the email invite is displayed in Figure 2 and a list of those invited is provided in Table 1.

Figure 2: Agency and Public Officials Briefing Email Invitation

Lynn Maloney-Mujica H003931 I-10 Calcasieu Bridge Public Officials Briefing Subject: Location: Zoom Thu 3/25/2021 3:00 PM Start: End: Thu 3/25/2021 4:00 PM Show Time As: Tentative Recurrence: (none) Meeting Status: Not yet responded Organizer: Kelsey Heavin Subject: I-10 Calcasieu Bridge Public Officials Briefing When: Thursday, March 25, 2021 3:00 PM-4:00 PM (UTC-06:00) Central Time (US & Canada). On behalf of the Louisiana Department of Transportation and Development, HNTB is inviting you to a briefing for the Calcasieu River Bridge Project. This briefing is to provide public officials with an opportunity to view the presentation 37(93) 1 10 deserves Pidd (and ask questions prior to Public Meeting #4, which is being held later in the day (see attached notice). The briefing will be held online via ZOOM using the link below. Join Zoom Meeting https://us02web.zoom.us/i/83599814919 Meeting ID: 835 9981 4919 One tap mobile +13017158592,,83599814919# US (Washington DC) +13126266799,,83599814919# US (Chicago) Dial by your location +1 301 715 8592 US (Washington DC) +1 312 626 6799 US (Chicago) +1 929 205 6099 US (New York) +1 253 215 8782 US (Tacoma) +1 346 248 7799 US (Houston) +1 669 900 6833 US (San Jose) Meeting ID: 835 9981 4919

Name	Title	Agency/Organization	Email
Tony Guillory	District 4	Calcasieu Parish Police Jury	tguillory@calcasieuparish.gov
Bryan C. Beam	Parish Administrator	Calcasieu Parish Police Jury	administration@cppj.net
John Cardone, Jr.	City Administrator	City of Lake Charles	jcardone@cityoflc.us
Nic Hunter	Mayor	City of Lake Charles	nhunter@cityoflc.us
Stuart	Council Member,		
Weatherford	District E, President	City of Lake Charles	citycouncil@cityoflc.us
		City of Lake Charles	
	Director of Planning	Planning and Development	
Mike Huber	and Engineering	Director	mike.huber@cityoflc.us
	Council Member		
Joy Abshire	District 4	Council Member District 4	jabshire@sulphur.org
Mike Danahay	Mayor	City of Sulphur	Mayorsoffice@sulphur.org
	Council Member		
Melinda Hardy	District 3	City of Sulphur	mmhardy@sulphur.org
	Director of Planning	City of Sulphur Public Works	
Stacy Dowden	& Engineering	Department	sdowden@sulphur.org
Robert Hardey	Mayor	City of Westlake	bellpepperbc@yahoo.com
Andrea Manhouz	City Clerk	City of Westlake	cityclerk@cityofwestlake.org
		CPPJ Engineering/Public	
Allen Wainwright	Director	Works	tconner@cppj.net
		CPPJ Planning &	
Wes Crain	Director	Development	wcrain@cppj.net
Mike Hollier	Director	IMCAL/MPO	mike@imcal.la

Table 1: Agency and Public Officials Briefing Invitation List

2.2 Briefing Format and Materials

The Agency and Public Officials Briefing was conducted online with the ZOOM App. It included the recorded video presentation that would also be presented at the public meeting, followed by a question/answer session. Using ZOOM audio and video features, agency and public officials were able to discuss the proposed project with the Project Team. Meeting topics included: design of the reasonable alternatives, toll fees and feasibility, real estate impacts and transactions, railroad spur relocation, economic development, and bridge aesthetics.

3.0 PUBLIC MEETING #4

3.1 Advertisement & Outreach

A wide range of advertisement and communication channels were utilized to notify agencies, public officials, stakeholder organizations, and the general public about Public Meeting #4.

Public Meeting Notices – Postcards (**Figure 3**) were mailed and emailed to over 200 individuals including property owners potentially impacted by the proposed alternatives.

YOU ARE INVITED TO AN -10 LAKE CHARLES **ONLINE PUBLIC MEETING!** The purpose of the meeting is to: Thursday, March 25, 2021 Solicit comments from the public about the alternatives 5:30 P.M. - 7:00 P.M. CST and proposed improvements, benefits and impacts of each one, and implementation strategies such as tolling **ZOOM ONLINE** and public-private partnerships. A live introduction will be followed by a www.i10lakecharles.com recorded presentation. After the presentation, for a ZOOM link to attend the meeting You may join by phone at 646-876-9923, Passcode 459770 for questions and comments will be accepted through the chat feature and the project team will respond in the order received. Comments may also be submitted through the website at www.i10lakecharles.com, the project email, or by leaving a message at the project phone. The US postal address is on the back of this card. Comments received by April 5 will become part of the meeting record. Project Phone: 225-366-9645 Email: CalcasieuBridge@hntb.com

Figure 3: Public Meeting #4 Postcard Notice

Newspaper Advertisements – An advertisement (**Figure 4**) was placed in the Lake Charles American Press for publication two weeks and within one week prior to the public meeting.

Figure 4: Public Meeting #4 Newspaper Notice

NOTICE OF PUBLIC MEETING

Notice is hereby given that the Louisiana Department of Transportation and Development (LADOTD) and the Federal Highway Administration (FHWA) will conduct a live public meeting online via ZOOM.

State Project No. H.003931
Federal Aid Project No. BR-10-1(212)29
I-10 Calcasieu River Bridge
(I-10/I-210 West End to I-10/I-210 East End)
Calcasieu Parish, Louisiana

The live meeting will be held using ZOOM on date and time:

THURSDAY

March 25, 2021 5:30 p.m. to 7:00 p.m.

Go to www.i10lakecharles.com for a ZOOM link to attend the meeting If you do not have access to a computer or If you prefer, you may join by phone at 646-876-9923, Passcode 459770 (audio only).

A live introduction will be followed by a recorded presentation. After the presentation, questions and comments will be accepted through the chat feature and the project team will respond in the order received. Comments may also be submitted through the website at www.i10lakecharles.com, the project email at Calcasieubridge@hntb.com, or by leaving a message at 225-366-9645. These comments will be collected, and responses will be provided in the meeting record. You may also write to us at

I-10 Calcasieu River Bridge Project c/o HNTB Corporation Suite 640 10000 Perkins Rowe Baton Rouge, LA 70810

Three afternatives have been selected for detailed evaluation in the Environmental Impact Statement. Alternative 3A includes a long-span bridge, an extension of Sulphur Avenue east across the river via a moveable bridge, and a partial interchange at North Lakeshore. Similarly, Alternative 3E includes a long-span bridge and an extension of Sulphur Avenue east across the river to a full interchange at North Lakeshore. Alternative 5G includes a full interchange at Sampson Street elevated above the existing Sampson Street and two railroad crossings. This afternative would not extend Sulphur Avenue, but it would require moving two railroad spur tracks that cross the I-10 Service Road and Isle of Capri Boulevard. Common to all afternatives are improvements at PPG Drive, North Lakeshore and Veterans Memorial, Enterprise Boulevard, and the widening of I-10 to three through lanes in each direction.

The purpose of the public meeting is to solicit comments from the public about the alternatives and proposed improvements, benefits and impacts of each one, and implementation strategies such as tolling and public-private partnerships.

If you are unable to attend the meeting at the scheduled time, you may view the recorded presentation by visiting the project website at https://www.i10lakecharles.com/. The presentation and other informational materials will be available for viewing after the meeting. Comments and questions may be submitted by clicking on the "Tell Us What You Think" bubble.

All comments received at the ZOOM meeting, through the website, by phone, email, or US mail by April 3, 2021 will become part of the meeting record.

If you require special assistance due to a disability or need an interpreter to attend the videoconference meeting, please contact us at Calcasieubridge@hntb.com or through one of the other channels described above at least 5 days in advance so that we can provide accommodations.



Proof of publication of the advertisement on Thursday, March 11 and Sunday, March 21, 2021 is provided in **Attachment A.**

MyDOTD Meeting Announcement – LADOTD uses the MyDOTD to send press release/public meeting notices to local media and public subscribers. An official announcement was made on the MyDOTD webpage two weeks before the public meeting as shown on **Figure 5.**

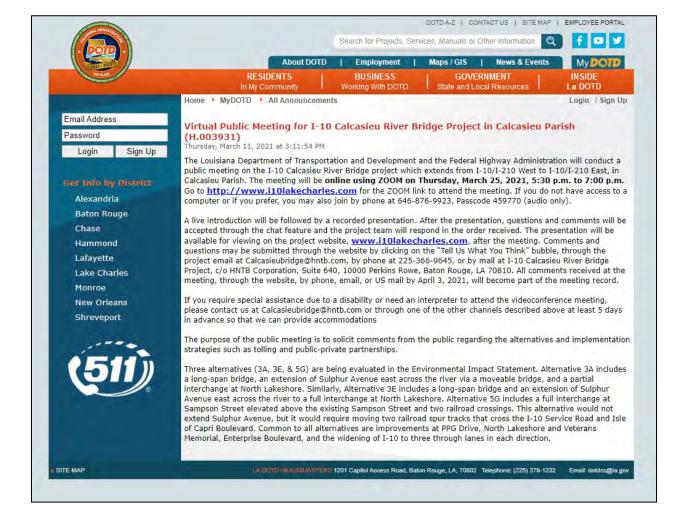


Figure 5: Public Meeting #4 MyDOTD Website Announcement

LADOTD Public Involvement Email Blast – LADOTD sent an announcement via an email blast on March 11, 2021. A copy of the announcement is provided as **Attachment B**.

Official Press Release – A press release issued to local media on March 22, 2021. A copy of the release is provided as **Attachment C.**

Social Media Posts – A meeting announcement (**Figure 6**) was posted on LADOTD social media the morning before the public meeting. The LADOTD Facebook page has over 60k followers and its Twitter pages has over 15k followers.

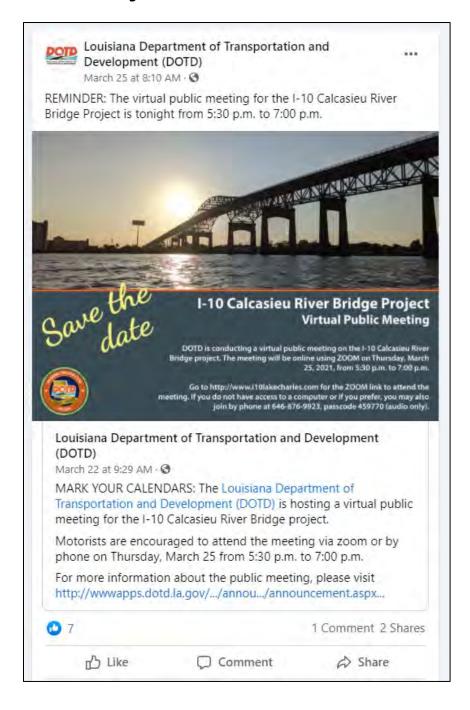


Figure 6: LADOTD – Social Media Post

3.2 Public Meeting #4 Attendance

Two hundred (200) people independent of the Project Team attended the Public Meeting #4. **Table 2** lists the participants by affiliation. The "Other" category represents members of the public or others whose affiliation could not be determined. A complete list of meeting Attendees list from Public Meeting #4 are included in **Attachment D**.

Table 2 summarizes the total number of meeting participants by affiliation.

Type of Participant **Tally of Participants** Consultant 81 Local Government 4 Other 111 Press 1 State Agency 1 State Government 2 Team 24 **Total participants** 224

Table 2: Tally of Public Meeting #4 Attendees by Affiliation

3.3 Meeting Format and Materials

Public Meeting #4 was held online using the ZOOM app. Participants were instructed to go to the project website to access the ZOOM link. A phone number with passcode was also provided to those who did not have access to a computer or who preferred to listen only.

The meeting was set-up using the ZOOM Webinar feature that allowed only the Project Team panelists to be seen and heard. A live introduction by the Project Team was followed by a 30-minute recorded video presentation and a Question and Answer (Q&A) period.

Live Introduction - Dr. Shawn Wilson, Secretary of Transportation, and Dr. Eric Kalivoda, Deputy Secretary of Transportation made opening remarks welcoming participants to the meeting. The meeting panelists were introduced, and a few process rules were explained. A list of the panelists is provided as **Attachment E**.

Recorded Video Presentation - A recorded video presentation with voiceover was played. The recorded presentation can be viewed online at www.i10lakecharles.com. A copy of the presentation with script notes is provided as **Attachment F** and the recorded presentation may be viewed online at www.i10lakecharles.com. An outline of the presentation topics is provided below.

- Welcome and Purpose Statement
- Project History
- Alternative Screening Process
- Reasonable Alternatives and Common Improvements
- Proposed Improvements Calcasieu Bridge:
- Featured Alternatives
- Comparative Impact Analysis Panels
- Railroad Spur Relocation for Alternative 5G
- Traffic Analysis
- Construction Closures for Alternative 5G
- Ethylene Dichloride (EDC) Contamination

- Public-Private Partnership (P3)
- Tolling Traffic and Revenue
- Permits, Mitigation, and Commitments
- More Opportunities for Public Input

Q&A - After the video presentation, participants were encouraged to submit their questions and comments via the ZOOM Q&A box. Many questions were read aloud, and the Project Team Panelists responded, generally on camera and using the audio feature so that all participants could see and hear the response. A few comments were responded to in writing using the Q&A box. All questions with responses were visible in the Q&A box to participants with computer access.

3.4 Comments Received

The public comment period officially opened on March 25, 2021 and ended April 5, 2021. However, comments were received through five channels:

- Submitting a written comment via the ZOOM Q&A box during Public Meeting #4
- Submitting a written comment to calcasieubridge@hntb.com
- Submitting a verbal comment by calling 225-366-9645
- Mailing a written comment to I-10 Calcasieu River Bridge Project c/o HNTB Corporation, 10000 Perkins Rowe, Suite 640, Baton Rouge, LA 70810; or
- Logging on to the project website (<u>www.i10lakecharles.com</u>) and selecting Contact Us or "Tell Us What You Think" link.

Project Email, Phone Number, and Mailing Address - A newsletter was mailed to an extensive list of interested parties in January 2021. The availability for making comments via the project email, phone number, mailing address, and website address were announced in that newsletter (see **Attachment G**). The project phone number, email, and mailing addresses were also announced in the Public Meeting #4 invitations and other outreach materials.

Website Survey and Comments - Comments received through the website required registration. In this manner, name, address, and contact information was collected for each registrant. They were also asked to answer a series of questions regarding their concerns, indicate their level of support for the project, express thoughts about how tolling might affect their driving habits, and state a preference for an alternative, if any, and why that alternative was preferred. A copy of the survey questions is provided in **Attachment H**.

Respondents were also offered a map to mark where they live and/or work. **Figure 7** is a copy of the map from the website survey. This comment feature was made available in January 2021.

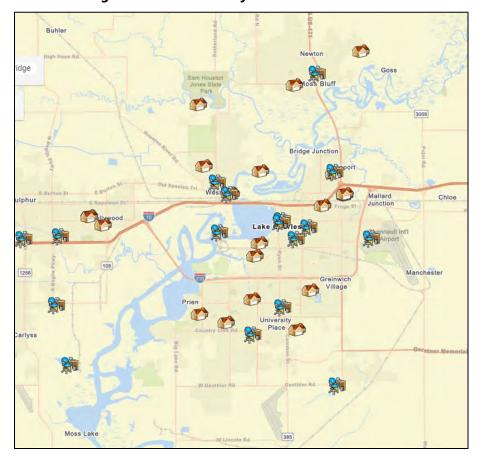


Figure 7: Website Survey Home and Work Locations

All comments and questions received through the five communication channels were compiled. As shown in **Table 3**, 151 individual submittals of comments and questions were received. Two-thirds of the comments were received at the Public Meeting using the ZOOM Q&A box.

 Source of Comment
 Tally of Comments Received

 Email
 21

 Letter via email
 1

 Phone
 1

 Project Website
 29

 Zoom Q&A Box
 99

 Total Comments
 151

Table 3: Comments Received by Source

Comments that were received through the five communication channels beginning in January 2021 through April 5, 2021 were added to the comments received at the public meeting. These comments are documented in the comments and response matrix in **Attachment I.**

Many of the individual submittals listed in **Table 3** contained more than one comment or question. Individual comments or questions were classified into 31 topics as shown in **Table 4**.

Table 4: Question and Comment Topics

Topic	Number of Questions / Comments
Preferred Alternative	39
5G	25
3E	7
3A	2
No Build	5
Bridge Design / Plans & Layouts	26
Tolls	26
Supports Tolling	2
Does Not Support Tolling	7
Supports Project	27
Yes	24
No	3
Funding	17
I-210	14
Traffic	14
Alternative Route	12
Vertical Clearance / Navigation	12
Construction Impacts	11
EDC	10
Pedestrian & Bike Facilities	10
Iconic / Signature Bridge	10
Movable Bridge	8
Westlake Impacts	8
P3	8
Costs / Estimates	7
Meeting / Comment Process	7
Schedule	6
Ryan to US 171	5
Visual / Aesthetics	5
Access	4
Local Economic Effects	4
Climate Change / Environmental Impacts	4
Drinking Water / Chico Aquifer	3
Railroad Relocations	3
Right of Way Acquisition / Relocations	3
Eagles	2
Recreational Resources	2
Noise	1
Cultural Resources	1
Total Number of Comments	306

The table also shows that 39 individuals expressed a preference for one of the build alternatives or the No Build. Approximately 64 percent of the respondents expressed a preference for Alternative 5G.

Due to the time limitation for Public Meeting #4, not all comments submitted through the Q&A box were answered. Public meeting notices and the meeting introduction advised participants that a response to unanswered questions would be provided in writing in the record of the meeting. The matrix provided in **Attachment I** includes responses to all the questions and comments received beginning in January 2021 through the end of the public meeting comment period on April 5, 2021.

4.0 ADDITIONAL OUTREACH

In addition to the activities described above, the Project Team provided information in advance of Public Meeting #4 to engage regional stakeholders and potentially impacted neighborhoods in Westlake.

4.1 Technical Advisory Committee Meeting Attendance

The Project Team was invited to a meeting of the Technical Advisory Committee (TAC) of the Calcasieu Metropolitan Planning Organization (MPO) and the Imperial Calcasieu (IMCAL) Planning and Development Commission on March 10, 2021 to present the reasonable alternatives and discuss the relative benefits and impacts.

The TAC meeting was attended by 37 individuals. **Table 5** categorizes the total number of TAC meeting participants by type. A list of attendees is provided in **Attachment J.**

Type of Participant	Tally of Participants
TAC Members	11
Transportation Policy Committee Members	1
MPO / IMCAL Staff	12
Guests	13
Total participants	37

Table 5: TAC Meeting Attendance Tally by Type of Participant

At the meeting, a subcommittee of the MPO/IMCAL made a recommendation supported by a PowerPoint document entitled "Calcasieu River Bike/Ped Crossing Recommendation". A copy of the PowerPoint document is also included in **Attachment J**.

4.2 Outreach to Westlake Neighborhood Potentially Impacted by Railroad Relocations

On March 10, 2021, the Project Team distributed door hanger/flyers in the Westlake neighborhood that could be impacted by the railroad spur relocation needed for construction of Alternative 5G. The door hangers/flyers were left at the residences between the Kansas City Southern (KCS) track at Pilley Street and Union Railroad Avenue) and Miller Avenue. Flyers were also left with representatives of businesses operating on Miller Avenue that could be impacted by the spur relocation of Alternative 5G. A copy of the doorhanger/flyer is provided in **Attachment K.**

LOCAL • STATE

BILL

Continued from A1

to make of tremendous consequence," said House Speaker Nancy Pelosi, D-Calif., "a decision that will make a difference for millions of Americans, saving lives and livelihoods."

For Biden and Democrats, the bill is essentially a canvas on which they've painted their core beliefs — that government programs can be a benefit, not a bane, to millions of people and that spending huge sums on such efforts can be a cure, not a curse. The measure so closely tracks Democrats' priorities that several rank it with the top achievements of their careers, and despite their slender congressional ma-

suspense over its fate.

They were also empowered by three dynamics: their unfettered control of the White House and Congress, polls showing robust support for Biden's approach and a moment when most voters care little that the national debt is soaring toward a stratospheric \$22 trillion. Neither party seems much troubled by surging red ink, either, except when the other is using it to finance its priorities, be they Democratic spending or GOP tax cuts.

Republicans noted that they've overwhelmingly supported five previous relief bills that Congress has approved since the pandemic struck a year ago, when divided government under then-President Donald Trump forced the parties to negotiate.

Lawmakers slam LSU response to sexual misconduct report

By The Associated Press

BATON ROUGE — Female state lawmakers on Wednesday lambasted Louisiana State University's response to a scathing report about its handling of sexual misconduct complaints, calling the university's disciplinary decisions weak and insulting to students whose abuse allegations were

mishandled. The legislators singled out for blistering criticism LSU's decision to briefly suspend rather than fire two employees for years of botched responses to claims of sexual harassment, domestic violence and

"I'm kind of just dumbfounded. I'm sad. I'm disgusted. I'm so disappointed in LSU, and I continue to be disappointed with the response that we're getting right now," said Rep. Paula Davis, a Baton Rouge Republican. "It's just

not enough." Rep. Barbara Freiberg, also a Baton Rouge Republican, called the discipline a "slap on the hand."

Franklinton Republican Sen. Beth Mizell, the Senate's second-ranking leader, added: "I don't think this is satisfactory at any level. The system is finding a way to protect the status quo."

The hearing involved the Senate Select Committee on Women and Children, but it drew female lawmakers from both the House and Senate and a few male lawmakers, too.

The meeting followed last week's release of a report from the law firm Husch Blackwell, hired by LSU to review its handling of sexual misconduct, harassment and discrimination complaints under federal Title IX laws. The lengthy

report details inappropriate responses and too few resources dedicated to the task.

Kansas ousted football coach Les Miles after the report detailed allegations of inappropriate behavior with students during his tenure at LSU. But no one whose conduct was criticized by Husch Blackwell has been fired by

LSU suspended executive deputy athletic director Verge Ausberry for 30 days and senior associate athletic director Miriam Segar for 21 days, without pay.

APPEAL

Continued from A1

jorities there was never real

truck and shot him. The victim crashed into a tree and later died in a local hospital.

Colston, who oversaw multiple rental properties, sometimes carried large sums of cash and prosecutors said at trial that robbery was the motive in the crime.

Judge Guy Bradberry, in sentencing Johnson, said laws passed by the Legislature as well as pending laws pertain ing to crimes committed by those who were under the age of 18 at the time of their crimes, had to be taken into

Because of Johnson's age

times before it took place.

Prosecutor Jacob Johnson said the state wanted Johnson to be sentenced to life without parole. Johnson said the crime was an especially heinous murder of an elderly man who he said was just going about

killed. Defense attorney King Alexander, in arguing for the possibility of parole for the defendant, said nobody knows what Johnson may be able to accomplish during 25 years behind bars and could possibly prove himself redeemable and worthy of life outside of

Philip Colston, son of the victim, read a statement to the court, which said, in part: "That day (of the murder) will haunt me and my family for the rest of our lives. Your ac-

tions have a lasting impact to

ARRESTS

all who knew or were impacted by my father. You committed a horrible crime and now

your actions." Alexander said he advised sentencing but said "that is

empathy on his part.' Bradberry, before sentencwhere we find ourselves today. Is Stacy Johnson permanently has put the court in a position

tal ball to look into the future." sentence at Louisiana State Penitentiary in Angola, will be parole-eligible 25 years

ence to Scott.

By Lisa Addison

laddison@americanpress.com

A 48-year-old former daycare employee has been arrested for cruelty to a juve-

nile, according to authorities. Annette L. Scott of Sulphur was arrested and booked into the Calcasieu Correc-

tional Center. She was released the following day after posting a \$50,000 bond set by Judge

laddison@americanpress.com

By Lisa Addison

David Ritchie. On March 3, Calcasieu Parish Sheriff's Office detectives received a complaint in refer-

Former daycare worker arrested

During the investigation, detectives were advised that when a mother picked up her 3-year-old from daycare, the girl had several scratches on her face. During the initial investigation, detectives were advised Scott had been terminated prior to the mother's

arrival at the daycare. After further investigation, on March 8, detectives questioned Scott about the scratches on the child, at which time she allegedly confirmed she scratched the child

on the cheek. Scott further reportedly advised detectives she was trying to make the child stay still for nap time. She told detectives she left the daycare after reportedly realizing she

left marks on the child. CPSO Det. James Jones is the lead investigator on this

consideration at sentencing.

and the complexity of the issues, his sentencing was

scheduled and postponed three

his business when he was

you have to pay the price for

his client not to comment at not to be taken as a lack of

ing Johnson, said, "Here's incorrigible and irretrievably depraved as defined by the Legislature? The Legislature where we almost need a crys-

Johnson, who is serving his after his sentence was imple-

mented.

The Calcasieu Parish Sheriff's Office reported the following arrests: Kecee Clint Lewis, 35, 503 W.

Alley St., Iowa, La. — domestic abuse battery, child endanger-

Shannon Eugene Simonds, 49, 292 Kelly Lane, Sulphur domestic abuse battery.

Eddie Dewayne Reado, 41, 113 Neely Road No. 1, Westlake domestic abuse battery.

Jeremy Gene Dechara, 24, Jarreau — thìrd-degree rape.

Kevin Clay Marcantel, 43, 2821 Kari Lane — simple burglary; simple criminal damage to property less than \$1,000; theft from \$1,000 but less than \$5,000

Dirone Andrew Collins, 54, 1807 Rose St. — illegal possession of stolen firearms, first offense; three counts drug possession; prohibited acts, drug paraphernalia; possession of firearm or carrying concealed weapon by a person convicted

of certain felonies. Adrian Reyes Orozco, 32, 826 Derry Drive, Sulphur - domestic abuse battery, child endan-

germent. Bond: \$8,000 Lindsey James Francis Jr.,

22, 1045 Walters St., No 515 battery of a dating partner. first offense; simple battery; possession of marijuana, first offense (14 grams or less); direct contempt of court.

Justin Michael Duhon, 31, 2709 Laurel Ridge Court — violations of protective orders; direct contempt of court.

State Project No. H.003931

Federal Aid Project No. BR-10-1(212)29

I-10 Calcasieu River Bridge (I-10/I-210 West End to I-10/I-210 East End)

Calcasieu Parish, Louisiana

Anastasia Joy Waddell, 36,

1607 Nichols St., Vinton domestic abuse aggravated assault; drug possession; illegal carrying of weapons during commission of a crime or in presence of CDS; prohibited act, drug paraphernalia; two counts direct contempt of court. Annette Langley Scott, 48,

2119 Kathy Drive, Sulphur cruelty to juveniles. Milton Frank, 39, 7315 McCown

Road No. 19, Iowa, La. - operating while intoxicated, first offense; use of certain wireless text messaging prohibited; two counts drug possession.

Nathan Lee Brown, 22, 211 Doe Road, Ragley — battery of a dating partner, first offense; direct contempt of court.

Jason Wayne Basco, 44, 805 Manuel Road — theft of a firearm; theft from \$1,000 but less than \$5,000; simple criminal damage to property less than \$1,000 theft less than \$1,000. Bond: \$35,500.

Lacey Ann Benoit, 37, 611 Ash St., Iowa, La. — domestic abuse

Adrian Everette Jackson, 40, 317 Orrin St. — simple criminal damage to property less than \$1,000; direct contempt of court; headlamps on motor vehicles; possession of marijuana, first offense (14 grams or less); drug possession; prohibited acts, drug paraphernalia; no drivers license on person; evidence of motor vehicle liability security contained in vehicle; resisting an officer by flight.

Betty Andrepont Perrodin, 61, New Orleans — possession of synthetic marijuana, first offense; four counts drug possession; possession of alcoholic beverages in motor vehicles; illegal use of controlled dangerous substances in the presence of persons under 17; prohibited acts, drug paraphernalia; certain activities regarding contraband in penal institutions prohibited; creation, distribution or possession with intent a counterfeit narcotic, Bond: \$6,300.

Phillip Andrew Prisoc, 52, Lakeland, Fla. — theft of a motor vehicle from \$1,000 but less than \$5,000. Bond: \$5,000.

Earl Joseph Bellard Jr., 48, 2829 General Wainwright — duty of offenders to notify law enforcement of change of address, residence or other registration information; failure to register and notify as a sex offender or child predator-failure to pay annual registration fee; two counts failure to register and notify as a sex offender or child predator, second offense.

Richard Robert Royer, 50, 1100 Hollie Lane, DeQuincy — ob-struction of justice; false swearing for purpose of violating public health or safety. Bond: \$10,000.

LOTTERY

Here are the results from Tuesday's drawings in games of area interest:

Pick 3: 1-1-2 Pick 4: 2-6-3-4

Mega Millions: 4-33-46-58-65 MB: 13 MP:2

pending late afternoon.

A homeless man has been arrested for failure to register as a sex offender after he went to a local hospital and told personnel he wanted to turn himself in to law enforcement, according to authori-

Israel J. Lachica, 67, was arrested and booked into the Calcasieu Correctional Center and charged with failure to register as a sex offender second offense. His bond was

On March 9, at approximately 3:30 p.m., Calcasieu

Sex offender turns self in to law enforcement

Parish Sheriff's Office deputies were dispatched to a local hospital in reference to Lachica reportedly advising personnel he wanted to turn himself in to authorities. When deputies arrived,

they spoke with Lachica and it was discovered that the man, who is a registered sex offender, had been living in Calcasieu Parish and failed to notify local law enforcement,

making him in violation of his sex offender registration requirements.

Lachica was convicted of molestation of a juvenile in Terrebonne Parish in 2003. In 2007 and again in 2012,

Lachica was convicted of failure to register as a sex offender. CPSO Sr. Cpl. Bradley La-

mar was the arresting deputy on this case. CPSO Det. Cory Myers is

the lead investigator on the



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ICE OF PUBLIC MEETING

Notice is hereby given that the Louisiana Department of Transportation and Development (LADOTD) and the Federal Highway Administration (FHWA) will conduct a live public meeting online via ZOOM. The live meeting will be held using

ZOOM on date and time:

THURSDAY March 25, 2021 5:30 p.m. to 7:00 p.m.

Go to www.i10lakecharles.com for a ZOOM link to attend the meeting If you do not have access to a computer or if you prefer, you may join by phone at 646-876-9923, Passcode 459770 (audio only).

A live introduction will be followed by a recorded presentation. After the presentation, questions and comments will be accepted through the chat feature and the project team will respond in the order received. Comments may also be submitted through the website at www.itDlakecharles.com, the project email at Calcasieubridge@hntb.com, or by leaving a message at 225-366-9645. These comments will be collected, and responses will be provided in the meeting record. You may also write to us at

> I-10 Calcasieu River Bridge Project c/o HNTB Corporation Suite 640 10000 Perkins Rowe

> > Baton Rouge, LA 70810

Three alternatives have been selected for detailed evaluation in the Environmental Impact Statement. Alternative 3A includes a long-span bridge, an extension of Sulphur Avenue east across the river via a moveable bridge, and a partial interchange at North Lakeshore. Similarly, Alternative 3E includes a long-span bridge and an extension of Sulphur Avenue east across the river to a full interchange at North Lakeshore. Alternative 5G includes a full interchange at Sampson Street elevated above the existing Sampson Street and two railroad crossings. This alternative would not extend Sulphur Avenue, but it would require moving two railroad spur tracks that cross the I-10 Service Road and Isle of Capri Boulevard. Common to all alternatives are improvements at PPG Drive, North Lakeshore and Veterans Memorial, Enterprise Boulevard, and the widening of I-10 to three through lanes in each direction.

The purpose of the public meeting is to solicit comments from the public about the alternatives and proposed improvements, benefits and impacts of each one, and implementation strategies such as folling and public-private partnership

If you are unable to attend the meeting at the scheduled time, you may view the recorded presentation by visiting the project website at https://www.i10lakecharles.com/. The presentation and other informational materials will be available for viewing after the meeting Comments and questions may be submitted by clicking on the "Tell Us What You Think" bubble.

All comments received at the ZOOM meeting, through the website, by phone, email, or US mail by April 3, 2021 will become part of the meeting record

If you require special assistance due to a disability or need an interpreter to attend the videoconference meeting, please contact us at Calcasieubridge@hntb.com or through one of the other channels described above at least 5 days in advance so that we can provide accommodations.

FROM PAGES A1, A3

DEATHS

Continued from A3

Andrew Bailey Fontenot



We mourn the loss of our husband, father, brother and son who always lived bigger than life itself. Andrew "Dru" Bailey Fontenot was born March 21, 1971 to Brenda and Bill Fontenot and left us on March 17, 2021

Dru was a lifelong resident of Lake Charles and a graduate of St. Margaret's Catholic School, St. Louis High School and McNeese State University. During his career, Dru was involved in a number of entrepreneurial endeavors. His most gratifying work came in the aftermath of Hurricane Rita. Working with Crowder Gulf, FEMA, and state and local governments, he brought all parties together to accomplish a common goal in recovering from the devastation left in Rita's wake. We were all so proud of his tireless and selfless efforts to help so many who were without.

Dru had zest for life, people and living large. He lived, loved and played hard. Always the life of the party, Dru's sense of humor was unequalled. In the best and the worst of times, Dru would always keep you laughing.

Beneath his jovial persona, Dru had a heart of gold. He was always trying to help someone out in one way or another. He was blessed with good looks, charm, charisma, out of this world intelligence and a heart for service. Dru knew lots of people from all walks of life. All were treated with respect and greeted

with that big smile and a warm handshake or hug. He made people, all people feel important. He was admired

Dru also loved the outdoors. Hunting and fishing were favorites of his but best when done with family and friends. However, Dru's greatest love was for family, his whole family. We all loved him unconditionally.

Dru is survived by his loving and caring wife, Cindi Reed Fontenot, son, Kade Reed Brashear, his mother Brenda Bailey Fontenot Mc-Call, husband Bob, sister Renee' Fontenot Free, brother Todd Fontenot, wife Beth, brother Andre' Fontenot, wife Marianne, nieces Cydney Free, Lauren Fontenot, Emily Fontenot, nephews Corey Free, Jacob Fontenot, Gavin and Garin Fontenot, and Will Fontenot. He was preceded in death by his father, William "Bill" Fontenot.

Visitation is on Monday, March 22, 2021 at Lake City Baptist Church from 5-8 p.m. with a rosary beginning at 7:30 p.m. On March 23 visitation will begin at 9 a.m. at St. Martin de Porres Catholic Church with mass at 11 a.m., all in Lake Charles.

In lieu of flowers please make donations to Ronald McDonald House, rmhc.org.



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- · Email all obits to obits@americanpress.com
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Dorothy Brammer Hanna



Dorothy Brammer Hanna, a lifelong resident of Calcasieu Parish, passed away on March 2, 2021, at age 95. Dorothy was baptized and confirmed in the faith at St. John Lutheran Church in Lake Charles. Dorothy graduated from high school in Lake Charles, from junior college at McNeese, from LSU with a BA in Music Education and from McNeese State University with a Masters degree in Music.

In 1946, she began teaching vocal music in the Vinton public school system. A year later she married Wallace (Jake) Hanna, Vinton High's football coach and later principal. Together they taught, disciplined and encouraged hundreds of students over 25 plus years. Dorothy taught vocals to Bobby Kimball, Toto's lead singer, whose hits include "Rosanna" and "Africa," Bobby visited Dorothy several times with autographed albums and thanked her for his success!

Dorothy sang alto for over 40 years in the Lake Charles Messiah Chorus and Orchestra, held annually at Mc-Neese. As a gifted pianist she accompanied choirs at First United Methodist Church on Broad Street in Lake Charles. After a generous gift, McNeese State University named the Department of Performing Arts after Dorothy and her late husband.

Dorothy was preceded in death by her parents, Peter and Edith Brammer; her husband of 54 years, Wallace Astor Hanna; her brother Karl Brammer of Gonzales. La, and her sister Shirley Rhodes of West Fork, Ark. She is outlived by her sister Lyndal Munoz of Tulsa, OK and Nephews Anthony Munoz, Daniel Munoz (Eve), Paul Munoz (Amber) and Carl Starr (all of Tulsa, Okla.); Robin

Rhodes (Brenda) of West Fork, Ark.: Roger Anderson (Rebecca) or Minden, La; Peter Brammer (Teresa) of Lake Charles, La; and Nieces Cynthia Griffith of Baton Rouge, La; Mattie Olson (Rod) of Gonzales, La; Twila Capshaw (Gary) of Farmington. Ark.; Elizabeth Decker (Dan) of Janesville, WI; and beloved cousin Billy Dodd (Nanette) of Willis, Texas.

A memorial service will be held at First United Methodist Church when Covid singing restrictions have been lifted. Thanks to her long term caregiver, Carly Fuselier Trahan, the staff at the Veranda, Lake Charles; and the staff (especially Lucky) at her Hurricane Laura evacuation home, the Gardens at Brook Ridge in Pharr, Texas.

Dora Lee Hayes



Dora Lee Hayes, 93, of Sulphur gained her Heavenly reward on Thursday, March 18, 2021.

A lifelong Sulphur resident, Dora was graduate of Sulphur High School Class of 1944. She worked at the Sulphur Mines and met her husband while working at Firestone Tire & Rubber Plant. Dora later obtained her cosmetology license; she was the owner and operator of Maplewood Beauty Salon for over 20 years. She was a member of Belleview Baptist Church of Westlake. Dora loved fishing, cooking, and playing Bingo. She adored her family and cherished time spent together.

She was reunited in Heaven with her husband of 71 years, Andrew U. "Red" Haves: son Garey Haves: infant son, Randall Hayes; her parents, Foster and Ada Griffith; and siblings, Carl Griffith, Shirley Griffith, Junior Griffith, Pearl Trice, Thelma Barton, and Ethel Dore.

Left to cherish her memory are her daughter, Carla Bragg and husband Tom of New Caney, Texas; daughter-in-law, Judy Hayes

State Project No. H.003931

Federal Aid Project No. BR-18-1(212)29

I-10 Calcasieu River Bridge

(I-10/I-210 West End to I-10/I-210 East End)

Calcasieu Parish, Louisiana

of Westlake; grandchildren, Jesse Bragg and wife Colleen, Roger Bragg and wife, Amanda, Kacee Boxley and husband Philip, and Randy Hayes; great grandchildren, Taylor, Megan, Andrew, William, Jacob, Nolan, and Hunter.

Funeral services honoring Dora's life will be 1 p.m., Tuesday, March 23, 2021 in Johnson & Robison Funeral Home. Burial will follow in Mimosa Pines Cemetery South. Visitation will be 11 a.m. - 1 p.m., Tuesday in the funeral home. Words of comfort and memories may be shared with the family at www.robisonfuneralhome.

The family wishes to express a special "Thank You" to the administration and staff of Stonebridge Assisted Living for the excellent care given to Dora. Also, a special note of gratitude to Heart of Hospice for the compassion and care given.



Terry Michael Hogue

Terry Michael Hogue,

66, passed away Thursday,

March 11, 2021 at his home

Gaye and caregivers, Chris

and Diane with his dog and

flying, motorcycles, fishing,

guns, eating out, traveling

and living life to its fullest.

He was also a good friend,

law. He will be missed by all

vived by his wife of 36 years

Oreda Gaye Morgan Hogue.

nurse, soldier and son in

who knew him. He is sur-

Princess his Boston Ter-

rier dog. His aunt Lucille

Starrett, his mother-in-law,

Claudette Morgan. He is also

survived by his cousin, Alan

Starrett, New Zealand. He

is preceded in death by his

parents Kenneth Dale Hogue

and Florence Park Hogue as

Gallant. He had many friends

who will miss him very much,

well as his stepfather, Jack

friend Princess. Terry loved

in Denham Springs. He was

surrounded by his wife Oreda

Special thanks to caregivers Chris, Diane, and Wanda and Amedisys Home Health. Terry was a high School drop out, but in the Army, during the Viet Namwar, he received his high school diploma. Terry graduated from Southeastern La. University with a degree in Psychology. He also received an RN degree from Our Lady of the Lake College, where he was employed for 21 years. He served 30 years in the US Army from which he retired as a Major in 2001. He received a Graduate Degree also from Command and General Staff College with the US Army. Terry grew up in New Orleans, moved to Baton Rouge after graduating from college and serving 2 years at Fort Polk. Later after meeting and marrying his wife, who was a blind date, they bought their home in 1993 in Denham Springs. Denham Springs became his home until his death. Now he has a new home in Heaven with his Lord and Savior. Memorial service at a later date.

See **DEATHS**, A5

Cameron picks assessor

By Pamela Sleezer psleezer@americanpress.com

In a tight race, Cameron Parish voters elected Howard Scott Lavergne as parish assessor Saturday, according to unofficial results from the Secretary of State's Office.

Lavergne clinched his victory with 52 percent, or 733 votes, over opponent David Conner who won 671 votes. The results showed a voter turnout of 26 percent.

With 85 percent of votes, parishwide Mosquito Abate ment District 1 will continue to levy a tax of ten mills for the next 10 years, with an expected \$4,671,223 to be collected each year.

The Fire Protection District 14 continuation of a 5.70 mills tax was approved for the next 10 years with 83 percent, or 629

The Fire Protection District 15 continuation of an 8.16 mills tax for the next 10 years was approved with 82 percent of votes. The tax increased by .16 over the previous millage rate that ends at the end of this year. The new millage rate will go into effect in 2022.

The Water and Wastewater District No. 1 millage rate of 9.40 was renewed with approval from 67 percent of votes, or 35 out of a total 52 votes cast on Saturday.

Waterworks District 11 will continue to levy a 5.70 mills tax for the next ten years.

415 N. MAIN ST. - JENNINGS -337-824-7136 Barriers to Women's Sexual Health

and How Your Doctor Can Help

Friday Mar h 26 at 12 noon Facebook.com/LakeCharlesMemorial ONLINE ONL seminar!

Women's sexual health is not often discussed, yet 40% of women report struggling with their sexual health. Many of the medical problems that impact women and prevent sexual satisfaction have good medical treatments. Because of these barriers, sexual health should not go ignored and should be taken seriously, edical problems that women com only experience in regards to sexual health may include menopause, pain, birth trauma, difficulties after cancer, etc. Addressing these issues with a physician ay be the key to improving sexual wellness.



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Lake Charles Memorial **Health System**

Family Medicine pecialist Kira Groves, DO, will discuss some com on barriers to women's sexual health at this month's online seminar. Dr. Groves has a degree in biology and chemistry from Louisiana College in Pineville, Louisiana. She received her Doctor of steopathic edicine from William Carey University College of steopathic edicine in Hattiesburg, ississippi. Her postgraduate training includes a family medicine residency from Sampson egional edical Center in Clinton, orth Carolina. Dr. Groves is a member of the American Academy of Family hysicians, American College of Osteopathic Family hysicians and American Osteopathic Association.

Dr Groves is epting new p tients nd w lk-ins t 2750 Aster treet in L ke Ch ries For more inform tion or to s hedule in

For more information, please visit lcmh.com/seminar or II (337) 494-2935. ou can watch the event live online y visiting Icmh.com/watch or facebook.com/LakeCharlesMemorial.

NOTE: lcmh.com/watch will only e active while the seminar is taking place. A recording of the seminar will e available on our outube page following the live stream.

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The live meeting will be held using ZOOM on date and time

THURSDAY

March 25, 2021 5:30 p.m. to 7:00 p.m. Go to www.i10lakecharles.com for a ZOOM link

to attend the meeting
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> I-10 Calcasieu River Bridge Project c/o HNTB Corporation 10000 Perkins Rowe Baton Rouge, LA 70810

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ATTACHMENT B

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Emails Sent For Public Meetings Virtual Public Meeting for I-10 Calcasieu River Bridge Project in Calcasieu Parish

Count: 687

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Public Meetings Virtual Public Meeting for I-10 Calcasieu River Bridge Project in Calcasieu Parish

Thursday, March 11, 2021 3:11 PM

The Louisiana Department of Transportation and Development and the Federal Highway Administration will conduct a public meeting on the I-10 Calcasieu River Bridge project which extends from I-10/I-210 West to I-10/I-210 East, in Calcasieu Parish. The meeting will be **online using ZOOM on Thursday, March 25, 2021, 5:30 p.m. to 7:00 p.m.** Go to http://www.i10lakecharles.com for the ZOOM link to attend the meeting. If you do not have access to a computer or if you prefer, you may also join by phone at 646-876-9923, Passcode 459770 (audio only).

A live introduction will be followed by a recorded presentation. After the presentation, questions and comments will be accepted through the chat feature and the project team will respond in the order received. The presentation will be available for viewing on the project website, www.i10lakecharles.com, after the meeting. Comments and questions may be submitted through the website by clicking on the "Tell Us What You Think" bubble, through the project email at Calcasieubridge@hntb.com, by phone at 225-366-9645, or by mail at I-10 Calcasieu River Bridge Project, c/o HNTB Corporation, Suite 640, 10000 Perkins Rowe, Baton Rouge, LA 70810. All comments received at the meeting, through the website, by phone, email, or US mail by April 3, 2021, will become part of the meeting record.

If you require special assistance due to a disability or need an interpreter to attend the videoconference meeting, please contact us at Calcasieubridge@hntb.com or through one of the other channels described above at least 5 days in advance so that we can provide accommodations

The purpose of the public meeting is to solicit comments from the public regarding the alternatives and implementation strategies such as tolling and public-private partnerships.

Three alternatives (3A, 3E, & 5G) are being evaluated in the Environmental Impact Statement. Alternative 3A includes a long-span bridge, an extension of Sulphur Avenue east across the river via a moveable bridge, and a partial interchange at North Lakeshore. Similarly, Alternative 3E includes a long-span bridge and an extension of Sulphur Avenue east across the river to a full interchange at North Lakeshore. Alternative 5G includes a full interchange at Sampson Street elevated above the existing Sampson Street and two railroad crossings. This alternative would not extend Sulphur Avenue, but it would require moving two railroad spur tracks that cross the I-10 Service Road and Isle of Capri Boulevard. Common to all alternatives are improvements at PPG Drive, North Lakeshore and Veterans Memorial, Enterprise Boulevard, and the widening of I-10 to three through lanes in each direction.

The Louisiana Department of Transportation and Development (DOTD) is committed to delivering transportation and public works systems that enhance the quality of life. In addition to more than 16,600 miles of roadway, including over 890 miles of interstate, DOTD supports the development of the state's aviation, marine and rail infrastructures. Through this work, we are able to facilitate economic development, create job opportunities, improve vital evacuation routes, and make critical freight corridors safer and more efficient.

For more information, please visit www.dotd.la.gov, email <a href="https://dots.org/dots.or





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Virtual Public Meeting for I-10 Calcasieu River Bridge Project in Calcasieu Parish Thursday, March 11, 2021

The Louisiana Department of Transportation and Development and the Federal Highway Administration will conduct a public meeting on the I-10 Calcasieu River Bridge project which extends from I-10/I-210 West to I-10/I-210 East, in Calcasieu Parish. The meeting will be **online using ZOOM on Thursday, March 25, 2021, 5:30 p.m. to 7:00 p.m.** Go to https://www.i10lakecharles.com for the ZOOM link to attend the meeting. If you do not have access to a computer or if you prefer, you may also join by phone at 646-876-9923, Passcode 459770 (audio only).

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If you require special assistance due to a disability or need an interpreter to attend the videoconference meeting, please contact us at Calcasieubridge@hntb.com or through one of the other channels described above at least 5 days in advance so that we can provide accommodations

The purpose of the public meeting is to solicit comments from the public regarding the alternatives and implementation strategies such as tolling and public-private partnerships.

Three alternatives (3A, 3E, & 5G) are being evaluated in the Environmental Impact Statement. Alternative 3A includes a long-span bridge, an extension of Sulphur Avenue east across the river via a moveable bridge, and a partial interchange at North Lakeshore. Similarly, Alternative 3E includes a long-span bridge and an extension of Sulphur Avenue east across the river to a full interchange at North Lakeshore. Alternative 5G includes a full interchange at Sampson Street elevated above the existing Sampson Street and two railroad crossings. This alternative would not extend Sulphur Avenue, but it would require moving two railroad spur tracks that cross the I-10 Service Road and Isle of Capri Boulevard. Common to all alternatives are improvements at PPG Drive, North Lakeshore and Veterans Memorial, Enterprise Boulevard, and the widening of I-10 to three through lanes in each direction.



FOR IMMEDIATE RELEASE

March 22, 2021

News Contact: Tammy York <Tammy.York@la.gov>

I-10 Calcasieu Bridge Public Meeting Online via ZOOM

An online public meeting will be held using ZOOM on Thursday, March 25, 2021, from 5:30 p.m. to 7:00 p.m. for the I-10 Calcasieu River Bridge Project. To join the meeting, the public is invited to go to www.i10lakecharles.com and click on the ZOOM link to attend the meeting. If you do not have access to a computer or if you prefer, you may join the meeting by phone at 646-876-9923, Passcode 459770 (audio only).

The purpose of the public meeting is to solicit comments from the public about the alternatives and proposed improvements, benefits and impacts of each one, and implementation strategies such as tolling and public-private partnerships. A live introduction will be followed by a recorded presentation. After the presentation, questions and comments will be accepted through the chat feature and the project team will respond in the order received.

Three alternatives are being evaluated in the Environmental Impact Statement. Alternative 3A includes a long-span bridge, an extension of Sulphur Avenue east across the river via a moveable bridge, and a partial interchange at North Lakeshore.

Similarly, Alternative 3E includes a long-span bridge, and an extension of Sulphur Avenue east across the river to a full interchange at North Lakeshore.



Alternative 5G includes a full interchange at Sampson Street elevated above the existing Sampson Street and two railroad crossings. This alternative would not extend Sulphur Avenue, but it would require moving two railroad spur tracks that cross the I-10 Service Road and Isle of Capri Boulevard.

Common to all alternatives are improvements at PPG Drive, North Lakeshore and Veterans Memorial, Enterprise Boulevard, and the widening of I-10 to three through lanes in each direction.

Comments and questions may be submitted through the website by clicking on the "Tell Us What You Think" bubble, through the project email at Calcasieubridge@hntb.com, by phone at 225-366-9645, or by US mail at the below address. All comments received at the meeting, through the website, by phone, email, or US mail by April 3, 2021, will become part of the meeting record.

I-10 Calcasieu River Bridge Project c/o HNTB Corporation Suite 640 10000 Perkins Rowe Baton Rouge, LA 70810

If you are unable to attend the meeting at the scheduled time, you may view the recorded presentation and other informational materials by visiting the project website at www.i10lakecharles.com.

For special assistance, you can contact members of the project team or request hard copies of meeting materials at 225-366-9645 or CalcasieuBridge@hntb.com at least 5 days in advance so that we can provide accommodations.

ATTACHMENT D

PUBLIC MEETING #4 MEETING ATTENDEES LIST

State Project Number: H.003931



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March 25, 2021



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State Project Number: H.003931

March 25, 2021



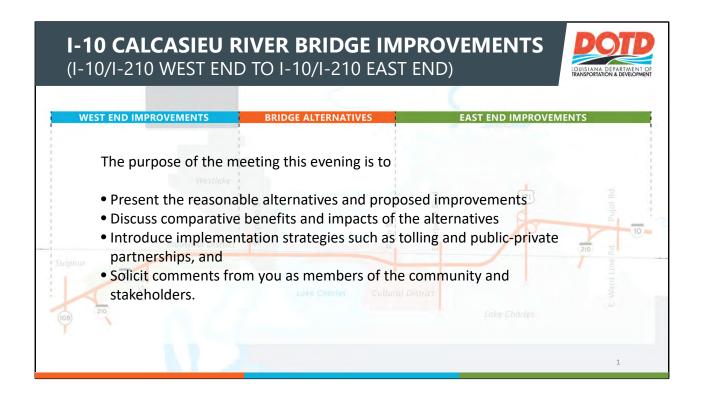
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ATTACHMENT E

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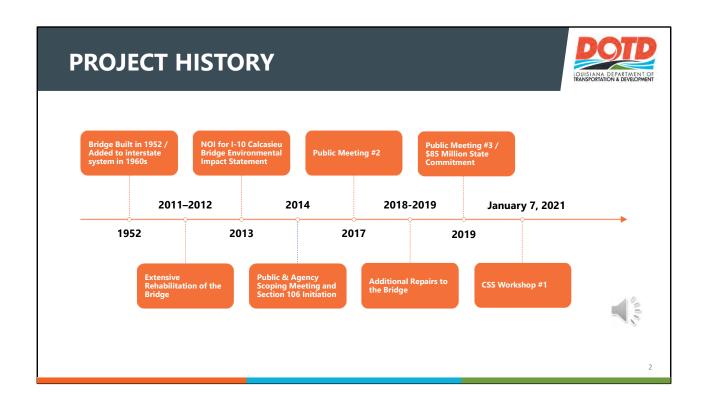


Welcome to the I-10 Calcasieu River Bridge and Improvements Project March 25, 2021 public meeting.

The project corridor extends from I-10 at I-210 West End to I-10 at I-210 East End, approximately 9 miles in length. It crosses through the City of Lake Charles and the City of Westlake, which are connected by the Calcasieu River Bridge.

The purpose of the meeting this evening is to

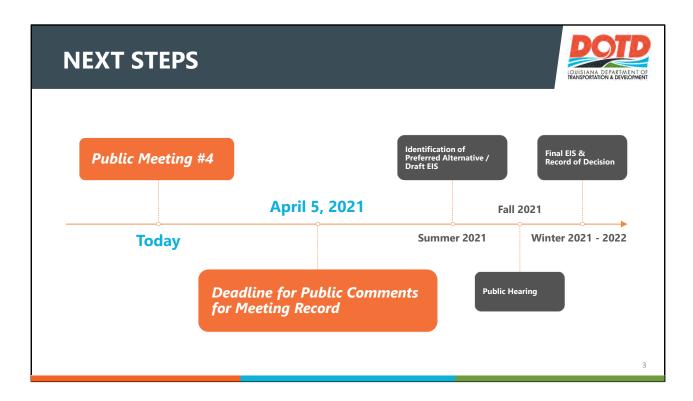
- Present the reasonable alternatives and proposed improvements
- Discuss comparative benefits and impacts of the alternatives
- Introduce implementation strategies such as tolling and public-private partnerships, and
- Solicit comments from you as members of the community and stakeholders.



The I-10 Calcasieu Bridge and Improvements Project Environmental Impact Statement (EIS) was initiated in 2013 (1) to address issues with the existing bridge, which is almost 70 years old. The bridge was rehabilitated in 2011-2012 (2) with additional repairs in 2018-2019 (3). This extended its service life, but it needs to be replaced. The remainder of I-10 within the project limits needs to be reconstructed as it is nearly 60 years old.

LADOTD has held three public meetings (4,5,6) and one workshop for the project to gather information about issues, concerns, and considerations. In 2019, the state allocated \$85 million towards construction of the bridge. (7)

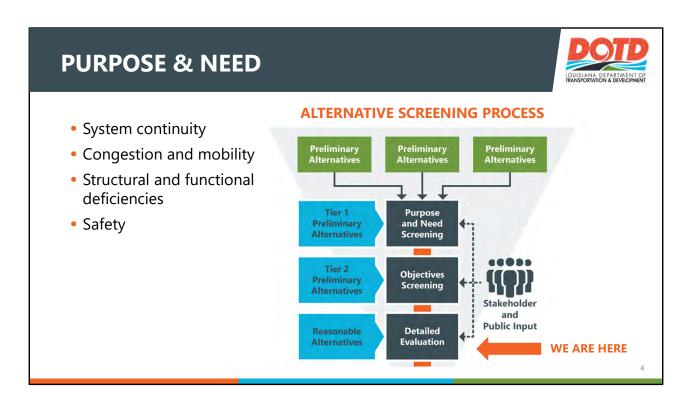
Most recently, LADOTD sponsored a Context Sensitive Solutions workshop (8) with local officials and other stakeholders to discuss local ideas about how the project could look and operate in a way that fits in with the adjacent communities. A summary of that meeting has been posted on the website for download and viewing at your convenience.



Today's meeting is Public Meeting #4, the last opportunity for the public to make comments before the draft Environmental Impact Statement is completed. A final meeting, known as a public hearing, will be held after the draft EIS is made available to the public and agencies for review.

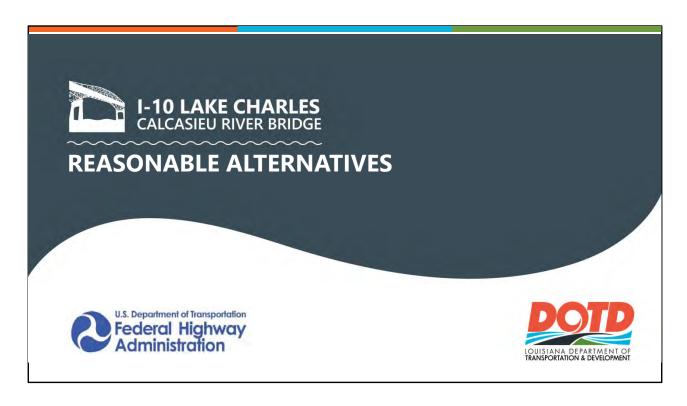
The draft EIS will identify a preferred alternative after considering comments from the project sponsors, agencies with regulatory and permitting authority, local officials, and the public.

As we present the alternatives being considered, please consider the advantages and disadvantages of each one. If you think one is better than the other, please let us know which one and why.

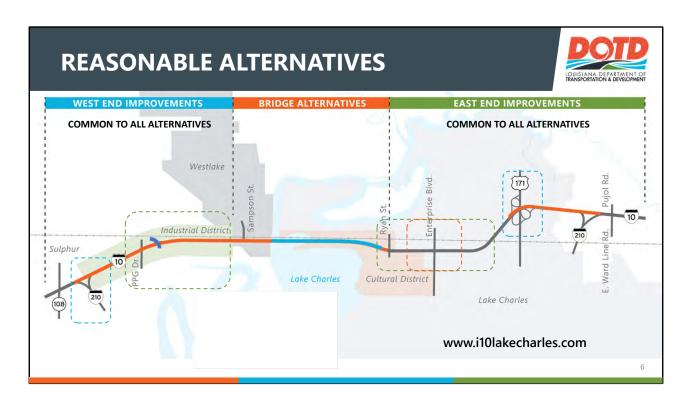


A complete range of alternatives was considered for the proposed project and put through a screening process. The three Alternatives that you will see this evening were brought forward for detailed evaluation in the EIS because they meet the purpose of the project and address the identified needs. Any reasonable alternative must

- Address the lack of system continuity on I-10
- Reduce congestion and improve mobility on I-10 and along Sampson Street
- Correct structural and functional roadway and bridge deficiencies
- Address safety concerns on I-10 including the Calcasieu River Bridge



This section will discuss the three reasonable alternatives that are being evaluated in the EIS.



All three reasonable alternatives would increase the number of through lanes on I-10 to three in each direction from I-210 west end to Ryan Street (1) and from US 171 to I-210 east end (2). This widening from 4 lanes to 6 lanes will take place no matter which alternative is selected. I-10 is already six lanes from Ryan Street to US 171.

Improvements proposed at the west end of the project corridor, from I-10 at I-210 west end to Sampson Street; and at the east end of the project corridor, from Ryan Street to I-10 at I-210 east end, are also common to all three alternatives. (3 and 4 together)

On the west end these common improvements include the I-10/I-210 interchange ramp (5) and I-10 frontage roads. The US 90 overpass bridge will be removed, and the highway will cross under a new I-10 overpass (6).

On the east end, the common improvements include concentrating access at the interchange at Enterprise (7), improving the overpass bridges and frontage roads from Ryan Street to Opelousas (8), and making changes to the interchange configuration at US 171 (9).

Changes at US 171 are necessary to accommodate the a six-lane interstate and ensure that the vertical clearance and geometry of the overpasses and ramps meet design criteria. The proposed reconfiguration does not address traffic issues or geometry on US 171 outside of

these boundaries.

Details of the improvements common to all alternatives can be viewed in the interactive maps provided online at www.i10lakecharles.com

What is not shown here but can be seen on those maps is that the mainline and bridge are shifted north of the existing alignment (highlight). This shift is needed to be able to maintain traffic during construction of the bridge. This shift will require that all of the businesses located on the north side of I-10 at North Lakeshore Drive be relocated.

PROPOSED IMPROVEMENTS – CALCASIEU BRIDGE



- Minimum Six Lanes (3 in each direction)
 - Alternative 5G provides two outside auxiliary lanes between Westlake and North Lakeshore
- Meet Design Criteria
 - Shoulders
 - Center Barrier
 - Eliminate Overhead Height Restriction
- Lower Bridge Height
 - Reduce Approach Grade from 5% to 3%
 - Provide 73 Feet Vertical Clearance for Maritime Traffic

The new bridge will be built to include six lanes, adequately sized shoulders for emergency use, and a central barrier built to meet design criteria. The overhead truss structure will be gone, eliminating the overhead height restriction.

The bridge height will be lowered, reducing the approach grades from 5% to 3%.

These improvements will be the same for all three build alternatives. Alternative 5G will include auxiliary lanes on the outside, allowing traffic between Westlake and North Lakeshore Boulevard to connect without entering mainline traffic.



Where the alternatives differ is in the center section that includes both the North Lakeshore area, the Calcasieu River Bridge and Sampson Street.

This area is where several engineering solutions have been considered. The designs shown are still preliminary but have been developed with enough detail to confirm that the geometry and structural elements in this middle section will address the identified needs and meet current design criteria.



The first reasonable alternative being considered is Alternative 3A. It makes no changes to existing Sampson Street.

The I-10 service roads on the south west(1) are shifted slightly south to improve curve radii, but the configuration of ramps remains the same.

The connecting intersection of the I-10 service road, Sampson Street and Isle of Capri also remains the same (2). The EDC area of contamination that was delineated in 2016 is shown on the map (3). Alternative 3A would cross this area with a long-span bridge.

What's new is a westbound off-ramp (arrow 3 (4)) that has been added to complete the interchange. This ramp departs from the new bridge as it passes over Miller Avenue.

Then, it turns south, crosses beneath the new bridge and comes to ground at the intersection of Isle of Capri Boulevard and Mike Hooks Road, which will be straightened to join Isle of Capri at a 90-degree angle (arrow 4 (5)).

Sampson Street will be unimproved and remain at grade. Traffic on Sulphur Avenue and Sampson Street north will be able to avoid two mainline railroad crossings of Sampson Street by using an extension of Sulphur Avenue.

A new movable bridge will be built over the river connecting to an elevated access road ending at a partial interchange at North Lakeshore in Lake Charles. The interchange is called partial because it provides only an eastbound on-ramp and a westbound off-ramp to and from I-10.

Access in the area using North Lakeshore and other roads will remain unchanged except they will shift to the north with the mainline.



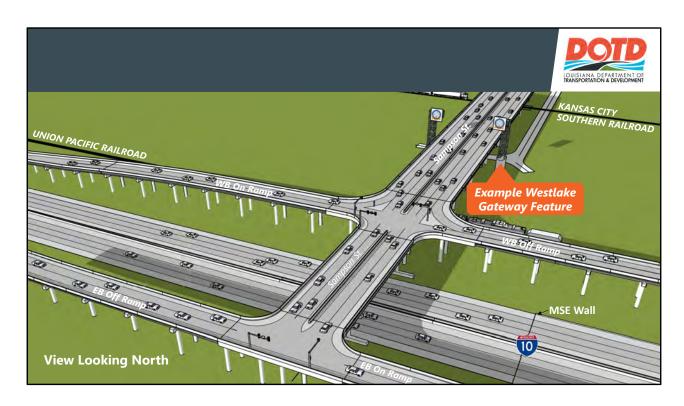
Alternative 3E differs from Alternative 3A only in that the interchange at North Lakeshore is complete (1). That is, drivers can enter or exit the interstate from all directions by way of the Sulphur Avenue extension. But they may also use I-10 to cross the river entering or exiting at Sampson Street. All other operational and access features, including the long-span crossing of the EDC (2), are the same as 3A.



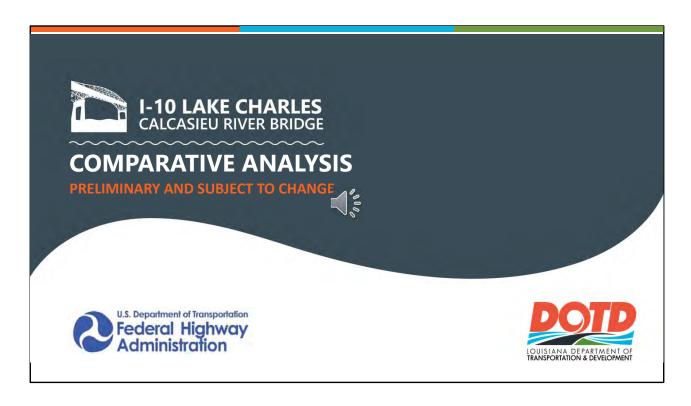
Alternative 5G was designed to provide a compact and complete interchange at Sampson Street. It does not require an extension of Sulphur Avenue because Sampson Street will be raised to connect to the bridge above grade, eliminating all conflicts with the railroad crossings on Sampson Street.

At its western end, the bridge will pass under Sampson Street, crossing the EDC area on a retaining wall. The retaining wall will be filled with light weight earthen material and supported with a shallow platform designed to spread the load horizontally.

Sampson Street at ground level will be demolished. Access to the I-10 Service Road and Isle of Capri will be maintained in Alternative 5G.



This is a sketch of what the new Sampson Street interchange would look like if Alternative 5G were built. Sampson Street at I-10 would not change from its existing condition for Alternatives 3A and 3E.



The three reasonable alternatives are being studied to determine what the comparative advantages and disadvantages of each would be. The analysis for each alternative has been conducted at the same level of detail as a basis for this initial comparison. Technical analyses for the alternatives are ongoing. Therefore, the information presented here is preliminary and may change as the analyses are completed.

COMPARATIVE IMPACT ANALYSIS – PANEL 1



PRELIMINARY AND SUBJECT TO CHANGE

Resource/Issue	Criterion	No Build Alternative	Alternative 3A	Alternative 3E	Alternative 5G
Section 4 (f) / 6(f)	Number of Sites Impacted	0	3	3	3
Cultural Resources (Section 106)	Number of Historic Sites Impacted	0	2	2	2
	Number of Archeological Sites Impacted	0	1	1	1
Habitat Impacts	Federally Protected T&E Species	None	None	None	None
	Bald Eagle Nest	No Impact	Within 650 feet	Within 10 feet	No Impact
Wetlands (Section 404)	Number of Acres Impacted	0	41.4	48.4	36.9

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What you see here is a comparison of impacts among the alternatives. Although the No-Build Alternative may appear to be best case in some instances, this alternative would not meet the purpose and need for the project. The No-Build information, which is an analysis of the condition in the future if nothing is done, is provided as a baseline or benchmark for comparison purposes.

A red highlighted result identifies a comparatively substantive impact on the resource or issue listed. No highlights in a row indicates that there is no substantive difference among the alternatives.

Panel 1 compares resources required to be considered by federal law. Section 4(f) and Section 6(f) refers to the sections of the laws that protect recreational, historic, and cultural resources along with properties set aside by the Land and Water Conservation Fund.

Cultural resources are evaluated pursuant to Section 106 of the National Historic Preservation Act. Three Section 106 resources are potentially impacted by the project: the existing Calcasieu River bridge, the US 90 overpass bridge near PPG Drive, and an archaeological site.

The archaeological site may or may not be eligible for listing on the national register of

historic places

Section 106 consulting parties have been engaged and that consultation will continue through the EIS process.

For comparison purposes, there is no difference among the reasonable alternatives related to these cultural resources.

For natural resource impacts, federally protected species such as those listed as threatened or endangered, migratory birds, and bald eagles were evaluated along with other natural resources such as habitats and wetlands.

The Sulphur Avenue extension for Alternatives 3A and 3E crosses through forested wetlands on the east bank of the river. An active bald eagle nest has been identified near the alignments for the extension. Alternative 5G does not include an extension and would not impact the nest. Similarly, because the Sulphur Avenue extension of 3A and 3E crosses the swamp, these alternatives would impact more wetlands than Alternative 5G.

COMPARATIVE IMPACT ANALYSIS – PANEL 2 PRELIMINARY AND SUBJECT TO CHANGE No Build Resource/Issue Alternative 3A Alternative 3E Alternative 5G Criterion **Alternative** RR Opt 1 RR Opt 2 Westlake between KCS No Change No Change No Change No Change (Pilley Street) and UPRR Community (Railroad Avenue) Residential Neighborhood Westlake Sulphur Avenue Change Land Use Change Land Use No Change No Change Impacts Sampson to River to Commercial to Commercial Viaduct from Ryan to Reconnect Street Reconnect Street No Change Reconnect Street Grid Opelousas Roadway More Roadway More View By Lakefront Users No Change No Change Visual Effects View of Westlake Gateway No Change No Change No Change Elevated above Industrial Clutter by Drivers

In this panel, several issues of concern to local communities are evaluated. The panel shows that there are two railroad spur relocation options being studied for Alternative 5G. Railroad Option 1 and Railroad Option 2 would cause different types of impacts and are listed in separate columns. Option 1 would align the existing spur track through the Westlake neighborhood located between the two railroad lines. Option 2 would move the spur tracks closer to the river, impacting only commercial buildings and have no effect on the residential neighborhood.

Alternatives 3A and 3E would widen Sulphur Avenue to four lanes and extend it across the river. The number of cars using this route will increase, and over time this traffic will likely induce a change in land use from primarily residential to commercial.

Visual intrusion is a potential issue for North Lakeshore because the interchanges needed for Alternatives 3A and 3E will be more prominent as viewed from the beach and lakefront when compared with Alternative 5G, which does not include an interchange.

In Westlake, Alternative 5G would elevate Sampson Street. Drivers would view the Westlake gateway feature without the visual clutter of surrounding industrial infrastructure.

COMPARATIVE IMPACT ANALYSIS – PANEL 3



PRELIMINARY AND SUBJECT TO CHANGE

Resource/Issue	Criterion	No Build Alternative	Alternative 3A	Alternative 3E	Alternative 5G
Safety	Number of Crashes 2042	428	418	446	424
	Number of At-Grade RR Crossings	3	3	3	1
Phase I ESA	No. of Confirmed RECs within/adjacent to the ROW	19	37	37	38
Cost Estimates	Construction (\$ Million 2020)	0	\$1,121	\$1,200	\$947

16

A safety analysis was performed using a model that predicts the number of crashes in the future. It was predicted that Alternative 3E would experience more crashes because the full interchange North Lakeshore would increase the number of conflict points between cars. However, the crash numbers are so close that they don't really predict which alternative would be safer. The most substantive safety benefit comes from Alternative 5G, which would eliminate the at-grade crossings of the mainline railroad tracks on Sampson Street.

When looking at properties classified as having experienced some kind of "recognized environmental condition" or REC, the numbers are relatively the same.

The cost estimates show Alternative 5G to be approximately \$170 to 250 million less expensive to build than the other two alternatives. This cost is estimated for construction only and does not include other costs such as right of way, utilities, final design, administration, and relocations.

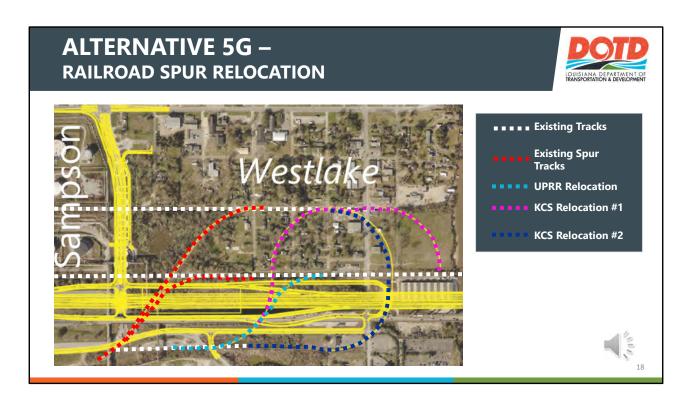
COMPARATIVE IMPACT ANALYSIS – PANEL 4 No Build Alternative 3A **Alternative 3E** Alternative 5G Resource/Issue Criterion **Alternative** RR Opt 1 RR Opt 2 Residential Relocations 0 14 14 12 0 12 **Business Relocations** 11 11 Relocations Total Number 0 25 27 25 Relocations RR Opt 1 RR Opt 2 Additional Acres to be 87.54 Right of Way 0 82.54 71.02 70.69 Acquired RR Opt 1 RR Opt 2 Number of Impacted Traffic Noise 379 407 386 Receptors Number of Known Navigation 0 2 2 Operations 17

This panel compares the number of residential and business relocations that would be required by each alternative. As shown, Alternative 5G would cause the most relocations. Railroad option 1 would relocate the most residences and Railroad option 2 would relocate the most businesses. We will discuss the railroad spur relocation in detail later in the presentation.

Alternative 3E would require the highest number of acres of unoccupied land for right of way and Alternative 5G would require the least.

A preliminary traffic noise study has been performed that predicts that traffic noise from Alternative 3A would potentially impact the most receptors.

All of the alternatives would impact two known operations on the riverfront in North Lake Charles by reducing the vertical clearance for navigation.



As shown here, Alternative 5G will require the existing railroad spur tracks, shown in red (1), to be relocated to the east. The existing tracks, shown in red, are in the path of the western bridge approach that will touch ground below Sampson Street.

UPRR is south of

KCS and

will be shifted to the east to the alignment shown in turquoise (2).

Two options for moving the KCS spur track are shown here (3).

The pink alignment mimics the current movement of the train, which moves east until it is far enough along to reverse direction and back down the spur. (Pulse)

The second option would allow KCS to head straight for its destination (Pulse)

by following the dark blue alignment in a wide arc.

Both UPRR and KCS would enter the yard on a refurbished existing track to the south



The traffic analysis is not complete, but preliminary results indicate that traffic operations will be improved by all three alternatives.

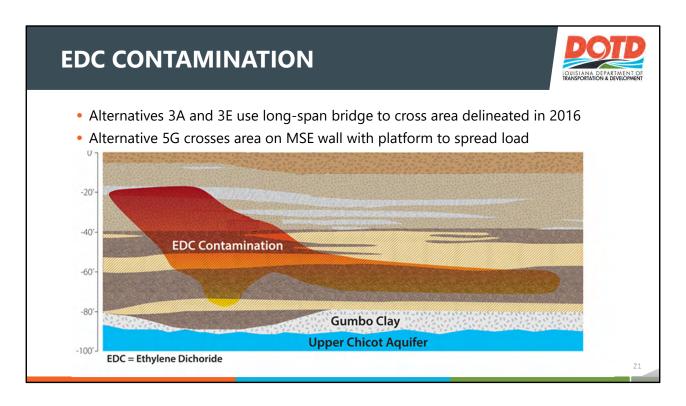
Alternative 5G will eliminate the disruptions to traffic related to trains blocking Sampson Street.



An important disadvantage of Alternative 5G is that it would require Sampson Street to be closed. For the first 18 months, when the new bridge is being built, Sampson Street would be open to traffic. But during construction of the elevated interchange, which should take about 12 months, Sampson Street from Sulphur to I-10 will be completely closed.

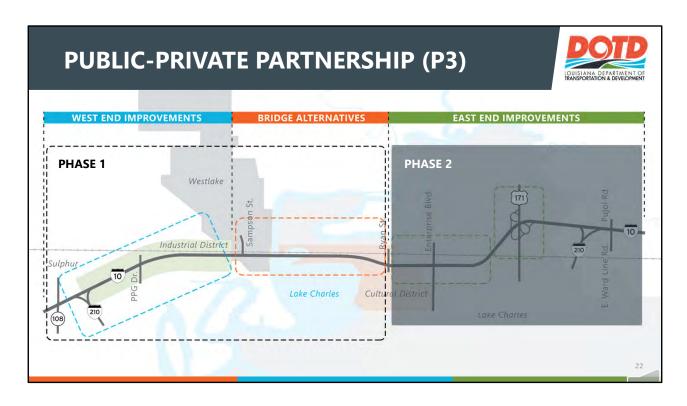
This closure would not be necessary for Alternative 3A or 3E.

This issue was discussed with local officials and the community in 2019 and all agreed that this disadvantage should not cause Alternative 5G to be dismissed.



Because Alternatives 3A and 3E would use a long-span bridge to cross over the area of contamination delineated in 2016, it may be less risky than Alternative 5G which would build the bridge approach on a platform that would spread the load over the shallow surface.

The level of uncertainty related to construction of any kind in this area is unknown and LADOTD is working on collecting more data to help understand the comparative risks.



The full extent of the I-10 Calcasieu Bridge and Improvements project is a \$1-billion project.

project in phases. The first phase includes the west end improvements and bridge replacement.

Phasing in this manner makes the project more attractive to outside financing through a public-private partnership (P3), where a private firm will complete the design, construct, and operate Phase 1 of the project . The Notice of Intent for the P3 was issued on January 4, 2021 and is moving forward concurrently with completion of the EIS so that the funding will be in place as soon as the environmental review is approved, and the necessary permits are secured.

TOLLING - TRAFFIC AND REVENUE



- Approximately 25% of the traffic on I-10 and the bridge would be diverted to I-210.
- A nominal toll rate of \$1-3 would provide substantial revenue that makes tolling a feasible option for funding of the bridge.
- Financial feasibility will be refined by the proposers for the Public Private Partnership (P3)
- No matter which alternative is selected, all crossings of the Calcasieu River (except I-210) will be tolled.

1,3

23

LADOTD also investigated the use of tolls to fund construction and operation of the proposed project for a period of 30 to 45 years. The P3 proposers will manage tolling operations and use the revenue to offset their costs.

Traffic and revenue studies show that approximately 25% of the traffic on I-10 and the bridge will divert to I-210 to avoid paying the toll. However, despite the diversion, the preliminary analysis showed that tolling provides a substantial revenue even when the toll is set at a nominal rate of \$1 to \$3.

The financial feasibility of tolling is still being evaluated, but it has been determined that tolling is the best way to fill the funding gap and build the bridge now.

The financial feasibility and exact numbers will be refined once the P3 team is selected.

All crossings of the Calcasieu River would be tolled, no matter which alternative is selected. The only toll-free route will be I-210 and the much longer detour to the north through Moss Bluff.

PERMITS, MITIGATION, COMMITMENTS



- Section 106 Historic Bridge Programmatic Agreement / Bridge Marketing
- USCG Bridge Permit
 - Mitigation for Impacts to Navigation / Business Operations
- Section 404/10
 - Mitigation for Impacts to Wetlands
- Interchange Modification or Justification Report
 - FHWA Approval
- Railroad Spur Relocation
 - FRA Cooperation
- Relocation and Right of Way Acquisition
 - Businesses and Residences

24

Regulatory permits are required to construct the project. Unavoidable impacts will be addressed through mitigation.

Replacement of the historic bridges must be managed per agreements with the State Historic Preservation Office and a permit from the US Coast Guard is required for the Calcasieu River bridge. Wetlands are protected by the US Army Corps of Engineers who prescribed appropriate mitigation measures to make sure that the wetlands loss in the project area will be offset by wetlands elsewhere. Changes in access along the interstate will be reviewed and approved by the Federal Highway Administration. Changes in the spur tracks will be negotiated with the individual railroads in cooperation with the Federal Railroad Administration.

And relocation and right of way acquisition procedures are governed by both federal law and state policy to ensure that any taking of real estate is fairly compensated and any occupant of a structure whether a resident or a business will be relocated to a comparable situation. A copy of the Right of Way Acquisition and Relocation policy is available in the document library on the i10lakecharles.com website or from LADOTD headquarters.

MORE OPPORTUNITIES FOR INPUT



- Context Sensitive Solutions Workshop #2 Summer
- Public Hearing Fall
- www.i10lakecharles.com "Tell Us What You Think"
- 225-366-9645 Leave a message
- CalcasieuBridge@hntb.com



25

Two more opportunities for public input will be made available this year.

Discussion among LADOTD, local entities, and public authorities about signature features for the bridge, and community enhancements such as sidewalks and bike paths, have been initiated. You may download a report about the first Context Sensitive Solution workshop from the project website to see what has been done to date. A second CSS workshop will be held this summer to continue the conversation.

Once the draft EIS has been made available for review by the public and agencies, a public hearing will be held. A decision about the format of the hearing will be made once we know more about the COVID situation.

Please check the website or contact us for more information about these opportunities for input.



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Thank you for your attention.

We will now turn the meeting over to the project team members who are ready to accept your comments and questions via the chat box.

ATTACHMENT G



Project Phone: 225-366-9645
Email: CalcasieuBridge@hntb.com
Online: www.i10lakecharles.com

Frequently Asked Questions

Which alternative will be selected for construction?

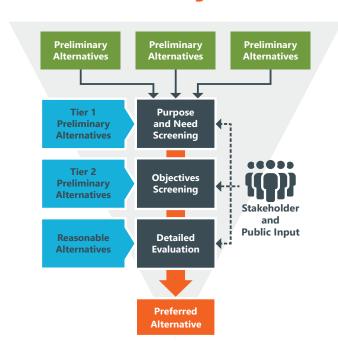
The draft EIS will present the alternative screening process and the detailed evaluation of three reasonable alternatives. This information will be presented to the public and agencies. Based on their input, one alternative will be selected as preferred for construction. The reasonable alternatives are shown on the inside.

Visit the project website at www.i10lakecharles.com to see zoomable maps of the reasonable alternatives, find more information about the project, and make comments. You may also make comments by sending us an email or calling us at 225-366-9645.

How long will it take to finish the EIS?

The project timeline shown on the front page illustrates the latest schedule.

Alternative Screening Process



Join us online at www.ilOlakecharles.com and click on the link to provide comments. Sign-up for alerts about future public participation events and other updates.

Tell Us What You Think

1-10 CALCASIEU RIVER BRIDGE

BATON ROUGE, LA 70810

BATON ROUGE, LA 70810



VISIT OUR RECENTLY UPDATED WEBSITE! • www.i10lakecharles.com



DECEMBER 2020

PROJECT UPDATES

The I-10 Calcasieu Bridge & Improvements Project is proposed to improve Interstate 10 (I-10) between the I-10/I-210 west and I-10/I-210 east interchanges in Calcasieu Parish, LA. The project corridor includes the Calcasieu River Bridge that connects the cities of Lake Charles and Westlake. The project length is approximately 9 miles and includes the interstate roadways, the bridge approaches, the I-10 frontage roads, and several interchanges that connect the interstate to state and local roads.

Three alternatives (see inside) have been identified as reasonable and will be studied in detail in the Environmental Impact Statement (EIS) being prepared for public and agency review in 2021.

Public Meeting #4 will be held in early 2021. The public meeting is proposed as a virtual meeting presented online. Please contact us by email or phone to let us know if you would like to attend and we will send you a link to the online meeting and instructions on how to use the meeting technology.

A Virtual Public Meeting will also be made available for viewing at your convenience on our website at www.i10lakecharles.com for a period of two weeks before and after the online meeting.



Calcasieu River Bridge

If you do not have access to the internet, a computer, and audio capabilities, or if you have a disability that requires special accommodations, please contact us at the project phone or email and we provide an alternative way for you to participate.

Project Phone: 225-366-9645
Email: CalcasieuBridge@hntb.com
Online: www.i10lakecharles.com

Timeline

Environmental Impact Statement





Project Phone: 225-366-9645 **Email:** CalcasieuBridge@hntb.com **Online:** www.i10lakecharles.com

Alternatives

Improvements at the west end of the corridor from I-210 to west of Sampson Street would be the same for all three alternatives. Improvements at the east end of the corridor from Ryan Street to I-210 would also be the same for all three alternatives. For purposes of evaluation, these improvements will be compared to the existing condition which is also known as the No-Build Alternative.

The area in the center of the corridor — from the point where the on- and off-ramps for Sampson Street begin to Ryan Street — is where the alternatives differ. These differences are being evaluated to determine which alternative in combination with the improvements at each end of the corridor will be selected for construction.

Tell Us What You Think

Join us online at www.i10lakecharles.com and click on the link to provide comments. Sign-up for alerts about future public participation events and other updates.

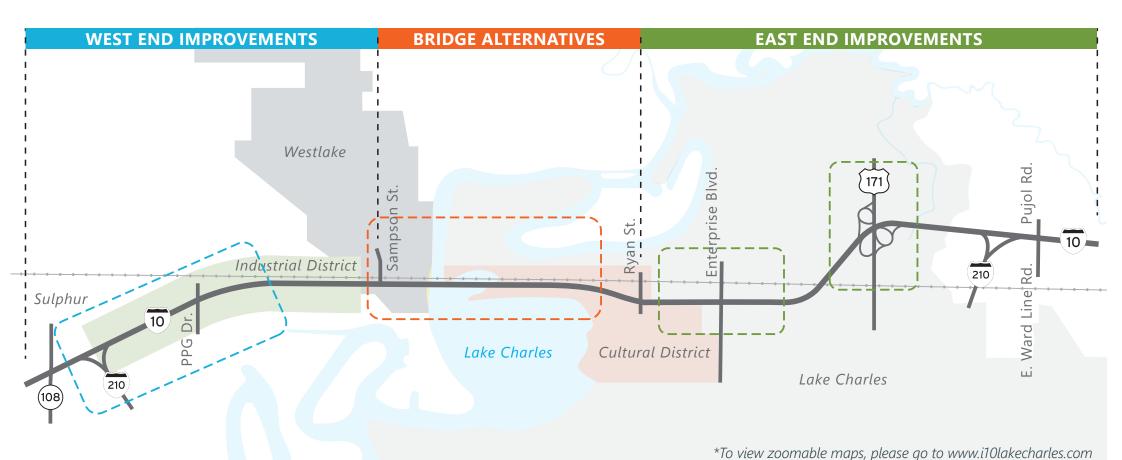
About the Bridge Height

The vertical clearance of the bridge will be 73 feet. An update to the navigational study is being prepared. Anyone with an interest in navigation in the Calcasieu River is encouraged to respond to a survey by using a

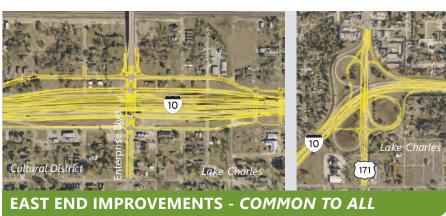


smart phone camera to scan the bar code, or visit https://s.surveyplanet.com/n1T1a3tnX.

You can also access the survey on our website You can also access the survey on our website at www.i10lakecharles.com.















ATTACHMENT H

H.003931 I-10 Calcasieu Bridge and Improvements Website Survey

- 1. Put a pin on the general location of where you live. (If you do not live in the area, skip this question.)
- 2. Put a pin on the general location of where you work. (If you do not work in the area, skip this question.)
- 3. Put a pin on properties other than your home or workplace that you own or rent. (If you do not own or rent any other properties, skip this question.)
- 4. On average, how many days per week do you drive on I-10 in the project study area?
 - a. 0 days per week
 - b. 1-2 days per week
 - c. 3-5 days per week
 - d. 6-7 days per week
- 5. On average, how many days per week do you drive on Sampson Street between I-10 and Sulphur Avenue?
 - a. 0 days per week
 - b. 1-2 days per week
 - c. 3-5 days per week
 - d. 6-7 days per week
- 6. On average, how many days per week do you drive on I-10 over the Calcasieu River Bridge?
 - a. 0 days per week
 - b. 1-2 days per week
 - c. 3-5 days per week
 - d. 6-7 days per week
- 7. What are your major concerns about driving in the study area? (Choose all that apply.)
 - a. Traffic congestion
 - b. Unpredictable slowdowns or stoppages
 - c. Entering and exiting the interstate
 - d. Steep grades
 - e. Sharp curves
 - f. Reduced speeds
 - g. Trains
 - h. Trucks
 - i. Other drivers
 - j. Other factors
 - k. I am not stressed while driving in the study area
- 8. What are your biggest concerns about the proposed project? (Choose all that apply.)
 - a. That it will never be completed
 - b. That it will cost too much
 - c. That there is no way to pay for it
 - d. That it will impact navigation
 - e. That EDC contamination will cause problems
 - f. That the bridge(s) will be tolled
 - g. That it will take my property

- h. I do not have any concerns
- i. Other concern
- 9. When the bridges are tolled, you will (Choose all that apply):
 - a. Take I-210 as an alternate route across the river
 - b. Take US 171 and LA 378 as an alternate route across the river
 - c. Carpool
 - d. Buy a toll tag or toll pass for frequent user discount
 - e. Limit the number of trips across the river
 - f. Never drive across the bridge again
 - g. Other
- 10. Which alternative do you prefer?
 - a. I do not have a preference
 - b. Alternative 3A
 - c. Alternative 3E
 - d. Alternative 5G
 - e. No Build
- 11. Overall, how do you feel about the project? (Use slider to indicate level of favorability).

 Not at all favorable (1)

 Extremely favorable (5)
- 12. We encourage and welcome your other comments:

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
1	David Balmos	2021-03-25	can you show detailed layouts showing how the railroad would be relocated in alternative 5G?	Public Meeting #4 Q&A Box	Details of Alternative 5G can be seen within the recorded presentation at minute 10:00. You may also view interactive maps at www.i10lakecharles.com.
3	Concernd Citizen Anonymous Attendee	2021-03-25	Which option has the least impact to the drinking water?	Public Meeting #4 Q&A Box Public Meeting #4 Q&A Box	All alternatives were carefully developed with consideration of the Chico Aquifer and the drinking water supply. Alternatives 3A and 3E pose the least risk as they avoid the known EDC contaminated area.
				-	
4	Robert Schmidt	2021-03-25	Will the new movable bridge be tolled?	Public Meeting #4 Q&A Box	All crossings of the Calcasieu River will be tolled. I-210 will remain free of tolls.
5	jen		 How will the highway department prevent "thru" traffic on 210? Will there be a local discount for residents on the toll? How is placing a toll on a federal highway legal? This is a publicly funded interstate system. 	Public Meeting #4 Q&A Box	There is no legal way to prohibit 'thru' traffic from using Interstate 210. It is a public facility open to all traffic. Drivers with a toll tag will be charged at a lower rate than drivers without a toll tag. Other discounts that could affect local drivers could be considered by the P3 developers. Under federal law, it is legal to toll an interstate facility if the proceeds are used to replace an interstate bridge.
6	Anonymous Attendee	2021-03-25	Will there be a differnce in toll charges for cars vs trucks?	Public Meeting #4 Q&A Box	Toll facilities around the country typically charge a higher rate for trucks than for cars. The rate schedule for the bridge will be determined in coordination with the P3 developers.
7	Michael Tritico		Comment for the DOTD online public ZOOM meeting to be held March 25, 2021 Submitted by Michael Tritico, P.O. Box 233, Longville, LA 70652 The most sensible alternative is not presented and may not have been considered. That alternative would add about 5 miles to the trip between Welsh and Vinton but it would completely bypass the urban/I-10 Bridge bottleneck that exists from Chloe to West Sulphur. Here is a description of a practical flat route that would make it unnecessary to build a new bridge in the EDC-contaminated zone: The "North Bypass" would take off to from just west of Welsh, go northwest about 9 miles to a cloverleaf at U.S. Highway 165 south of Fenton, then turn west, go along the existing high ground to avoid as much wetland disruption as possible, cross with only slightly-elevated bridges Bayou Serpent and the Calcasieu River, (a distance for this segment of about 11 miles), then proceed another 6 miles to a cloverleaf at U.S. Highway 171 south of Gillis, proceed westward another 5 mil [comment overflowed Q&A Box]	Public Meeting #4 Q&A Box	Early in the process, the Project Team considered alternative realignments, but they were extremely expensive. A far northern bypass may be be considered for the area in the long-term, but it would only serve through traffic and the surrounding small towns and villages. The I-10 Calcasieu Bridge project must deal with an aging bridge that needs to be replaced and the aging interstate that serves Lake Charles, Westlake, and Sulphur residents and businesses.
8	David Balmos	2021-03-25	as a follow up, what is the purpose of the railroad relocation?	Public Meeting #4 Q&A Box	The railroad relocation is only necessary for the 5G alternative because the bridge at its west end will approach ground level over the existing railroad spurs. The resulting vertical clearance will not be sufficient for the trains to pass.
9	Anonymous Attendee		For property owners who have been "right-of-way'd" in no change/alternative zones, please make clear when the process of acquiring properties will take place in process or timeline (of course all things considering the project is a go) and GREAT job to everyone especially DR. Shawn Wilson for making this happen!	Public Meeting #4 Q&A Box	It is too early in the project to determine a timeline for property acquisition. Before the right of way acquisition process can begin, we must complete the environmental review process, initiate final design, conduct property appraisals, and make contact with property owners. A brochure about the policies and process are available in the Project Library on the website at www.i10lakecharles.com.
10	Anonymous Attendee	2021-03-25	You said the \$1 billion excludes ROW, Design and utilities relocations and other things I missedhow much would these add to the project?	Public Meeting #4 Q&A Box	It is estimated that the costs of right of way, design, and utility relocations will add 10% - 15% to the overall project cost. Relocation costs of businesses and residences will be determined.
11	Anonymous Attendee	2021-03-25	which alternative the project team is recommending as a preferred alternative?	Public Meeting #4 Q&A Box	The Project Team does not have a preferred alternative at this point. We are going to consider public comments and other factors before making a recommendation.
12	Byron Martin Anonymous		If a new movable bridge is constructed at Sulphur Ave what would the elevation be? Will typical recreational boaters be able to cross under without opening the bridge? is any further subsurface environmental sampling planned? If so, why has it taken so long to complete this	Public Meeting #4 Q&A Box	The type of movable bridge has not been determined. In the open position, the movable bridge would provide 73 feet at a minimum (vertical lift bridge) up to unlimited clearance (swing span or drawbridge). A navigation study is being prepared. Impacts to navigation will be provided in that document, which will be posted online.
13	Attendee	2021-03-25	phase? thanks.	Public Meeting #4 Q&A Box	Yes, more sampling is planned. DOTD is currently in contracting for the investigation.
14	Stafford Frrank	2021-03-25	Was a northern route similar to the southern route for I210 considered	Public Meeting #4 Q&A Box	Early in the process, the Project Team considered alternative realignments, but they were extremely expensive. A far northern bypass may be be considered for the area in the long-term, but it would only serve through traffic and the surrounding small towns and villages. The I-10 Calcasieu Bridge project must deal with an aging bridge that needs to be replaced and the aging interstate that serves Lake Charles, Westlake, and Sulphur residents and businesses.
15	Jade Rung	2021-03-25	What is the required height of the proposed new bridge(s)?	Public Meeting #4 Q&A Box	The fixed span for the I-10 bridge will provide 73 feet of vertical clearance. The Sulphur Avenue Extension Bridge movable bridge type has not been determined. A navigation study is being prepared. Impacts to navigation will be provided in that document, which will be posted online.
16	Anonymous Attendee		How many Bald Eagles are in the nesting area?	Public Meeting #4 Q&A Box	There is a nesting pair and at least one fledgling
	Anonymous Attendee		What is approximate span length required for long span bridge over EDC Area for 3A and 3C?	Public Meeting #4 Q&A Box	Based on available information, it is estimated that the span length over the EDC area for Alternatives 3A and 3E would be 800 – 900 feet.
18	Linda Lebert	2021-03-25	what noise abatements will be made for residents at foot of movable bridge?	Public Meeting #4 Q&A Box	A traffic noise analysis is being prepared. The results of the noise study will be published in the environmental document, and the public will be given an opportunity to comment on proposed noise abatement measures at the public hearing.

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
19	kristy armand	2021-03-25	is the idea of including a pedestrian walkway and bike path part of the plan? This is very important for our community and connectivity of the SWLA region.	Public Meeting #4 Q&A Box	Louisiana Revised Statute 32:263 spedifically prohibits the use of any Louisiana Interstate highway by pedestrian, bicycles, or non-motorized vehicles. Bicycle and pedestrian accommodations may be possible on Alternatives 3A and 3E along the proposed Sulphur Avenue extension including the moveable bridge although a reasonable connection to Ryan Street presents some challenges. Even without a legal prohibition, a bicycle/pedestrian path attached to a new I-10 bridge is not practical. The path would have approximate one-half mile approaches on each side at a three percent grade to reach the crest. It would have to be separated from the traffic lanes by a concrete barrier and it would have to be at least 14 feet wide to accommodate maintenance vehicles, police vehicles, and EMS vehicles. The facility would have to be ADA accessible probably through the use elevators on each side of the river in combination with a Igazag path or some other means for citizens with disabilities to reach the crest. Due to the length of the approaches (approximately one-half mile), it is likely that skateboarders, those with rollerblades, some cyclists, etc. will exceed safe speeds when descending from the crest potentially resulting in serious injury crashes. Security, particularly at night, will be an issue. The facility will need frequent trash pickup and periodic deaning. A bicycle/pedestrian path attached to a new I-10 bridge would be expensive to build and very expensive to operate. These costs would have to be reflected in the toll rates and at the end of the public private partnership, responsibility for the operating and maintenance costs, including security, would transfer to the cities of Lake Charles and West Lake as DOTD does not maintain or operate multiuse paths.
				The second second	All of the toll facilities will be electronic and will not require toll booths where drivers have to stop and pay. A gantry will be placed over the roadway and equipped with a toll tag reader and with a camera that will photograph the license plate of vehicles without toll tags
20	Rusty Vincent	2021-03-25	How can we be assured that a toll based bridge will not slow down traffic and cause traffic backups	Public Meeting #4 Q&A Box	so they can be billed by mail.
21	James Cormier	2021-03-25	5G seems too leave more of Westlake intact - ditizens are talking about the old town area being revitalized - and takes river frontage from public use	Public Meeting #4 Q&A Box	Thank you for your comment. Your preference has been noted. Coordination with Westlake officials, businesses, and residences is ongoing.
22	Anonymous Attendee	2021-03-25	ts there one toll location and where will it be located.	Public Meeting #4 Q&A Box	Only the bridge crossing(s) will be tolled. There will be no toll booths. An automated toll collection gantry to collect tolls will be located near the bridge or bridges, depending upon the alternative that is built. The location of the gantry or gantries will be determined during design.
23	Anonymous Attendee	2021-03-25	Have we begun to look into utilizing funds from the American Rescue Plan for infrastructure projects such as the I-10 Calcasieu Bridge?	Fublic Meeting #4 Q&A Box	Yes, the Project Team has explored the option by requesting formal interpretation of the bill and is currently awaiting a response.
24	Anonymous Attendee	2021-03-25	Would interstate trucks be barred from using the I-210 bypass? If not then they would avoid tolls	Public Meeting #4 Q&A Box	There is no legal way to prohibit trucks from using I-210. LADOTD does not have any regulatory authority to do so.
25	john pohorelsky	2021-03-25	I support Alternate 5G oldes expensive of the existing RR of the existing RR owill not impact the waterways and wetlands as would other aits. The replacement of the bridge is critical to the future of this area. The I-10 Calcasieu River Bridge task force represents the business community, has been charged with the obligation to make sure the bridge responds to the desires of the community and expects to remain involved in the design process. A P3 with tolling is a proven solution for infrastructure projects and has been used across the country. Would prefer no toll but will support a reasonable toll if it is necessary to close a funding gap withink-residents should withhold judgement on the use of a P3 until the bids are received and more specific information is available. The I-210 Bridge has been refurbished and offers a free route for those not wanting to pay a toll.	Public Meeting #4 Q&A Box	Thank you for your comments and support. Your preference has been noted. Louisiana Revised Statute 32:263 spedifically prohibits the use of any Louisiana interstate highway by pedestrian, bicycles, or non-motorized vehicles. Bicycle and pedestrian accommodations may be possible on Alternatives 3A and 3E along the proposed Sulphur Avenue extension including the moveable bridge although a reasonable connection to Ryan Street presents some challenges. Even without a legal prohibition, a bicycle/pedestrian path attached to a new I-10 bridge is not practical. The path would have approximate
26	ĵohn pohorelsky	2021-03-25	contd: Every year the project is delayed results in increased cost of roughly \$25 million due to inflation. *The bridge continues to age. *No one knows when the bridge will be taken out of service. *The Chamber thanks Governor John Bel Edwards and LA-DOTD Secretary Shawn Wilson for taking action and beginning the process for the new bridge our region needs. *I want an iconic bridge: *Pedestrian/bike path *OFestive, adaptive LED lighting *OCable stay appearance	Public Meeting #4 Q&A Box	without a legal promotion, a province presenting part attends to reach the crest. It would have to be separated from the traffic land by a concrete barrier and it would have to be at least 14 feet wide to accommodate maintenance vehicles, police vehicles, and EMS vehicles. The facility would have to be ADA accessible probably through the use elevators on each side of the river in combination will a zig-zag path or some other means for citizens with disabilities to reach the crest. Due to the length of the approaches (approximate one-half mille), it is likely that skateboarders, those with rollerblades, some cyclists, etc. will exceed safe speeds when descending from the crest potentially resulting in serious injury crashes. Security, particularly at night, will be an issue. The facility will need frequent trash pickup and periodic cleaning. A bicycle/pedestrian path attached to a new I-10 bridge would be expensive to build and very expensive to operate. These costs would have to be reflected in the toll rates and at the end of the public private partnership, responsibility for the operating and maintenance costs, including security, would transfer to the cities of Lake Charles and West Lake DOTD does not maintain or operate multiuse paths. The goal is to provide a very attractive, but also affordable, bridge.
27	m f	2021-03-25	What accomodations will be made for possibly disturbing the bald eagle nest in Alternative 3E7 it seems to me that it is unacceptable to disturb a bald eagle nest.	Public Meeting #4 Q&A Box	The Project Team is in the process of coordinating with the agencies that protect threatened and endangered species. The Bald Eagles are protected under the Bald and Golden Eagle Protection Act. Therefore, if Alternatives 3A or 3E were to be selected, the appropriate mitigation would be undertaken in accordance with the law and best practices.
28	Robert Čarballo	2021-03-25	Are there any future plans to improvement I-2107. If so what types of improvements are anticlapted.	Fublic Meeting #4 Q&A Box	At this time there are no plans for any capacity improvements to I-210. It is the intend to treat I-210 like the rest of the interstate system that is subject to the annual project programming process.

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
29	Michael Fontenot	2021-03-25	No matter which option is chosen when will construction begin?	Public Meeting #4 Q&A Box	The Project Team does not currently have a schedule for construciton.
30	cwilrye		how would the movable bridge work how much of an impact would this have on traffic in westlake and lake charles in the event of a barge crossing or bridge maintance.	Public Meeting #4 Q&A Box	The type of movable bridge has not been determined. A navigation study is being prepared. Impacts to navigation will be provided in that document, which will be posted online.
31	jen		Really appreciate y'all taking the time to answer our questions and for your hard work. I'm for the project even though there will be tolls.	Public Meeting #4 Q&A Box	
32	Anonymous Attendee		Will this be a signature bridge? Will some requirements be levied to provide some aesthetic requirements for the local community?	Public Meeting #4 Q&A Box	The design presented provides a basic bridge form that will cost the least to build and maintain while meeting current design criteria. A signature type bridge or signature features will be considered as part of the Context Sensitive Solution (CSS) process. However, each additional feature or amenity will raise costs that would have to be offset with higher toll rates. Additional information will be published on the website. The goal is to provide a very attractive, but also affordable, bridge
33	Anonymous Attendee	2021-03-25	For ALT SG: What will DOTD do to protect the public and industry from EDC when constructing the retaining wall?	Public Meeting #4 Q&A Box	Although construction of the retaining wall will require excavation in the EDC zone, subsurface disturbance will remain shallow. Special measures will be implemented during work in the area to monitor for the presence of EDC in order to protect workers and the public from additional exposures.
34	Anonymous Attendee	2021-03-25	When would Phase 2 be contracted? Is that going to be part of the P3?	Public Meeting #4 Q&A Box	Phase 2 (Ryan Street to I-210 East End) will not be part of the Public Private Partnership (P3) project. No time frame for constructing Phase 2 has been established.
35	Rob Anonymous		I'd like additional information on the Sulphur Avenue bridge concerning the effects on commercial marine traffic. Height, width and availability to get through. There is already a railroad bridge and saltwater barrier and this will be a third delay within a short distance. please ask: Would interstate trucks be barred from using the I-210 bypass? if not then they would avoid	Public Meeting #4 Q&A Box	The type of movable bridge has not been determined. A navigation study is being prepared. Impacts to navigation will be provided in that document, which will be posted online.
36	Attendee	2021-03-25		Public Meeting #4 Q&A Box	There is no legal way to prohibit trucks from using I-210. LADOTD does not have any regulatory authority to do so.
37	mattscnc	2021-03-25	How do collect toll from vehicles that aren't local	Public Meeting #4 Q&A Box	A gantry will be placed over the roadway and equipped with a toll tag reader and with a camera that will photograph the license plate of vehicles without toll tags so they can be billed by mail.
38	Mount Calvary Baptist Church	2021-03-25	How will the interstate change between 171 and Enterprise Blvd	Public Meeting #4 Q&A Box	From Ryan St. to Opelousas, the interstate will be built on a viaduct, or landbridge. The Project Team will have an opportunity to connect the street grid under that viaduct. The access roads will be concentrated at enterprise. Therefore some of the existing ramps will go away, and there will be an opportunity to reconnect the bike and pedestrian network to that extent possible.
39	Michael Tritico	2021-03-25	The advance publicity for this meeting said that comments would be accepted. Was that incorrect or will the comment I submitted be read?	Public Meeting #4 Q&A Box	Your comments have been received and recorded in the meeting record published online at www.i10lakecharles.com.
40	Anonymous Attendee		Would any potential future federal funding from the Biden administrstion have any effect on this being a tolled P3 project? In other words, could the procurement delivery model change?	Public Meeting #4 Q&A Box	The Project Team has turned to the Public-Private model as the current path forward and only viable option. Any additional federal funding may help lower the toll rates or construct Phase 2 from Ryan Street to I-210 on the east side.
41	Anonymous Attendee		How many openings would the movable bridge see per day/month?	Public Meeting #4 Q&A Box	A navigation study is being prepared. Impacts to navigation will be provided in that document, which will be posted online.
42	Donna Little	2021-03-25	What is the archeological site that you mentioned? Where is it and what settlement, population, etc. created it?	Public Meeting #4 Q&A Box	We do not have that information at this time. The information about the site, but not its location, will be provided in the Cultural Resources Report to be completed later this year.
43	Anonymous Attendee	2021-03-25	what happens if the drinking water supply is impacted/contaminated by the construction	Public Meeting #4 Q&A Box	All alternatives were carefully developed with consideration of the Chico Aquifer and the drinking water supply. Measures protective of drinking water including monitoring for the presence and movement of the EDC above the Chico Aquifer will be implemented during construction.
44	Anonymous Attendee		Is there an option which favors means and methods of construction which are locally available? Is there an option which has a longer life span or lower maintenance costs over the life of the facilities?	Public Meeting #4 Q&A Box	The intended service life of new bridges is 75 years. With proper maintenance and repairs, a structure's service life could be extended beyond this 75-year period. Concrete bridges generally cost less to build and maintain. For projects that involve federal funds, federal law prohibits local contracting or local hiring requirements or preferences. It is anticipated that the P3 contract will require some percentage of the work be contracted with a registered Disadvantaged Business Enterprises (DBE) or Small Business Enterprise (SBE). The geographic location of the DBE or SBE will not be stipulated but it is expected that local firms will be considered.
45	Anonymous Attendee		If DOTD is closing Sampson Street for 12 months, has DOTD considered rerouting I10 to I210 and keeping the current alignment? Also, I didn't see the difference in cost between the alternativescan you please identify the diffrence in cost between the proposed.	Public Meeting #4 Q&A Box	Early in the process, the Project Team considered alternative realignments, including a southern alignment inside the I-210 loop. A cost comparison of the preliminary build alternatives is provided in Public Meeting #3 Summary in the Document Library on the project website at www.i10lakecharles.com. A cost comparison of the three reasonable alternatives is provided in the presentation prepared for Public Meeting #4, which is also available on the website. Alternatives 3A and 3E involve the extension of Sulphur Avenue including a new moveable bridge whereas Alternative 5G does not.
46	Michael Fontenot	2021-03-25	I support option 5G as it makes the most sense and is similar to other interstate sections of 110. If other options are used I believe there would be a safety concern around westlake middle school students.	Public Meeting #4 Q&A Box	Thank you for your comments. Your preference has been noted.

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
			Thank you for major progress on this crucial project. Will the design be iconic, have lighting, and a		Louisiana Revised Statute 32:263 specifically prohibits the use of any Louisiana interstate highway by pedestrian, bicycles, or non-motorized vehicles. Bicycle and pedestrian accommodations may be possible on Alternatives 3A and 3E along the proposed Sulphur Avenue extension including the moveable bridge although a reasonable connection to Ryan Street presents some challenges. Even without a legal prohibition, a bicycle/pedestrian path attached to a new I-10 bridge is not practical. The path would have approximate one-half mile approaches on each side at a three percent grade to reach the crest. It would have to be separated from the traffic lanes by a concrete barrier and it would have to be at least 14 feet wide to accommodate maintenance vehicles, police vehicles, and EMS vehicles. The facility would have to be ADA accessible probably through the use elevators on each side of the river in combination with a zig-zag path or some other means for citizens with disabilities to reach the crest. Due to the length of the approaches (approximately one-half mile), it is likely that skateboarders, those with rollerblades, some cyclists, etc. will exceed safe speeds when descending from the crest potentially resulting in serious injury crashes. Security, particularly at night, will be an issue. The facility will need frequent trash pickup and periodic cleaning. A bicycle/pedestrian path attached to a new I-10 bridge would be expensive to build and very expensive to operate. These costs would have to be reflected in the toll rates and at the end of the public private partnership, responsibility for the operating and maintenance costs, including security, would transfer to the cities of Lake Charles and West Lake as DOTD does not maintain or operate multiuse paths.
47	George Swift	2021-03-25	pedestrian and bike path.	Public Meeting #4 Q&A Box	The goal is to provide a very attractive, but also affordable, bridge.
			The comments above by John Pohorelsky are also supported by the I-10 Calcasieu River Bridge Task Force as		
48 [Denise Rau	2021-03-25	a whole. I believe IMCAL is also in support of Alt 5G	Public Meeting #4 Q&A Box	Thank you for your comment and support. Your preference has been noted.
	Anonymous		What additional traffic impact does DOTD anticipate on Hwy. 171 north of I-10 during the construction of		
	Attendee Anonymous	2021-03-25	the I-10 bridge corridor?	Public Meeting #4 Q&A Box	Construction affecting US 171 will be phased so that traffic during peak times is obstructed by construction as little as possible.
50 A	Attendee	2021-03-25	Will the fixed bridge be concrete or steel?	Public Meeting #4 Q&A Box	The bridge structures will be primarily concrete with steel components used as needed.
	Paul Geary	2021-03-25	THE BUILTING OF THIS NEW BRIDGE WILL ONSTATE QO S BE OPEN FOR THU TRAFFIC.	Public Meeting #4 Q&A Box	We did not understand the question.
	Anonymous Attendee	2021-03-25	Thanks everyone for your hardwork, it shows. Great job presenting a complex project!	Public Meeting #4 Q&A Box	Thank you for your comment and your support
53 A	Anonymous Attendee	2021-03-25	Will there be a study on the impacts to businesses in the downtown area due to the toll?	Public Meeting #4 Q&A Box	Impacts from tolling are being evaluated in terms of the percent of vehicles that will divert to alternate routes and how the toll rate would affect low-income and minority households. No other studies are being prepared as part of the EIS process.
	Anonymous Attendee	2021-03-25	What is the anticipated USCG navigation channel width under the new I-10 bridge?	Public Meeting #4 Q&A Box	The existing channel width of approximately 200 feet will be maintained.
	Matthew Duberville	2021-03-25	What is the expected ratio of public to private funding?	Public Meeting #4 Q&A Box	LADOTD and the State of Louisiana have allocated \$85 million in state funding to match federal funds for construction of the proposed project. In addition to that, LADOTD will make a contribution of its regular federal funds from the department's annual budget that it gets from the federal government. A ratio of public to private funding has not yet been determined; it will be partly based on which alternative is chosen.
	Anonymous Attendee	2021-03-25	what type of movable bridge do you envision and what is approximate span length?	Public Meeting #4 Q&A Box	The type of moveable bridge has not yet been determined. The span length will be 160 - 200 ft.
57 5	Stafford Frrank	2021-03-25	I know I heard bridge should last 100 years. Considering pollutants in the air, how much less of life of bridge expectancy was considered into this project.	Public Meeting #4 Q&A Box	The intended service life of new bridges is 75 years. With proper maintenance and repairs, a structure's service life could be extended beyond this 75-year period. Air pollution in this region is not anticipated to negatively impact the service life of any new structures constructed.
58 E	Edwin DeRouen	2021-03-25	Will the parties resposibele for the EDC spill be required to contribute to the cost of the bridge?	Public Meeting #4 Q&A Box	There is a lawsuit pending that will determine if the parties responsible for the EDC release will have to pay damages.
	Anonymous Attendee	2021-03-25	I am in support of alternative 5G because it is least expensive and impacts the fewest. It also maintains good access to the community of Westlake, and improves the bad crossings of the railroads	Public Meeting #4 Q&A Box	Thank you for your comments. Your preference has been noted.
60 F	Paul Geary	2021-03-25	no sound	Public Meeting #4 Q&A Box	We apologize for the technical issues.
	Anonymous Attendee	2021-03-25	When will we see a Developer chosen/announced?	Public Meeting #4 Q&A Box	It is anticipated that the Project Team will announce a Public Private Partnership (P3) developer by Spring of 2023.
4	Anonymous Attendee		Has the option of 2 toll lanes and 1 free lane been evaluated in each direction that would eliminate the question of 210 being the adjacent free route?	Public Meeting #4 Q&A Box	This scenario has not been studied because financial feasibility requires that all lanes on the bridge crossings be tolled so that the overall toll rate can be set at a reasonable rate.
	Anonymous Attendee	2021-03-25	How long will I10 be shut down for construction for the 3 alternatives?	Public Meeting #4 Q&A Box	Construction on I-10 will be phased so that it remains open during construction and traffic during peak times is obstructed as little as possible. Some lanes may be closed occasionally but at least one lane in each direction on I-10 will remain open at all times.
64 E	Brett Downer		What are some similar bridge solutions in America that we can look to — or that you've already looked into — particularly along a U.S. interstate?	Public Meeting #4 Q&A Box	Major bridge replacements on interstate systems is relatively common. Example bridge designs including interstate tolled bridges will be provided as part of the Context Sensitive Solution (CSS) process. However, each additional feature or amenity above the basic design presented will raise costs that would have to be offset with higher toll rates. Additional information will be published on the website.

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
65	Anonymous Attendee	2021-03-25	how will the chosen Developer be held to selecting 'local, disadvantaged' businesses in this construction	Public Meeting #4 Q&A Box	For projects that involve federal funds, federal law prohibits local contracting or local hiring requirements or preferences. It is anticipated that the Public Private Partnership (P3) contract will require some percentage of the work be contracted with a registered Disadvantaged Business Enterprises (DBE) or Small Business Enterprise (SBE). The geographic location of the DBE or SBE will not be stipulated but it is expected that local firms will be considered.
66	Byron Martin		I understand Alt 5G would result in the closure of Sampson street access to I10 for approximately 12 months. If so, what would be the alternate route?	Public Meeting #4 Q&A Box	Several detours would be available to drivers during this period. From LA 378 in Westlake to Sam Houston Jones Parkway and US 171 in Moss Bluff is the longest but least complicated route for avoiding construction. Sulphur Avenue west to Trousdale and PPG Drive is a second detour and Sulphur to Miller to Isle of Capri and the I-10 Service Road is a third.
67	Anonymous Attendee	2021-03-25	Will there still have a boat launch? I vote for SG.	Public Meeting #4 Q&A Box	The boat launch will be kept in the same general location. Your preference has been noted.
	Anonymous				
68	Attendee	2021-03-25	Is the movable bridge only present in Alternative 5G?	Public Meeting #4 Q&A Box	The movable bridge is present in Alternatives 3A and 3E. 5G does not include a movable bridge.
69	Anonymous Attendee	2021-03-25	For anyone opposed to tolls, the I-210 will remain free. I use toll roads all over the country because they are in better condition, safer, and provide more convenient and faster transportation.	Public Meeting #4 Q&A Box	Thank you for your comments.
70	Anonymous	2024 02 25	Courte, transplanta being a side of	Dublis Maskins #4 OR A Day	The design presented provides a basic bridge form that will cost the least to build and maintain while meeting current design criteria. A cable-stay or other signature type bridge will be considered as part of the Context Sensitive Solution (CSS) process. However, each additional feature or amenity will raise costs that would have to be offset with higher toll rates. Additional information will be
70	Attendee	2021-03-25	DustyIs a cable stay bridge being considered I continue to support, and will continue to help pursue, any and all available funding, such as federal grants,	Public Meeting #4 Q&A Box	published on the website. The goal is to provide a very attractive, but also affordable, bridge.
71	Bart	2021-03-25	possible federal infrastructure bills, disaster relief funding, state funds and environmental settlements to reduce or eliminate both the amount to be financed and the toll.	Public Meeting #4 Q&A Box	Thank you for your comments and your support.
72	DuRousseau	2021-03-25	Would give some examples of Bridges DOTD considered aesthetically appealing?	Public Meeting #4 Q&A Box	Example bridge designs will be provided as part of the Context Sensitive Solution (CSS) process. However, each additional feature or amenity will raise costs that would have to be offset with higher toll rates. Additional information will be published on the website. The goal is to provide a very attractive, but also affordable, bridge.
73	Anonymous Attendee		Do exhibits on website show Required R/W taking lines and required Control of Access lines for each alt? If not, when will this be available? Do the R/W acreage and relocations reported include consideration for properties that will lose access to existing or new roads?	Public Meeting #4 Q&A Box	The interactive maps on the website show the existing and/or proposed right of way limits based on Geographic Information System references. Acquisition of right of way will be based on geographic limits certified by a land surveyor for the preferred alternative. Access issues will be addressed on a case-by-case basis.
74	Anonymous Attendee	2021-03-25	Will there be any toll waviers or reductions in toll fee for local traffic (as opposed to thru traffic)?	Public Meeting #4 Q&A Box	The purchase of a toll tag will provide a discount for frequent use that is typical of local trips.
75	Anonymous Attendee		We have been told that a protected pedestrian/bike path can be constructed along the side of the bridge at a reasonable cost and that this has been done in other states. Will you consider such a plan if the current statute that prohibits it is rescinded?	Public Meeting #4 Q&A Box	Louisiana Revised Statute 32:263 specifically prohibits the use of any Louisiana interstate highway by pedestrian, bicycles, or non-motorized vehicles. Bicycle and pedestrian accommodations may be possible on Alternatives 3A and 3E along the proposed Sulphur Avenue extension including the moveable bridge although a reasonable connection to Ryan Street presents some challenges. Even without a legal prohibition, a bicycle/pedestrian path attached to a new I-10 bridge is not practical. The path would have approximate one-half mile approaches on each side at a three percent grade to reach the crest. It would have to be separated from the traffic lanes by a concrete barrier and it would have to be at least 14 feet wide to accommodate maintenance vehicles, police vehicles, and EMS vehicles. The facility would have to be ADA accessible probably through the use elevators on each side of the river in combination with a zig-zag path or some other means for citizens with disabilities to reach the crest. Due to the length of the approaches (approximately one-half mile), it is likely that skateboarders, those with rollerblades, some cyclists, etc. will exceed safe speeds when descending from the crest potentially resulting in serious injury crashes. Security, particularly at night, will be an issue. The facility will need frequent trash pickup and periodic cleaning. A bicycle/pedestrian path attached to a new I-10 bridge would be expensive to build and very expensive to operate. These costs would have to be reflected in the toll rates and at the end of the public private partnership, responsibility for the operating and maintenance costs, including security, would transfer to the cities of Lake Charles and West Lake as DOTD does not maintain or operate multiuse paths.
76	Stafford Frrank		What type of protections are being put in place to insure that any jobs that can be performed by Lake Charles/West Lake/surrounding area is done by local residents	Public Meeting #4 Q&A Box	For projects that involve federal funds, federal law prohibits local contracting or local hiring requirements or preferences. It is anticipated that the Public Private Partnership (P3) contract will require some percentage of the work be contracted with a registered Disadvantaged Business Enterprises (DBE) or Small Business Enterprise (SBE). The geographic location of the DBE or SBE will not be stipulated but it is expected that local firms will be considered
77	david		My question Where is the current gas tax dedicated to roads and bridges at, and why are they not being used Where is the casino money and why is this not being used	Public Meeting #4 Q&A Box	LADOTD and the State of Louisiana have allocated \$85 million in state funding to match federal funds for construction of the proposed project. In addition to that, LADOTD will make a contribution from its annual budget (gas tax), and has already applied for—and will continue to seek—grant funds and use of infrastructure funding from federal programs such as BUILD (now RAISE), INFRA, and American Rescue Act. However, competition and evolving regulations make these sources unpredictable. For this reason, the Project Team has turned to the Public-Private Partnership (P3) model as the most reliable path forward.
78	Michael Tritico	2021-03-25	How many people are attending this ZOOM meeting?	Public Meeting #4 Q&A Box	224 people attended: 24 were panelists or members of the Project Team, i.e. LaDOTD, FHWA, HNTB, and subcontractors
79	david		John bell Edward's advised he would put up Louisiana part why is a toll necessary	Public Meeting #4 Q&A Box	Without tolling revenue, there is no incentive for the private sector to participate in the Public-Private Partnership (P3). The State has committed \$85 million as match for federal funding. However, the balance of federal funding is not available necessitating private

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
			With the cost sharing going from to 90/10 fed/state now to 10/90 for state to cover the cost sharing		The standard cost share for this project ranges between 80 and 90 percent, federal. The State Transportation Improvement Plan (STIP) is developed and revised annually to determine how state transportation funds, including budgeted federal funds, will be spent. The \$85 million allocation was appropriated specifically for the I-10 Calcasieu Bridge and Improvements project. However, the balance of
80	Keith	2021-03-25	85million was the last low est. WHY is the priority being reallocated to improved of the city roads?	Public Meeting #4 Q&A Box	federal funds needed to full fund the project is not available.
81	Jason Boaz		With 5G the railroad spur impact looks to be significant to both City of Westlake possible riverfront development as well as impacting not 1 but both entrances to Isle of Capri Casino. What options were researched to minimize or eliminate these issues?	Public Meeting #4 Q&A Box	The railroad relocation is only necessary for the 5G alternative because the bridge at its west end will approach grade over the existing railroad spurs will not provide sufficient vertical clearance for the trains to pass. The options presented are preliminary and will be coordinated with the railroads and other interested parties if Alternative 5G is determined to be the preferred alternative.
82	Anonymous Attendee	2021-03-25	Will a proposed new I-10 interstate bridge in Baton Rouge be a P3 project (w/tolls) also?	Public Meeting #4 Q&A Box	Information about the proposed Mississippi River Crossing in the Capital Area can be found online at https://capitalareabridge.org/. Information on the widening of I-10 connecting to the existing Mississippi River Bridge (Wilkinson Bridge) can be found online at https://i10br.com/.
83	Anonymous Attendee		What percentage of funding is dedicated to context sensitive solutions like iconic/aesthetic features. EX: Is it an 10% - 15% of total cost?	Public Meeting #4 Q&A Box	No determination about the costs or funding for amenities and improvements considered during the Context Sensitive Solutions (CSS) process has been made. The design presented provides a basic bridge form that will cost the least to build and maintain while meeting current design criteria. Each additional feature or amenity will raise costs that would have to be offset with higher toll rates. The goal is to provide a very attractive, but also affordable, bridge. Additional information will be provided during the CSS process and published on the website.
84	Anonymous Attendee	2021 02 25	wouldn't a moveable bridge have a signficantly higher long-term maintenance cost than fixed bridges?	Public Mosting #4 O.S.A. Boy	Yes, movable bridges have moving components that require more maintenance than fixed bridges.
85	Keith		If a toll is put in place then how will you prevent traffic avoidance of the toll (everyone will use the 171 moss bluff and the 210 bridge) it is illegal to force tolls on anyone. Why is the budge not alloacated for what can be built per contract bids. If the contractor can not build for the ammount then other funding needs to be looked at. Such as investigating federal funding as it is a major evacuation route		There is no way legal way to prohibit 'thru' traffic from using Interstate 210, because it is a public facility open to all traffic. Other funding sources have been and are being pursued, but the P3 model has been determined to be the most reliable form of filling the funding gaps. The P3 is a form of design-build contracting and the developer/designer will be obligated to complete the work and operate the facility within an agreed upon cost.
					Only the bridge crossing(s) will be tolled. There will be no toll booths. A gantry to collect tolls will be located near the bridge or bridges,
86	mattscnc	2021-03-25	How far does the toll go from east to west on i10	Public Meeting #4 Q&A Box	depending upon the alternative that is built. The location of the gantry or gantries will be determined during final design.
87	Anonymous Attendee	2021-03-25	Does the project plan to to replace the I-210/I-10 interchange and US 90 bridges on the West side of the project	Public Meeting #4 Q&A Box	The I-10 Calcasieu Bridge and Improvements project does not include improvements to I-210 except at the I-210 West End interchange. The ramp that exits I-210 to I-10 westbound will be reconstructed to meet the design criterion for curve and superelevation. The US 90 overpass will be removed and a new I-10 overpass will be built to cross over US 90 that will be reconstructed at-grade.
88	Edwin DeRouen	2021-03-25	How long will tolls be charged? Will they be in place forever?	Public Meeting #4 Q&A Box	Assuming a 35- to 50-year concession agreement, where the first five years consists of the design-build process, it is anticipated that tolls will remain in place, once the bridge is open to traffic, for a period of 30 to 45 years, but not to exceed 50 years.
89	Anonymous Attendee	2021-03-25	At the end of the tolling phase, is DOTD going to ask the PPP company to refurbish the movable bridge before turning the facility back over?	Public Meeting #4 Q&A Box	At the end of the concession period of 35 to 50 years, the Public Private Partnership (P3) will be required to return the facility including the bridge(s) to a reasonable state of repair.
90	Anonymous Attendee	2021-03-25	To touch on the previous question. I think they were asking whether or not local contractors willl be favored for receiving the contracts.	Public Meeting #4 Q&A Box	For projects that involve federal funds, federal law prohibits local contracting or local hiring requirements or preferences. It is anticipated that the Public Private Partnership (P3) contract will require some percentage of the work be contracted with a registered Disadvantaged Business Enterprises (DBE) or Small Business Enterprise (SBE). The geographic location of the DBE or SBE will not be stipulated but it is expected that local firms will be considered.
91	Anonymous Attendee	2021-03-25	If DOTD is closing Sampson Street for 12 months, has DOTD considered rerouting 10 to 210 and keeping the current bridge alignment as opposed to relocating the bridge? I think it would save cost.	Public Meeting #4 Q&A Box	Relocation of the new bridge is needed to ensure that travel on I-10 would be maintained during the project's construction. This includes maintaining traffic on I-10 while the new Calcasieu River Bridge is constructed and while I-10 between the I-210 interchanges is under construction.
92	Keith		Why is the 85 million not being realloacted to the ryan st / hwy 14 roads that need to be reworked ? Why is the contraband bayou bridge not being looked at (which has a high pedistrian death rate) :?	Public Meeting #4 Q&A Box	The State Transportation Improvement Plan (STIP) is developed and revised annually to determine how state transportation funds, including budgeted federal funds, will be spent. The \$85 million allocation was appropriated specifically for the I-10 Calcasieu Bridge and Improvements project. A southern alignment inside the I-210 loop was considered but dismissed as causing too many impacts to natural resources including Bayou Contraband.
93	Mark Conner	2021-03-25	Will the toll ever end	Public Meeting #4 Q&A Box	Assuming a 35- to 50-year concession agreement, where the first five years consists of the design-build process, it is anticipated that tolls will remain in place, once the bridge is open to traffic, for a period of 30 to 45 years, but not to exceed 50 years.
94	Rob	2021-03-25	Will the bridge be wide enough to expand from 6 to 8 lanes if needed in the next 75 years?	Public Meeting #4 Q&A Box	The current proposal for I-10 is three lanes in each direction, which has been determined to meet the capacity needs of future traffic volumes through the year 2042. Additional traffic information will be provided in the Traffic Analysis being prepared for the project.

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
95	Anonymous Attendee	2021-03-25	Do you need a bridge? Can you do an embankment instead?	Public Meeting #4 Q&A Box	Any crossing of the Calcasieu River will require a bridge. If you are referring to the viaduct or land bridge for the section of I-10 from Ryan Street to Opelousas Street, this design was determined to be more efficient than alternating sections of bridge and embankment, because the footprint of an embankment that meets design criteria would require a wide area that would encroach upon the adjacent neighborhoods, service roads, and cemeteries. It would also require major reconstruction if I-10 ever needed to be widened.
96	cwilrye	2021-03-25	I support option 5G	Public Meeting #4 Q&A Box	Thank you for your comment. Your preference has been noted.
	,				
97	Stafford Frrank Anonymous	2021-03-25	If I heard correctly, Dusty Bastion mentioned that top of bridgewill have gerdes at top. Wouldn't that make bridge less safe when wet?	Public Meeting #4 Q&A Box	We do not understand the question.
98	Attendee	2021-03-25	When will the answers to the unanswered questions be available on your website?	Public Meeting #4 Q&A Box	A summary of Public Meeting #4 with responses to questions and comments will be provided on the project website soon.
99	Keith	2021-03-25	If all the lanes are tolled then no one will go through them and most will by pass If there is a wreck on 210 and you have to bypass to the 110 will toll be lifted? you can not force tollways to people due to hardship conditions	Public Meeting #4 Q&A Box	It is the intention of LADOTD to keep the toll rates as low as possible so that using the I-10 bridge does not impose a financial burden on any household in the area.
100	Anonymous Attendee	2021-03-25	is current DOTD policies regarding control of access near ramps as well as traffic signal policies spacing been considered in these alts?	Public Meeting #4 Q&A Box	Control of access and traffic signalization will follow current LADOTD and FHWA policies.
101	Anonymous Attendee	2021-03-25	My prefrence is the one that avoids disturbance of the EDC contamination.	Public Meeting #4 Q&A Box	Thank you for your comment. Your preference has been noted.
102	Edwin DeRouen	2021-03-25	How will Westlake traffic be able access to Lake Charles during the reconstruction of Sampson St. (18 months) be provided?	Public Meeting #4 Q&A Box	It is anticipated that Sampson Street will be closed to traffic for a period of 12 months. Several detours would be available to drivers during this period. From LA 378 in Westlake to Sam Houston Jones Parkway and US 171 in Moss Bluff is the longest but least complicated route for avoiding construction. Sulphur Avenue west to Trousdale and PPG Drive is a second detour and Sulphur to Miller to Isle of Capri and the I-10 Service Road is a third.
	Anonymous				
103	Attendee	2021-03-25	I vote for the no build and widen I-210. Use a ferry for local crossings.	Public Meeting #4 Q&A Box	Thank you for your comments. Your preference has been noted.
104	Keith	2021-03-25	TBH this community thinks it is ridcoulus that we are looking at 85million + for a bridge when after a hurricane there are more important problems and streets that are horrid and not compliant	Public Meeting #4 Q&A Box	The State Transportation Improvement Plan (STIP) is developed and revised annually to determine how state transportation funds, including budgeted federal funds, will be spent. The \$85 million allocation was appropriated by the legislature specifically for the I-10 Calcasieu Bridge and Improvements project.
105	Mount Calvary Baptist Church		I am very much concern about the corridors from I-210 to Ryan Street. Our City did open the frontage roads along side the I-10 to create access within the community. Therefore it is my hope that you would make sure that the interstate between 171 and Ryan street would be beautified so the locals can enjoy the convenience would not be a forgotten	Public Meeting #4 Q&A Box	Concepts regarding visual appeal and incorporation of other amenities such as bike paths, sidewalks, signage, and lighting of I-10 from Ryan Street to US 171 were provided in the first Context Sensitive Solution (CSS) Workshop. A summary report of that meeting is in the Project Library online at www.i10lakecharles.com. More ideas will be proposed as a part of the continuing CSS process. Funding and responsibility for maintenance of amenities in this section of the project will be determined.
106	Andrea Travani	2021-03-25	Hello: what are the horizontal and vertical clearances of the main span? Are there any conceptual plans available?	Project Email	According to the line and grade plans, vertical clearance of the main span will be 73 feet minimum and horizontal clearance will be 200 feet minimum.
107	Ben Herrera	2021-04-01	We must have an Iconic Bridge with a Pedestrian/Bike Path, with the cable stay appearance and festive LED lighting.	Project Email	Louisiana Revised Statute 32:263 specifically prohibits the use of any Louisiana interstate highway by pedestrian, bicycles, or non-motorized vehicles. Bicycle and pedestrian accommodations may be possible on Alternatives 3A and 3E along the proposed Sulphur Avenue extension including the moveable bridge although a reasonable connection to Ryan Street presents some challenges. Even without a legal prohibition, a bicycle/pedestrian path attached to a new I-10 bridge is not practical. The path would have approximate one-half mile approaches on each side at a three percent grade to reach the crest. It would have to be separated from the traffic lanes by a concrete barrier and it would have to be at least 14 feet wide to accommodate maintenance vehicles, police vehicles, and EMS vehicles. The facility would have to be ADA accessible probably through the use elevators on each side of the river in combination with a zig-zag path or some other means for citizens with disabilities to reach the crest. Due to the length of the approaches (approximately one-half mile), it is likely that skateboarders, those with rollerblades, some cyclists, etc. will exceed safe speeds when descending from the crest potentially resulting in serious injury crashes. Security, particularly at night, will be an issue. The facility will need frequent trash pickup and periodic cleaning. A bicycle/pedestrian path attached to a new I-10 bridge would be expensive to build and very expensive to operate. These costs would have to be reflected in the toll rates and at the end of the public private partnership, responsibility for the operating and maintenance costs, including security, would transfer to the cities of Lake Charles and West Lake as DOTD does not maintain or operate multiuse paths. The goal is to provide a very attractive, but also affordable, bridge.
					LADOTD published the notice of intent for the Bublic Britishe Destaurbin (D3) in Learning 2024, Date in recording 40, 22
108	Carmine Borea	2021-03-30	When the RFQ for the noted project will be released? Can you share the link of where the document will be published?	Project Email	LADOTD published the notice of intent for the Public Private Partnership (P3) in January 2021. Details regarding the P3, including the letters of interest, can be viewed on LADOTD's website at https://bit.ly/3c2KO0b. The draft EIS document will be made available later this year. Please check the website at www.i10lakecharles.com for information.
109	Christopher Bertrand	2021-03-27	No concept of tolls. Stop trying to take money from us. No private partnerships! Let the state and federal government work it out.	Project Email	Other funding sources have been and are being pursued, but the Public Private Partnership (P3) model has been determined to be the most reliable form of filling the funding gaps. Tolls will only cover a portion of the costs to design, build, operate, and maintain the proposed project but without this revenue, there is no incentive for the private sector to participate in the P3.

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
			The most sensible alternative is not presented and may not have been considered. That alternative would		
			add about 5 miles to the trip between Welsh and Vinton but it would completely bypass the urban/I-10 Bridge bottleneck that exists from Chloe to West Sulphur.		
			Here is a description of a practical flat route that would make it unnecessary to build a new bridge in the EDC		
			contaminated zone:		
			The "North Bypass" would take off to from just west of Welsh, go northwest about 9 miles to a cloverleaf at U.S. Highway 165 south of Fenton, then turn west, go along the existing high ground to avoid as much		
			wetland disruption as possible, cross with only slightly-elevated bridges Bayou Serpent and the Calcasieu		
			River, (a distance for this segment of about 11 miles), then proceed another 6 miles to a cloverleaf at U.S.		
			Highway 171 south of Gillis, proceed westward another 5 miles to again, a low-level crossing of Hickory Branch and then go four miles to a cloverleaf at Louisiana State Highway 27 north of Sulphur, then turn		
			southwesterly for about 15 miles to a clove leaf at Eduisiana State Figure 27 hotti of Sulphur, then turn southwesterly for about 15 miles to rejoin Interstate 10 just east of Vinton. That is a 50 mile route, about 5		
			miles longer than the existing route between one end of the bottleneck to the other. The enhancement of		
			safety for motorists, the reduction in energy wastage on steep slopes and from stop and go situations all make a flat bypass north of the current route more logical than are the alternatives presented even given		
			the extra 5 miles of travel required.		
			Furthermore, the route and the cloverleaves described above would simplify trips to and from places like		Early in the process, the Project Team considered alternative realignments, but they were extremely expensive. A far northern bypass
			Alexandria, Kinder, DeRidder, Houston, and New Orleans by removing the need to navigate the Calcasieu		may be be considered for the area in the long-term, but it would only serve through traffic and the surrounding small towns and
110	Michael Tritico		metropolitan area. A much more efficient highway system will also diminish greenhouse gases. Thank you for the opportunity to submit these comments.	Project Email	villages. The I-10 Calcasieu Bridge project must deal with an aging bridge that needs to be replaced and the aging interstate that serves Lake Charles, Westlake, and Sulphur residents and businesses.
					Louisiana Revised Statute 32:263 specifically prohibits the use of any Louisiana interstate highway by pedestrian, bicycles, or non-
					motorized vehicles. Bicycle and pedestrian accommodations may be possible on Alternatives 3A and 3E along the proposed Sulphur
					Avenue extension including the moveable bridge although a reasonable connection to Ryan Street presents some challenges. Even
					without a legal prohibition, a bicycle/pedestrian path attached to a new I-10 bridge is not practical. The path would have approximate one-half mile approaches on each side at a three percent grade to reach the crest. It would have to be separated from the traffic lanes
					by a concrete barrier and it would have to be at least 14 feet wide to accommodate maintenance vehicles, police vehicles, and EMS
					vehicles. The facility would have to be ADA accessible probably through the use elevators on each side of the river in combination with
					a zig-zag path or some other means for citizens with disabilities to reach the crest. Due to the length of the approaches (approximately one-half mile), it is likely that skateboarders, those with rollerblades, some cyclists, etc. will exceed safe speeds when descending from
					the crest potentially resulting in serious injury crashes. Security, particularly at night, will be an issue. The facility will need frequent
			The opportunity to build a bridge in our lifetime with the significance of the I-10 bridge is a seminal moment.		trash pickup and periodic cleaning. A bicycle/pedestrian path attached to a new I-10 bridge would be expensive to build and very
			I urge LADOTD to not only look to function but to form as they plan for this bridge. This bridge will send a message about our community and the aesthetics of the bridge are just as important as the functioning of		expensive to operate. These costs would have to be reflected in the toll rates and at the end of the public private partnership, responsibility for the operating and maintenance costs, including security, would transfer to the cities of Lake Charles and West Lake as
			the bridge. It is my hope that LADOTD will provide an Iconic Bridge with a Pedestrian/Bike Path, with the		DOTD does not maintain or operate multiuse paths.
111	Daryl Burckel	2021-04-01	cable stay appearance and festive LED lighting.	Project Email	The goal is to provide a very attractive, but also affordable, bridge.
			Hey, good morning. This is Terez, here in Baton Rouge. I'm with Dodge reports and I'm calling about some		
112	Therese Ruffino		information on the Calcasieu Bridge. If you could please return my call, my phone number is (413) 648-6103. Again, my number is (413) 648-6103. Thank you!	Project Phone	Thank you for your interest in the project. We responded to your inquiry by phone and email and provided you with the contact information for the P3 team.
112	Therese Ruffino	2021-03-31	Again, my number is (413) 648-6103. Thank you!	Project Priorie	Information for the P3 team.
113	Donna Fabacher	2021-03-29	Why not build I-10 bridge for the future, instead of the present only? In 20-30 years we will need 8 lanes in stead of 6, and then what? We need 6 now, so please be smart and build 8.	Project Email	The current proposal for I-10 is three lanes in each direction, which has been determined to meet the capacity needs of future traffic volumes through the year 2042. Additional traffic information will be provided in the Traffic Analysis being prepared for the project.
-113				. roject Email	200 Each and the project
			Can you please send me or tell me how to obtain pictorial layouts or other types of layouts of the new 1-10		Illustrations of the alternatives can be coopyrithin the recorded presentation. You may also view interesting and the second state of the second s
114	Gary Woods	2021-03-26	bridge layouts. Layouts lets me understand a lot clearer what is actually being proposed versus a written description,	Project Email	Illustrations of the alternatives can be seen within the recorded presentation. You may also view interactive maps online at www.i10lakecharles.com.
	,			•	
115	DSTipton	2021-03-25	We are trying to sign onto to zoom meeting. Need a project id#	Project Email	We apologize for the technical issues.
					Louisiana Revised Statute 32:263 specifically prohibits the use of any Louisiana interstate highway by pedestrian, bicycles, or non-motorized vehicles. Bicycle and pedestrian accommodations may be possible on Alternatives 3A and 3E along the proposed Sulphur
					Avenue extension including the moveable bridge although a reasonable connection to Ryan Street presents some challenges. Even
					without a legal prohibition, a bicycle/pedestrian path attached to a new I-10 bridge is not practical. The path would have approximate
					one-half mile approaches on each side at a three percent grade to reach the crest. It would have to be separated from the traffic lanes by a concrete barrier and it would have to be at least 14 feet wide to accommodate maintenance vehicles, police vehicles, and EMS
					vehicles. The facility would have to be ADA accessible probably through the use elevators on each side of the river in combination with
					a zig-zag path or some other means for citizens with disabilities to reach the crest. Due to the length of the approaches (approximately
					one-half mile), it is likely that skateboarders, those with rollerblades, some cyclists, etc. will exceed safe speeds when descending from
			As a life long resident and grandson of former Governor Sam Houston Jones, who was instrumental in		the crest potentially resulting in serious injury crashes. Security, particularly at night, will be an issue. The facility will need frequent trash pickup and periodic cleaning. A bicycle/pedestrian path attached to a new I-10 bridge would be expensive to build and very
			bringing much of the petro-chemical industry to Westlake, I feel like the replacement for this iconic bridge		expensive to operate. These costs would have to be reflected in the toll rates and at the end of the public private partnership,
			deserves to be iconic in its own way, including a pedestrian and bicycling path. Furthermore, a pedestrian-		responsibility for the operating and maintenance costs, including security, would transfer to the cities of Lake Charles and West Lake as
116	Houston Jones		friendly connection from Westlake to downtown Lake Charles could provide economic stimulus for the area. Thanking you in advance for your consideration on this request,	Project Email	DOTD does not maintain or operate multiuse paths. The goal is to provide a very attractive, but also affordable, bridge.
110		2021 03-31	Training you in destruct for your consideration on this request,	1 Toject Email	The Boards to provide a very accidence, but also arroradate, bridge.

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
			I have a few question/comments below.		
			1. I look through the information on the project website and please forgive me if I missed it, but looking at		
			the proposed alternative routes for a 2nd crossing over the river, instead of extending Sulphur Ave across		
			north of the I-10 bridge through the swap area, would it be more economically efficient to extend Sulphur		
			Ave to Westlake Ave then south to Isle of Capri Blvd and extend Isle of Capri Blvd across the river with a		
			moveable bridge to North Lake Shore drive. I have attached a screen image showing the route I suggest I		
			apologize if the resolution low.		
			2. Is there and estimate of the revenue that will be generated from the scrape metal value of the old I-10		
			bridge structure once it is dismantling?		
			3. To generate revenue for the project the currently railing on the bridge that has the pistols in a crossing		
			pattern could be sold at auction to raise more money than their scrap value.		
			4. Also to relive some of the congestion during the construction of the I-10 project will DOTD perform the		1. Early in the process, the Project Team considered alternative alignments. An extension of Isle of Capri Boulevard would impact the
			following work. 4.a widen highway 378 from Westlake to Moss Bluff area there is only a 3 miles section left that is currently		casino on the west bank and the beach / recreation areas on the east bank. 2. The recovery and/or disposal of scrap metal will be considered as part of the demolition estimates.
			2 lanes this would give local traffic a route to the industrial sector without using the interstate system and a		The recovery and/or disposal of scrap metal will be considered as part of the demolition estimates. Thank you for your suggestion.
			detour route during construction or accidents.		4a. Widening of LA 378 is not within the scope of the I-10 Calcasieu Bridge and Improvements project but your comments are noted.
			4b. widening I-210 on the west end between I-10 and the I-210 bridge to 3 lanes this is just a 0.75 mile		4b. Widening of I-210 is not within the scope of the I-10 Calcasieu Bridge and Improvements project but your comments are noted.
			section currently there are 4 routes feeding down to 2 in this area I-10 has 2 lanes, Pete Manena RD has an		and a stope of the 120 constant and project out your comments are noticed.
			on ramp, and I-10 west bound has 1 lane, 3 lanes would reduce congestion by also more time for traffic		
117	J Watkins	2021-03-25	organize before going down to 2 lanes on the I-210 bridge.	Project Email	
			Million and delik Data Manager Delika anti-adad at the 140 Coming Delia West John Herritage College		
1			Why couldn't Pete Manena Rd be extended to the I-10 Service Rd to West Lake. It would relieve some of the		Widening of Reta Manage Read is not within the scope of the L10 Calcada, Bridge and Improvements
118	IDM	2021 02 20	I-10 off ramp congestion as well as be a alternate route for wreck and emergencies on the portion from I- 210 and I-10 west.	Project Email	Widening of Pete Manena Road is not within the scope of the I-10 Calcasieu Bridge and Improvements project but your comments are noted.
118	JUM	2021-03-28	210 and 1-10 west.	Project Email	noteu.
			Not only could Pete Manena be extended, the Hwy 90 ramp to I-10 could also be routed via I- 10 Service Rd		Widening of Pete Manena Road is not within the scope of the I-10 Calcasieu Bridge and Improvements project but your comments are
119	JDM	2021-02-28	to West Lake as will, giving two alternate routes to relieve traffic congestion in emergencies.	Project Email	noted.
			I am writing in hopes that my message is read clearly from the stakeholders proposing the different		
			variations of the I-10 Bridge revisions. Central Crude has a barge terminal located north of the saltwater		
			barrier at Old Town Bay. Several times in a month we sell our crude oil to our customers at our dock. Our		
			customers make arrangements for barges to arrive at our dock and receive the product. As you may		
			understand now, it already takes several hours for tugs/barges to make their way to old town bay due to the		
			railroad bridge as well as the saltwater barrier restrictions for hours of operation. With that being said, any		
			other variation of the bridge construction that is not "Alternate 5G" will create more demurrage times for		
			our customers barges. Additionally if there are additional obstacles that barges will face then that will create		
			economic issues for Central Crude and its customers, thus making future business unviable. Should anyone		A navigation study is being prepared. Impacts to navigation will be provided in that document, which will be posted online. Your
120	Joe Guthrie	2021-03-31	have any questions please feel free to contact me via email or phone.	Project Email	preference has been noted.
121	Joseph Posson	2021-03-30	Ridiculous to have a toll on I-10	Project Email	Without tolling revenue, there is no incentive for the private sector to participate in the Public-Private Partnership (P3).
	зозерит оззон	2021 00 00	Indications to have a toll off 120	r roject zman	The local coming revenue, where is no meetite for the private sector to participate in the radius rivate radius rivate.
					Your preference has been noted. Louisiana Revised Statute 32:263 specifically prohibits the use of any Louisiana interstate highway by
					pedestrian, bicycles, or non-motorized vehicles. Bicycle and pedestrian accommodations may be possible on Alternatives 3A and 3E
					along the proposed Sulphur Avenue extension including the moveable bridge although a reasonable connection to Ryan Street presents
					some challenges. Even without a legal prohibition, a bicycle/pedestrian path attached to a new I-10 bridge is not practical. The path
					would have approximate one-half mile approaches on each side at a three percent grade to reach the crest. It would have to be
					separated from the traffic lanes by a concrete barrier and it would have to be at least 14 feet wide to accommodate maintenance
					vehicles, police vehicles, and EMS vehicles. The facility would have to be ADA accessible probably through the use elevators on each
					side of the river in combination with a zig-zag path or some other means for citizens with disabilities to reach the crest. Due to the
			I have reviewed all the options and believe that alternative 5G is the best option. I also feel very strongly		length of the approaches (approximately one-half mile), it is likely that skateboarders, those with rollerblades, some cyclists, etc. will
			that the bridge needs to include a dedicated bicycle and pedestrian lane going from lake Charles to Westlake		exceed safe speeds when descending from the crest potentially resulting in serious injury crashes. Security, particularly at night, will be
			and back on the south side of the bridge. Lake Charles is in the process of a comprehensive bike plan and the		an issue. The facility will need frequent trash pickup and periodic cleaning. A bicycle/pedestrian path attached to a new I-10 bridge
			mayor of Westlake is very progressive and is developing an area north of the Isle of Capri. These two cities		would be expensive to build and very expensive to operate. These costs would have to be reflected in the toll rates and at the end of
			need to be connected for bicycles and all pedestrians. I also believe we need to spend some extra money to		the public private partnership, responsibility for the operating and maintenance costs, including security, would transfer to the cities of
422		2024 02 21	make the bridge iconic which should include some form of pistols and lighting. Thank you for your	D. 1. 1. 5 1	Lake Charles and West Lake as DOTD does not maintain or operate multiuse paths.
122	Lee Boyer	2021-03-31	consideration,	Project Email	The goal is to provide a very attractive, but also affordable, bridge.

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
			I am not a resident of Calcasieu parish, but I am a Louisiana citizen (and therefore taxpayer) and a concerned		
			citizen of the planet.		
			Regarding the bridge project:		
			1. The EDC should have been cleaned up long ago. The clean-up should be the #1 priority since it threatens		
			drinking water supply of the area. I understand that DOTD is not responsible directly for the cleanup, but any		
			efforts on bridges are futile in the absence of an environmental solution. ie, DOTD should defer to LDEQ and		
			citizen environmental groups.		
			2. While "growth" and new construction is often considered a good thing, as the world enters an era of		
			climate catastrophe, any new construction must be considered in light of the future of the planet. New		
			construction involves more cement, which is very greenhouse gas-intensive, and fossil fuel transport. Moreover, encouraging more passenger traffic with easier driving is anti-thetical to a healthy planet. The		There is a lawsuit pending that will determine if the parties responsible for the EDC release will have to pay damages.
			Biden administration is also prioritizing climate considerations for all new construction. The money would be		There is a lawsuit pending that will determine if the parties responsible for the EDC release will have to pay damages. Thank you for your comments.
			better spent on electric vehicle or high-speed transport infrastructure.		3. Special measures will be implemented during work in the area to monitor for the presence of EDC in order to protect workers and
			3. There are very simple ways to redirect traffic, either on 210, or on smaller roads, until the clean-up is		the public from additional exposures. Construction will be phased so that I-10 will remain open to traffic except some lanes may be
123	Marion Freistadt	2021-03-25	completed and the unsafe bridge is either removed or repaired.	Project Email	closed for short durations.
l			At this time we would like to see ALT5G built over the Calcasieu River. It is the only option that doesn't have		
ł			an additional bridge built at Sulphur Ave. We currently deal with delays at the UPRR bridge and at the saltwater barrier. The Sulphur Ave. bridge would be a third location to delay marine traffic in a stretch of		A navigation study is being prepared. Impacts to navigation will be provided in that document, which will be posted online. Your
124	Rob Frye	2021-03-31	river that is less than 3 miles long.	Project Email	preference has been noted.
	11001170	2021 00 01	THE CHECK I COS CHAIT STRINGS TONIS.	r roject zman	
					Thank you for your comments and support. Your preference has been noted. Louisiana Revised Statute 32:263 specifically prohibits the use of any Louisiana interstate highway by pedestrian, bicycles, or non-motorized vehicles. Bicycle and pedestrian accommodations
					may be possible on Alternatives 3A and 3E along the proposed Sulphur Avenue extension including the moveable bridge although a
					reasonable connection to Ryan Street presents some challenges. Even without a legal prohibition, a bicycle/pedestrian path attached
					to a new I-10 bridge is not practical. The path would have approximate one-half mile approaches on each side at a three percent
					grade to reach the crest. It would have to be separated from the traffic lanes by a concrete barrier and it would have to be at least 14
					feet wide to accommodate maintenance vehicles, police vehicles, and EMS vehicles. The facility would have to be ADA accessible
					probably through the use elevators on each side of the river in combination with a zig-zag path or some other means for citizens with
					disabilities to reach the crest. Due to the length of the approaches (approximately one-half mile), it is likely that skateboarders, those
			The Chamber SWLA in Lake Charles has over 1200 members. In late 2017, a Task Force was appointed by the		with rollerblades, some cyclists, etc. will exceed safe speeds when descending from the crest potentially resulting in serious injury
			Chamber Board of Directors to advocate for a plan to build a new 1-10 River Bridge in Lake Charles. The Chamber fully supports LADOT's plan for a new bridge using a P-3 Process, including a reasonable toll. We		crashes. Security, particularly at night, will be an issue. The facility will need frequent trash pickup and periodic cleaning. A bicycle/pedestrian path attached to a new I-10 bridge would be expensive to build and very expensive to operate. These costs would
			believe the SG option is the best as to route and to service Westlake. The Task Force strongly supports an		have to be reflected in the toll rates and at the end of the public private partnership, responsibility for the operating and maintenance
			iconic design as this is the gateway to Lake Charles. One component we strongly support is a Pedestrian/Bike		costs, including security, would transfer to the cities of Lake Charles and West Lake as DOTD does not maintain or operate multiuse
			Pathway. This would connect Westlake and Lake Charles and enhance the development of the Lake Charles		paths.
125	SWLA	2021-04-05	lakefront.	Letter via Email	The goal is to provide a very attractive, but also affordable, bridge.
426		2024 04 05	Loof Allower of C	Post of Food	No. 1 Communication and the
126	Vernon Meyer	2021-04-05	I prefer Alternative 5 G.	Project Email	Your preference has been noted.
					Thank you for your support. Louisiana Revised Statute 32:263 specifically prohibits the use of any Louisiana interstate highway by
					pedestrian, bicycles, or non-motorized vehicles. Bicycle and pedestrian accommodations may be possible on Alternatives 3A and 3E along the proposed Sulphur Avenue extension including the moveable bridge although a reasonable connection to Ryan Street presents
					some challenges. Even without a legal prohibition, a bicycle/pedestrian path attached to a new I-10 bridge is not practical. The path
					would have approximate one-half mile approaches on each side at a three percent grade to reach the crest. It would have to be
					separated from the traffic lanes by a concrete barrier and it would have to be at least 14 feet wide to accommodate maintenance
					vehicles, police vehicles, and EMS vehicles. The facility would have to be ADA accessible probably through the use elevators on each
İ					side of the river in combination with a zig-zag path or some other means for citizens with disabilities to reach the crest. Due to the
					length of the approaches (approximately one-half mile), it is likely that skateboarders, those with rollerblades, some cyclists, etc. will
					exceed safe speeds when descending from the crest potentially resulting in serious injury crashes. Security, particularly at night, will be
			I was unable to attend the public meeting but I wanted to express my support of the new bridge. I am in		an issue. The facility will need frequent trash pickup and periodic cleaning. A bicycle/pedestrian path attached to a new I-10 bridge would be expensive to build and very expensive to operate. These costs would have to be reflected in the toll rates and at the end of
			support of the P3 and tolls if they are necessary. I also think our bridge needs to have an iconic design since		the public private partnership, responsibility for the operating and maintenance costs, including security, would transfer to the cities of
l			it is on our Lakefront and the "entry" to our city. As a cyclist, I would like to see pedestrian and bike lanes on		Lake Charles and West Lake as DOTD does not maintain or operate multiuse paths.
127	Tobie Hodgkins	2021-03-31	the new bridge.	Project Email	The goal is to provide a very attractive, but also affordable, bridge.
				.,	

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
			I am not a resident of Calcasieu parish, but I am a Louisiana citizen (and therefore		
			taxpayer) and a concerned citizen of the planet.		
			Regarding the bridge project:		
			1. The EDC should have been cleaned up long ago. The clean-up should be the #1		
			priority since it threatens drinking water supply of the area. I understand that DOTD is		
			not responsible directly for the cleanup, but any efforts on bridges are futile in the		
			absence of an environmental solution. ie, DOTD should defer to LDEQ and citizen		
			environmental groups.		
			While "growth" and new construction is often considered a good thing, as the world		
			enters an era of climate catastrophe, any new construction must be considered in		
			light of the future of the planet. New construction involves more cement, which is very		
			greenhouse gas-intensive, and fossil fuel transport. Moreover, encouraging more		
			passenger traffic with easier driving is anti-thetical to a healthy planet. The Biden		
			administration is also prioritizing climate considerations for all new construction. The		
			money would be better spent on electric vehicle or high-speed transport		
		March 25th	infrastructure.		1. There is a lawsuit pending that will determine if the parties responsible for the EDC release will have to pay damages.
	Marion Freistadt -	2021, 3:00	There are very simple ways to redirect traffic, either on 210, or on smaller roads,		2. Thank you for your comments.
128	70118	pm	until the clean-up is completed and the unsafe bridge is either removed or repaired.	Project Website	3. Construction will be phased so that I-10 will remain open to traffic except some lanes may be closed for short durations.
		ľ	,	.,	
					Although adding a second bridge on a new location parallel to the existing bridge and using the existing bridge as a one-way couplet
					could address some of the horizontal geometric deficiencies, the steep vertical grade and bridge structure deficiencies could not be
		January 6th	Why not build a second, parallel bridge like they did in Bridge City TX (Rainbow Bridge) and make each bridge		addressed. Adding a second bridge and still having to maintain a bridge which is past its service life and does not meet all design criteria
	Patricia Davison -	2021, 1:58	a one-way eastbound and westbound bridge? That would half the number of travelers on the old bridge,		and would not meet the purpose and need for the project. Continuing to pay the costs of maintenance and operation (O&M) for the
129	70663	pm	allowing it more safe, functional life.	Project Website	old bridge on top of the O&M costs for a new one would not be a prudent use of public funds.
			Your website doesn't allow me or others to enter multiple work locations. I just went back on and entered		
		January 6th	my email for replies, and entered my other employer Sowela (first pass I'd entered employer USPS Drew		
	Patricia Davison -	2021, 2:06	Station on Lake St). I hope my first survey saved my comment that no tolls should be required of local		
130	70663	pm	residents; interstate/intrastate travelers should pay.	Project Website	Your comments have been received and recorded in the meeting record published online at www.i10lakecharles.com.
	James Whittington -	January 18th			
131	70669	2021. 9:10 am	We also need to raise the railroad where it crosses the river	Project Website	The railroad bridge is the responsibility of Union Pacific Railroad.
131	70003	2021, 5:10 dill	We also need to raise the rainoud where it crosses the river	Troject Website	The fall odd bridge is the responsionity of origin in define train odd.
					LADOTD and the State of Louisiana have allocated \$85 million in state funding to match federal funds for construction of the proposed
		January 20th	Options 3 A and 3 E call for a second bridge when we have needed 1 bridge for over 30 years and that		project. In addition to that, LADOTD will make a contribution from its annual budget (gas tax), and has already applied forand will
		2021, 9:08	doesn't seem to be able to get done. What about highway taxes collected for years and haven't been		continue to seek-grant funds and use of infrastructure funding from federal programs such as BUILD (now RAISE), INFRA, and
132	Lattie Polk - 70663	pm	applied properly	Project Website	American Rescue Act.
			I recognize the bridge needs to be replaced and updated, but I am opposed to tolls as a means of financing		
			the build. We have enough trucks that drive over the bridge that can be charged a tax to cover initial build		
			and continued maintenance. The municipalities can construct a passive tag system that monitors all trucks		
			coming and going through infrared technology and bill the employer at the end of each quarter. The tax the		Toll facilities around the country typically charge a higher rate for trucks than for cars. The rate schedule for the bridge will be
			employer incurred will be passed to its customers and clients as it does all other costs incurred while doing		determined in coordination with the Public Private Partnership (P3) developers. Other funding sources have been and are being
	L	January 25th	business. The parish can place an ordinance for a small increase in gas taxes for 10 - 20 years, without being		pursued, but the P3 model has been determined to be the most reliable form of filling the funding gaps. Tolls will only cover a portion
	Gayle Sledge -	2021, 1:39	renewed, to pay for the increased cost to repair the bridge. The toxic remediation should be charged to all		of the costs to design, build, operate, and maintain the proposed project but without this revenue, there is no incentive for the private
133	70601	pm	of the pollutant companies along the Calcasieu River for clean up.	Project Website	sector to participate in the (P3). Remediation measures to address hazardous waste and spills are required by LDEQ.
					Although adding a second bridge on a new location parallel to the existing bridge and using the existing bridge as a one-way couplet
					could address some of the horizontal geometric deficiencies, the steep vertical grade and bridge structure deficiencies could not be
		March 22nd	I think someone should take a trip to Texas and look at the sunshine bridge and the new bridge they built.		addressed. Adding a second bridge and still having to maintain a bridge which is past its service life and does not meet all design
	Jason Poynter -	2021, 1:57	They rehabbed the sunshine bridge and made it one way and the newer bridge runs traffic the opposite		criteria, would not meet the purpose and need for the project. Continuing to pay the costs of maintenance and operation (O&M) for
134	70663	pm	direction.	Project Website	the old bridge on top of the O&M costs for a new one would not be a prudent use of public funds.
		10		,	· · · · · · · · · · · · · · · · · · ·

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
			Comment for the DOTD online public ZOOM meeting to be held March 25, 2021 Submitted by Michael		
			Tritico, P.O. Box 233, Longville, LA 70652 The most sensible alternative is not presented and may not have		
			been considered. That alternative would add about 5 miles to the trip between Welsh and Vinton but it		
			would completely bypass the urban/I-10 Bridge bottleneck that exists from Chloe to West Sulphur. Here is		
			a description of a practical flat route that would make it unnecessary to build a new bridge in the EDC-		
			contaminated zone: The "North Bypass" would take off to from just west of Welsh, go northwest about 9		
			miles to a cloverleaf at U.S. Highway 165 south of Fenton, then turn west, go along the existing high ground		
			to avoid as much wetland disruption as possible, cross with only slightly-elevated bridges Bayou Serpent and		
			the Calcasieu River, (a distance for this segment of about 11 miles), then proceed another 6 miles to a		
			cloverleaf at U.S. Highway 171 south of Gillis, proceed westward another 5 miles to again, a low-level		
			crossing of Hickory Branch and then go four miles to a cloverleaf at Louisiana State Highway 27 north of		
			Sulphur, then turn southwesterly for about 15 miles to rejoin Interstate 10 just east of Vinton. That is a 50 mile route, about 5 miles longer than the existing route between one end of the bottleneck to the other.		
			The enhancement of safety for motorists, the reduction in energy wastage on steep slopes and from stop		
			and go situations all make a flat bypass north of the current route more logical than are the alternatives		
			presented even given the extra 5 miles of travel required. Furthermore, the route and the cloverleaves		Early in the process, the Project Team considered alternative realignments, but they were extremely expensive. A far northern bypass
		March 22nd	described above would simplify trips to and from places like Alexandria, Kinder, DeRidder, Houston, and New		may be be considered for the area in the long-term, but it would only serve through traffic and the surrounding small towns and
			Orleans by removing the need to navigate the Calcasieu metropolitan area. A much more efficient highway		villages. The I-10 Calcasieu Bridge project must deal with an aging bridge that needs to be replaced and the aging interstate that serves
135		pm	system will also diminish greenhouse gases.	Project Website	Lake Charles, Westlake, and Sulphur residents and businesses.
				· · · · · · · · · · · · · · · · · · ·	
			In favor of the bridge - the addition of a loop to Sulphur Avenue would be devastating - we are minutes from		
			family, friends and work. Total privacy yet filled with beautiful sunrises, entertainment provided by		
			fishermen, families boating fun, access to deep water, boat slip, our scenic bridge - we totally enjoy our little		
		2021, 5:49	piece of heaven. With work near by and dependent children just 10 minutes away we are always there when		
136	70669	pm	needed.	Project Website	Thank you for your support. Your preference has been noted.
	Danielle Richardson	March 25th			
137	70669	2021, 9:41 nm	I like Alternative 3A and 3E.	Project Website	Your preference has been noted.
157	70003	piii	The Alternative SA and SE.	Froject Website	Tour preference has been noted.
		March 26th			
138	Pete Tucker - 70526	2021, 6:50 am	Alternative 5G	Project Website	Your preference has been noted.
		March 26th		•	
		2021, 12:34			
139	70601	pm	The i10calcasieu website is fantastic. Excellent work! Alsoplease build this bridge in my lifetime	Project Website	Thank you for your comments and support.
			The state of the s		
		March 26th 2021, 1:51	This work is way overdue and the cost increases annually with each delay. All federal and state options for financing should be on the table with a toll being the last choice. Whatever it takes, this project should be		Other funding sources have been and are being pursued, but the Public Private Partnership (P3) model has been determined to be the
140	James Rock - 70605		on a fast track in the interest of safety of the public, image of the community and cost.	Project Website	most reliable form of filling the funding gaps. Tolls will only cover a portion of the costs to design, build, operate, and maintain the proposed project but without this revenue, there is no incentive for the private sector to participate in the P3.
140	James Nock - 70005	piii	on a last track in the interest of safety of the public, image of the community and cost.	Froject Website	proposed project but without this revenue, there is no intentive for the private sector to participate in the F3.
					Thank you for your support. The design presented provides a basic bridge form that will cost the least to build and maintain while
					meeting current design criteria. A traffic study has confirmed that the proposed three lanes in each direction will accommodate future
					traffic volumes in 2042. A signature type bridge or signature features will be considered as part of the Context Sensitive Solution (CSS)
					process. However, each additional feature or amenity will raise costs that would have to be offset with higher toll rates. Additional
					information will be published on the website. The goal is to provide a very attractive, but also affordable bridge.
					Regarding bicycle/pedestrian accommodations, Louisiana Revised Statute 32:263 specifically prohibits the use of any Louisiana
					interstate highway by pedestrian, bicycles, or non-motorized vehicles. Bicycle and pedestrian accommodations may be possible on
					Alternatives 3A and 3E along the proposed Sulphur Avenue extension including the moveable bridge although a reasonable connection
					to Ryan Street presents some challenges. Even without a legal prohibition, a bicycle/pedestrian path attached to a new I-10 bridge is
					not practical. The path would have approximate one-half mile approaches on each side at a three percent grade to reach the crest. It
					would have to be separated from the traffic lanes by a concrete barrier and it would have to be at least 14 feet wide to accommodate
					maintenance vehicles, police vehicles, and EMS vehicles. The facility would have to be ADA accessible probably through the use
					elevators on each side of the river in combination with a zig-zag path or some other means for citizens with disabilities to reach the
			This project is way overdue and the safety of the community, the continuity of business and industry and the		crest. Due to the length of the approaches (approximately one-half mile), it is likely that skateboarders, those with rollerblades, some cyclists, etc. will exceed safe speeds when descending from the crest potentially resulting in serious injury crashes. Security,
			annual increase in cost are reasons it should be constructed as soon as possible. Given that it could have a		particularly at night, will be an issue. The facility will need frequent trash pickup and periodic cleaning. A bicycle/pedestrian path
		March 26th	life of up to 100 years, it should be an iconic structure that travelers will remember and enjoy and it should		attached to a new I-10 bridge would be expensive to build and very expensive to operate. These costs would have to be reflected in
			be equipped with sufficient lanes and shoulders to accommodate anticipated future traffic and also include a		the toll rates and at the end of the public private partnership, responsibility for the operating and maintenance costs, including
141	James Rock - 70605	pm	walking/biking provision for inclusion in future health and quality of life initiatives.	Project Website	security, would transfer to the cities of Lake Charles and West Lake as DOTD does not maintain or operate multiuse paths.
		March 26th	O O O C C C C C C C C C C C C C C C C C	,	
	Cameron Fultz -	2021, 2:26			
142	70605	pm	Alternative 5G	Project Website	Thank you for your participation in the survey. Your preference has been noted.
				•	<u>, , , , , , , , , , , , , , , , , , , </u>

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
		March 30th 2021, 10:09			Mitigation to offset impacts to the affected communities during construction will include keeping access to and travel on I-10 available at all times. Detours for the period when Sampson Street is closed to traffic will be clearly marked and managed as needed. Construction noise and emissions from equipment will be minimized using best management practices and good maintenance. Night
143	Paul Bellow - 70615	am	Mitigate impact to the affected community during construction.	Project Website	time work in the vicinity of residences will be avoided to the extent practicable.
144	Andy Derouen - 70611	March 26th	Firstly, thank you so much for your dedication to this project. The options, in my opinion, are well considered. As a daily commuter to Lake Charles from Moss Bluff, I must say that any option that provides an additional method of transport across the river is a huge bonus. Am I understanding this correctly? With Alternatives 3A and 3E, I wouldn't even have to engage Interstate 10 at all? That's added convenience for which even I, as a local, would be willing to pay tolls for an extended period of time. I think support for the "cheapest" option (Alt. 5G) is shortsighted, because at the end of the day, we would have spent almost \$1 Billion, if not more, and we'd still only have two means of crossing the river. 5G is not as advantageous for local commuters as the other options.	Project Website	Thank you for your comments and support. If Alternative 3A or 3E is built, you will still have to travel on I-10 to Lake Charles. From North Lakefront Drive you will be able to access the Sulphur Avenue Extension, cross the bridge over the Calcasieu River, and arrive in Westlake at the intersection of Sulphur Avenue and Sampson Street. Your preference has been noted.
145	Paul Boudreaux - 70605		If the bridge becomes a toll bridge, all truck and out of town traffic will take 210. This will cause daily traffic issues and local people will always have to take alternate routes. Toll is not the answer, I drove trucks for 10 years, everyone just goes around the toll road or bridge. Local people will be the only ones with a toll pass because LA doesn't have a network of toll roads. So essentially local people will be the majority of ones paying for it. That's not fair, considering the high volume of interstate traffic that causes wear to the roads and bridges.	Project Website	All of the toll facilities will be electronic and will not require toll booths where drivers have to stop and pay. A gantry will be placed over the roadway and equipped with a toll tag reader and with a camera that will photograph the license plate of vehicles without toll tags so they can be billed by mail. Agreements with other states allows for tolls to be collected on behalf of Louisiana during vehicle registration and other interactions.
146	Justin Mouser - 70611		No one has explained why we can't have a flat low level bridge. You could make it a swinging open bridge for the 3 times a year something that large ever needs to go under it. You can install light and warning boards miles in advance of the 210 exit to give ample time to take 210 when the bridge opens. Doing that would cost significantly less, be better for the environment, and will no hamper views of Lake Charles. Then, you could focus the Westlake efforts going over the interstate and rail tracks, at a much lower cost. The flat bridge approach works amazingly in this scenario; given all the restraints facing this project.	Project Website	Current records of the UPRR bridge show that the bridge opens approximately 1250 times per year. This kind of disruption to interstate traffic would increase traffic congestion and reduce mobility, two issues that the proposed project is designed to address.
147	Cornell Marshall - 70669	March 26th 2021, 10:09 pm	As a whole the residents should file suit/explore legal opportunities against DOT and local official signing off on project, too many projects going on in La to have SW LA residents pay tolls while other areas are getting projects and funding	Project Website	LADOTD and the State of Louisiana have allocated \$85 million in state funding to match federal funds for construction of the proposed project. In addition to that, LADOTD will make a contribution from its annual budget (gas tax), and has already applied for—and will continue to seek—grant funds and use of infrastructure funding from federal programs such as BUILD (now RAISE), INFRA, and American Rescue Act. However, competition and evolving regulations make these sources unpredictable. For this reason, the Project Team has turned to the Public-Private Partnership (P3) model as the most reliable path forward.
148	Nelson Lebert - 70669	March 28th	Why should we give up waiting on a train for waiting on a Tug boat? I live on the river near the proposed 3A and 3E crossing, we get 4 are5 Tugs with barges per day the Tugs with two or more barges leave all but one barge behind and make several trips up river. they can not make the sharp turns with two barges. Also the increase traffic in front of school and the noise that goes with it.	Project Website	Thank you for your comments. Your preference has been noted. Current records of the UPRR bridge show that the bridge opens approximately 1250 times per year.
149	Mary Bellon - 70648	pm March 30th	I think that we should find the absolute best bridge engineer and take his/her advice, however I know that it is absolutely insane to build a bridge with less lanes of traffic that what is feeding into it. I see this all the time and have to say to myself this is not rocket science, what do they think is going to happen when they do this. You also should put a shoulder safe enough for a wreck to get over onto. I know this is more money but lives depend on it and if you are going to do it just do it right.	Project Website	The proposed project would provide a continuous section of three lanes in each direction including the proposed bridge. The project will meet all current design criteria including shoulders wide enough to use for emergency stops from I-210 east end to I-210 west end.
	Paul Bellow - 70615	March 30th 2021, 10:18		Project Website	Mitigation to offset impacts to the affected communities during construction will include keeping access to and travel on I-10 available at all times. Detours for the period when Sampson Street is closed to traffic will be clearly marked and managed as needed. Construction noise and emissions from equipment will be minimized using best management practices and good maintenance. Night
151	Paul Bellow - 70615	am March 30th	Minimize impact to affected community during construction	Project Website	time work in the vicinity of residences will be avoided to the extent practicable.
		2021, 12:15	I feel the committee knows much better than me the best plan to go with so let's get this show on the road.		
152	70607	pm March 30th	Choose the best one and get going.	Project Website	Thank you for your comments and your support.
		2021, 1:12			
153 154		March 30th	Just build it already I think 3E is the best option. The secondary bridge to Westlake would eliminate a lot of the plant traffic on the main bridge and avoid a lot of the slowdowns because of the sharp turn to exit the bridge. It also has a less angled route than 3A, so it wouldn't require as sharp a turn on the secondary bridge itself, reducing further slowdowns there. My only concern is that the secondary bridge would only reduce traffic one way, since the eastbound on-ramp in Westlake is still a very sharp turn requiring very slow speeds, so unless it also has an entrance-only lane, traffic will still significantly back up on the west side of the bridge as people merge from that tight ramp.	Project Website Project Website	Thank you for your comment. Thank you for your comments. Your preference has been noted. Coordination with Westlake officials, businesses, and residences is ongoing.

Public Meeting #4 Comments/Questions Responses

ID	Name	Date	Question / Comment	Source	Response
	Nathaniel Langford -	2021, 10:24	I'm still trying to wrap my head around the elevated Sampson interchange in 5G. Does Samson Street have access to the service roads on the south side of I-10 and vice versus? Also I continue to believe lowering the bridge is a bad idea. It needs to be at a height that will not negatively impact the businesses on the river north of the bridge.	Project Website	Alternative 5G would raise Sampson Street to connect with an elevated interchange that would allow movement to and from I-10 in four directions. The service roads on the south side would be connected to the bridge with elevated ramps. Access and travel patterns on the south side service roads and Isle of Capri Boulevard to Miller Avenue would not be affected.
	Delana Carter -		PLEASE DONT MAKE THIS DANGEROUS FOR US. IF YOU NEED MY PROPERTY I AM WILLING TO RELOCATE BUT I DONT WANT TO BE CLOSER TO THE HWY	Project Website	A brochure about acquisition of right of way and relocation assistance is available in the Project Library on the website at www.i10lakecharles.com.
157	Debbie	2021-04-08	This is Debbie. My email is silkyhair54@gmail.com. I'm in Lake Charles, Louisiana and I believe the alternative 5G, as in giraffe, is the one that I support most on the rebuilding of the I-10 bridge. Thank you	Project Phone	Your preference has been noted.
	Lake Charles MPO				Louisiana Revised Statute 32:263 specifically prohibits the use of any Louisiana interstate highway by pedestrian, bicycles, or non-motorized vehicles. Bicycle and pedestrian accommodations may be possible on Alternatives 3A and 3E along the proposed Sulphur Avenue extension including the moveable bridge although a reasonable connection to Ryan Street presents some challenges. Even without a legal prohibition, a bicycle/pedestrian path attached to a new I-10 bridge is not practical. The path would have approximate one-half mile approaches on each side at a three percent grade to reach the crest. It would have to be separated from the traffic lanes by a concrete barrier and it would have to be at least 14 feet wide to accommodate maintenance vehicles, police vehicles, and EMS vehicles. The facility would have to be ADA accessible probably through the use elevators on each side of the river in combination with a zig-zag path or some other means for citizens with disabilities to reach the crest. Due to the length of the approaches (approximately one-half mile), it is likely that skateboarders, those with rollerblades, some cyclists, etc. will exceed safe speeds when descending from the crest potentially resulting in serious injury crashes. Security, particularly at night, will be an issue. The facility will need frequent trash pickup and periodic cleaning. A bicycle/pedestrian path attached to a new I-10 bridge would be expensive to build and very expensive to operate. These costs would have to be reflected in the toll rates and at the end of the public private partnership, responsibility for the operating and maintenance costs, including security, would transfer to the cities of Lake Charles and West Lake as
	Technical Advisory				responsibility for the operating and maintenance costs, including security, would transfer to the cities of Lake Charles and West Lake as DOTD does not maintain or operate multiuse paths.
158	Committee	2021-03-10	See Attached Presentation	Mtg	The goal is to provide a very attractive, but also affordable, bridge.

ATTACHMENT J

TECHNICAL ADVISORY COMMITTEE (TAC) MEETING ATTENDEES LIST

State Project Number: H.003931

March 10, 2021



Name	Affiliation		
TAC Memebers (Quorum)			
Brian Duplichan	City of Westlake		
Carlos McCloud	Federal Highway Administration (Non-Voting Member)		
Casey Lewis	LADOTD (Non-Voting Member)		
Dawn Sholmire	LADOTD (Non-Voting Member)		
Heath Allen	Lake Charles Airport		
John Bruce	Calcasieu Parish		
John Cardone	City of Lake Charles		
Mary Bass Fontenot	Calcasieu Parish School Board		
Nick Pestello	Port of Lake Charles (Vice Chair)		
Seth Woods	LADOTD - District 7		
Stacey Dowden	City of Sulphur (Chair)		
	TPC Members		
Wes Crain	Chair		
Imperial C	Clacasieu Regional Planning & Development Commission		
Abby Coyle-Richard	IMCAL		
Brennan Marque	ICRPDC		
Deborah Fanagan	ICRPDC		
Edward Campany	ICRPDC		
Erin Whidden	ICRPDC		
Jamie Gaines	ICRPDC		
Michael Bernard	ICRPDC		
Michael Hollier	ICRPDC		
Paige Simpson	ICRPDC		
Robert Smith	ICRPDC		
Steve Jiles	ICRPDC		
Walter Council	ICRPDC		

TECHNICAL ADVISORY COMMITTEE (TAC) MEETING ATTENDEES LIST

State Project Number: H.003931

March 10, 2021



Name	Affiliation
	Guests
Angela Murrell	LADOTD
Ben Magallon	Alliance Transportation Group
Cade McClemore	Calcasieu Parish
Cindi Richard	City of Lake Charles
Clair Marceaxu	IMCAL Board Chair
David Cagnolatti	Phillips 66
Joe Umeozulu	LADOTD
Jon Amanns	Bike/Ped Advocate
Kelli Van Norman	City of Lake Charles
Lee Boyer	Bike/Ped Advocate
Megan Hartman	Phillips 66
Mike Huber	City of Lake Charles
Tammy York	LADOTD - District 7



Calcasieu River Bike/Ped Crossing Recommendation



- 1) Utilize existing I 10 Bridge
 - Pro Existing Structure
 - Cons Cost to maintain
 - Steepness (bridge Height 135 FT)
 - ADA challenge



- 1) Utilize existing I 10 Bridge
- 2) Include in New Bridge
 - Pro Less steep (new bridge is 73 Ft high)
 - Maintenance low cost
 - : new and part of bridge
 - Con Initial cost



- 1) Utilize existing I 10 Bridge
- 2) Include in New Bridge Another Fact: currently 22 Interstate Bridges with Bike/Ped Paths









- 1) Utilize existing I 10 Bridge
- 2) Include in New Bridge
- 3) Bike Ferry
 - Pro Low initial cost
 - Con Daily Operational costs: Labor, Fuel, Maint.
 - Limited hours of operation



- 1) Utilize existing I 10 Bridge
- 2) Include in New Bridge
- 3) Bike Ferry
- 4) New Bike/Ped Bridge by itself
 - Con location south of the I-210 Bridge would have to be same height as I 210 bridge
 - location north of 1-10 bridge gets too far away from Lake Charles plus getting into wetlands
 - high initial cost



Build the Bike/Ped path on the new I 10 bridge



Build the Bike/Ped path on the new I 10 bridge

Key Points: I 10 Task Force uniformly recommend this approach



Build the Bike/Ped path on the new I 10 bridge

Key Points: Recommend installing 2-way lane on southside of bridge





Build the Bike/Ped path on the new I 10 bridge

Key Points: Remove old I 10 Bridge

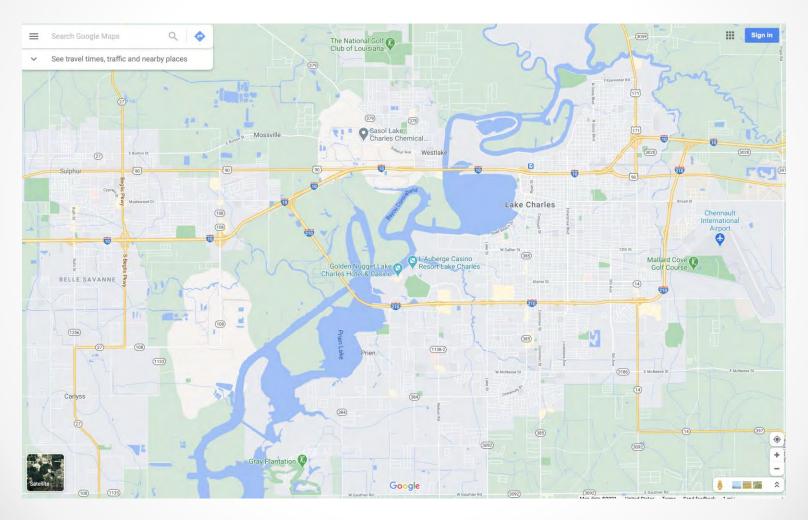


Build the Bike/Ped path on the new I 10 bridge

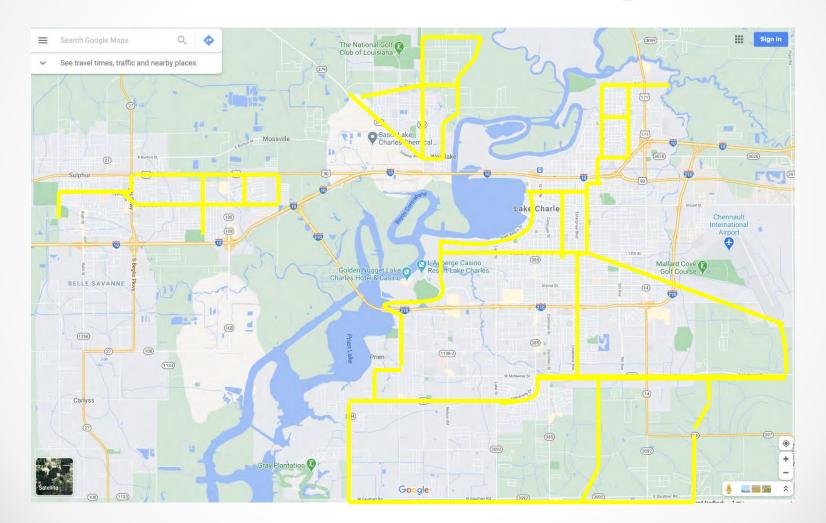
Key Points: Educate Public and Officials on value
Bike crossing to west side of river

Google Map View of MPO

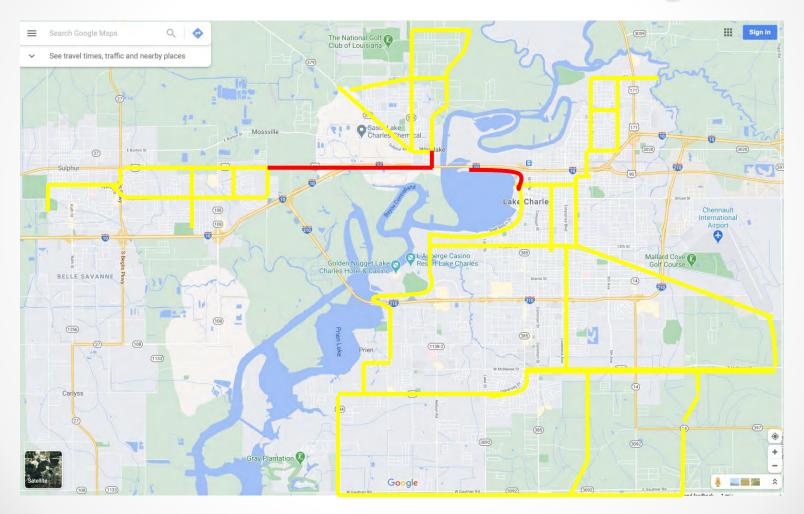




MPO with Tentative Bike Path Proposal'



MPO w/ Links to Bike Paths From Bridge



NOTICE OF PUBLIC MEETING

Notice is hereby given that the Louisiana Department of Transportation and Development (LADOTD) and the Federal Highway Administration (FHWA) will conduct a live public meeting online via ZOOM.

State Project No. H.003931
Federal Aid Project No. BR-10-1(212)29
I-10 Calcasieu River Bridge
(I-10/I-210 West End to I-10/I-210 East End)
Calcasieu Parish, Louisiana

The live meeting will be held using ZOOM on date and time:

THURSDAY

March 25, 2021 5:30 p.m. to 7:00 p.m.

Go to

www.i10lakecharles.com for a ZOOM link to attend the meeting

If you do not have access to a computer or if you prefer, you may join by phone at 646-876-9923, Passcode 459770 (audio only).

A live introduction will be followed by a recorded presentation. After the presentation, questions and comments will be accepted through the chat feature and the project team will respond in the order received. Comments may also be submitted through the website at www.i10lakecharles.com, the project email at Calcasieubridge@hntb.com, or by leaving a message at 225-366-9645. These comments will be collected, and responses will be provided in the meeting record. You may also write to us at

I-10 Calcasieu River Bridge Project c/o HNTB Corporation Suite 640 10000 Perkins Rowe Baton Rouge, LA 70810

Three alternatives have been selected for detailed evaluation in the Environmental Impact Statement. Alternative 3A includes a long-span bridge, an extension of Sulphur Avenue east across the river via a moveable bridge, and a partial interchange at North Lakeshore. Similarly, Alternative 3E includes a long-span bridge and an extension of Sulphur Avenue east across the river to a full interchange at North Lakeshore. Alternative 5G includes a full interchange at Sampson Street elevated above the existing Sampson Street and two railroad crossings. This alternative would not extend Sulphur Avenue, but it would require moving two railroad spur tracks that cross the I-10 Service Road and Isle of Capri Boulevard. Common to all alternatives are improvements at PPG Drive, North Lakeshore and Veterans Memorial, Enterprise Boulevard, and the widening of I-10 to three through lanes in each direction.

The purpose of the public meeting is to solicit comments from the public about the alternatives and proposed improvements, benefits and impacts of each one, and implementation strategies such as tolling and public-private partnerships.

If you require special assistance due to a disability or need an interpreter to attend the videoconference meeting, please contact us at 225-366-9645, Calcasieubridge@hntb.com or through www.i10lakecharles.com at least 5 days in advance so that we can provide accommodations.



CSS / D Meeting #1 Summary Report January 2021



Context Sensitive Solutions and Design (CSS/D) Community Meeting #1 Summary

I-10 Calcasieu River Bridge Improvements (I-10/I-210 West End to I-10/I-210 East End) Calcasieu Parish, Louisiana

State Project Number: H.003931

February 19, 2021





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ATTACHMENT

CSS/D Community Meeting 1 Presentation

1.0 INTRODUCTION

The Louisiana Department of Transportation and Development (LADOTD), in conjunction with the Federal Highway Administration (FHWA), are preparing an Environmental Impact Statement (EIS) for the I-10 Calcasieu River Bridge Project. The proposed project is approximately 9 miles in length and includes alternatives for improvements to I-10 in the Lake Charles region between the I-210 interchanges, including the Calcasieu River Bridge (see **Figure 1**).

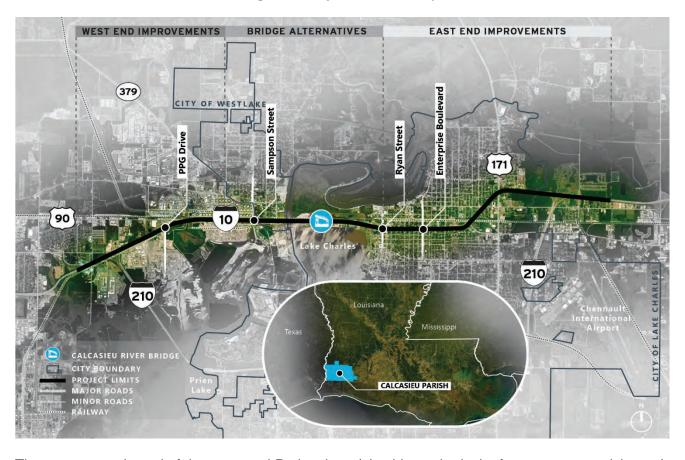


Figure 1: Project Location Map

The purpose and need of the proposed Project is to (a) address the lack of system connectivity on I-10; (b) reduce congestion; (c) address roadway and bridge deficiencies; and (d) address roadway and

bridge safety concerns. The alternatives developed to address the above needs will be evaluated in the EIS. An EIS studies a range of reasonable alternatives, demonstrates compliance with environmental laws, and provides a means for public and agency input into the decision-making process.

The following document summarizes input obtained as part of the first Context Sensitive Solutions and Design (CSS/D) community meeting held virtually on January 7, 2021. The purpose of this community meeting was to obtain input from stakeholders on functional things they want considered for the project such as sidewalks or other aesthetic features.

What is CSS/D?

Context Sensitive Solutions and Design (CSS/D) is a collaborative, interdisciplinary decision-making process and design approach that involves all stakeholders to develop a transportation facility that fits its physical setting.

Source: FHWA

The meeting format was as follows:

- · Instructions for the virtual meeting;
- Introduction of meeting participants (see Table 1);
- · Pre-recorded meeting presentation (see Attachment); and
- Input/questions and answer period (see Section 2.0).

Table 1: CSS/D Community Meeting #1 Participants

Participant Name	Participant Organization	Participant Title
Noel Ardoin	LADOTD	Environmental Section
Jason Boaz	Isle of Capri Casino	Director of Facilities
Tammy Broussard	Meyer and Associates/Westlake	90
John Bruce	Calcasieu Parish Police Jury	Civil Engineer
Walter Council	Imperial Calcasieu Regional Planning and Development Commission (IMCAL)	Transportation Planner
Wes Crain	Calcasieu Parish Police Jury	Director of Planning/Development and Chairman of the MPO
Brandi Crawford	HNTB	Project Team
Darbone Fitzgerald	African American Chamber of Commerce of Lake Charles	President
Scott Dougherty	Lake Charles Police	Traffic Supervisor
Stacy Dowden	City of Sulphur	Public Works Director
Don Duberville	LADOTD	District Administrator Lake Charles
Kyle Edmiston	Southwest Louisiana Convention and Visitors Bureau	President and CEO
Jamie Gaines	IMCAL	Planner
Alberto Galan	Calcasieu Parish Police Jury	Assistant to the Administrator
Robert "Bob" Hardey	City of Westlake	Mayor
Rick Hathaway	HNTB	Project Team
Michael Hollier	IMCAL	Director
Donald Hyatt	Isle of Capri Casino	Facilities Manager
Bryan Jones	HNTB	Project Team
Eric Kalivoda	LADOTD	Deputy Secretary
Tom Kramer	HNTB	Project Team
Lena McArthur	West Calcasieu Chamber of Commerce	Executive Director
Lynn Maloney-Mujica	HNTB	Project Team
Denise Rau	I-10 Calcasieu River Bridge Task Force	Member
Jim Rock	I-10 Calcasieu River Bridge Task Force	Member
Jeanne Rogers	Isle of Capri Casino	Executive Assistant to the General Manager Jeff Favre
Jo Schexneider	Calcasieu Historical Preservation Society	
George Swift	Southwest Louisiana Economic Development Alliance	President/CEO
Joachim Umeozulu	LADOTD	Project Manager
Alan Wainwright	Calcasieu Parish Police Jury	Director of Public Works
Seth Woods	LADOTD	Assistant Administrator of Engineering Lake Charles
Bart Yakupzack	I-10 Calcasieu River Bridge Task Force	Member

2.0 COMMENTS

Meeting attendees were given the opportunity to ask questions/provide comments subsequent to the pre-recorded presentation and could also provide comments via the project phone number, email, website, or mail through January 18, 2021, when the comment period ended. All comments/questions were received as part of the January 7, 2020 community meeting. **Table 2** summarizes the comments received by name, organization, title, and provides a response from LADOTD.

Responses
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Summary
Comments
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Table

		lable 2: Collin	Table 2: Comments Summary and Responses	
Name (Last, First)	Organization	Title	Comment/Question	Response
Dougherty, Scott	Lake Charles Police	Traffic Supervisor	What will the proposed new corridor look like near Opelousas Street?	Vertical and horizontal alignments have been developed for three build alternatives which can be viewed using an interactive map at www.l-10LakeCharles.com . This includes the roadway design near Opelousas Street.
Rock, Jim	I-10 Calcasieu River Bridge Task Force	Member	Is there a fourth public meeting scheduled for the project?	A fourth public meeting is currently scheduled for February 2021.
Kalivoda, Eric	LADOTD	Deputy Secretary	Question directed to Sergeant Scott Dougherty with the Lake Charles Police. The section of the proposed project from Ryan Street to I-210 East would be primarily elevated. What would be the best treatment option for the underneath sections of the elevated interstate from a security standpoint?	Sergeant Dougherty stated that faux walls would be best and that lighting is important under elevated structures.
Crain, Wes	Calcasieu Parish Police Jury	Director of Planning and Development and Chairman of the MPO	Is a suspension bridge still being considered for the project? Because the bridge is going to be there for a long time, an iconic bridge design is desired.	Bridge design and aesthetics are of critical importance to LADOTD. A significant portion of the project will be funded by toll revenues. Accordingly, the bridge's design will be necessarily based on the public's opinion on toll prices and the ability for the bridge to be affordable for everyone. LADOTD is committed to designing an aesthetically beautiful bridge for the community, regardless of the bridge type. A fixed bridge with accent lighting so that the bridge can be seen at night is one example. Different concepts of bridge design with varying costs will be generated for the public to view and consider as the project moves through the development process.

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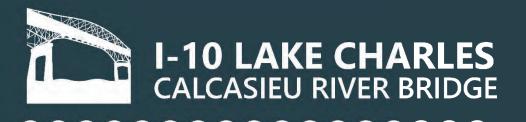
		lable 2: Comm	Table 2: Comments Summary and Responses	
Name (Last, First)	Organization	Title	Comment/Question	Response
Unknown (Comment provided via chat box)	-		There are a number of industrial pipelines and pipe racks along the project corridor. Any routing would need to allow access to these pipelines for continued maintenance.	The Project Team is considering pipeline access in the development of alternatives.
Bruce, John	Calcasieu Parish Police Jury	Civil Engineer	Is an observation platform near the crest of the bridge that would be accessible to pedestrians feasible and practicable?	If your question relates to an overlook on the interstate bridge, an observation platform accessible to pedestrians and bicyclists is not practicable as pedestrians and bicyclists are not allowed on interstates in Louisiana.
Umeozulu, Joe	LADOTD	Project Manager	Provided clarification on the typical section of Alternative 5G, which includes outside auxiliary lanes that would allow the motorist to drive from Westlake to Lake Charles without getting into the through lane traffic on the I-10 main lanes.	Comment noted.
Fitzgerald, Darbone	African American Chamber of Commerce of Lake Charles	President	Was it ever considered to elevate the railroad crossing over the river along with the I-10 Calcasieu River Bridge?	Elevating the railroad is not practicable from an engineering standpoint. Railroads are different from highways in that railroads require a much longer approach to elevate the railroad at the same height as a roadway bridge. Furthermore, elevating the railroad would be a substantially expensive endeavor. LADOTD would not be responsible for paying for elevating the railroad because the railroad is a privately-owned company. It would be the responsibility of the railroad company to pay for an elevated railroad structure.

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		lable z. Colliil	Table 2: Comments Summary and Responses	
Name (Last, First)	Organization	Title	Comment/Question	Response
Fitzgerald, Darbone	African American Chamber of Commerce of Lake Charles	President	Who would maintain the toll booths?	The Public Private Partnership (P3) selected for the project, which is a private company, will implement toll operations. Tolling will be all electronic and motorists will either utilize a pre-paid toll tag or a picture will be taken of their license plate and they will subsequently receive a bill in the mail. There will be no toll booths associated with the project.
Schexneider, Jo	Calcasieu Historical Preservation Society	ı	Will the project affect the cemetery near Banks and Church Street?	No cemeteries would be impacted by the project.
Maloney-Mujica, Lynn	HNTB	Project Team	Question directed to Mr. Darbone Fitzgerald with the African American Chamber of Commerce of Lake Charles. The Project Team is interested in coordinating with the businesses on the north side of the bridge that could be affected by the bridge height. The Project Team is currently seeking input through an electronic survey to determine who is operating vessels on the north side of the bridge. The Project Team is requesting Mr. Fitzgerald's assistance with facilitating that coordination, as appropriate.	Mr. Fitzgerald noted this request and Ms. Maloney-Mujuca will send a link of the latest navigation study to Mr. Fitzgerald.
Ardoin, Noel	LADOTD	Environmental Section	Reminder to all attendees to feel free to send comments after the meeting during the official comment period, which ends January 18, 2021.	Comment noted.

Table 2: Comments Summary and Responses

	Table 4. Collin		
Organization	Title	Comment/Question	Response
Lake Charles Police	Traffic Supervisor	How will the tolling aspect of the project be implemented?	The P3 selected for the project, which is a private company, will implement toll operations, including billing and violation notifications. Tolling will be all electronic and motorists will either utilize a pre-paid toll tag or a picture will be taken of their license plate and they will subsequently receive a bill in the mail. Violations will not be civilian in nature. If motorists refuse to pay, they will be unable to re-register their vehicles until those toll violations are paid. Louisiana also has reciprocity agreements with some other states such as Texas, so that a motorist cannot re-register their vehicle in Texas unless the toll violations are paid in Louisiana.
	Organization Lake Charles Police	nization harles Traffic	harles Traffic Supervisor



Attachment:

Context Sensitive Solutions and Design (CSS/D) Community Meeting #1 Presentation



I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE

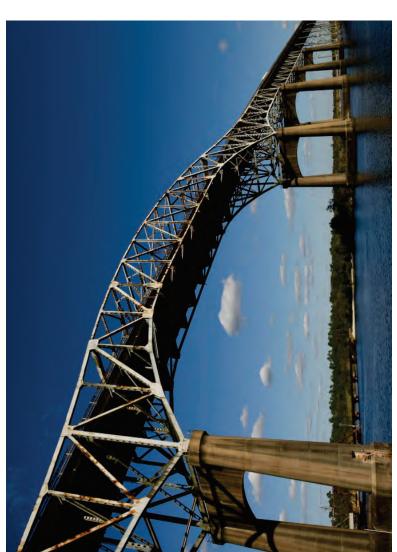
Context Sensitive Solutions and Design CSS/D COMMUNITY MEETING | January 7, 2021



Presentation Outline



- What is CSS/D?
- Examples of CSS/D Solutions
- I-10 Project
- I-10 Corridor Elements
- Input, Questions and Answers



Source: clayhiggins.house.gov



Context Sensitive Solutions and Design (CSS/D)



Context Sensitive Solutions and Design (CSS/D)

A collaborative, interdisciplinary decision-making process and design approach that involves all stakeholders to develop a transportation facility that fits its physical setting.

Source: FHWA



CSS/D Input



This process starts with getting input from you - the **Stakeholders**

- Your input might include functional things you want considered like
- It might be suggestions to improve how the facility looks.
- It might be sharing information about another project that is being planned by a local government, agency or private developer.







Enhancements require agreements with a local authority to fund all or part of the added construction cost and to assume long-term responsibility for maintenance. Local Authority Participation



P3 Participation

IOUISIANA DEPARTMENT OF TRANSPORTATION & DEVELOPMENT

This project is expected to include a Public-Private Partnership (P3). The P3 Proposer would be responsible for funding and maintaining certain improvements. The details of this arrangement have not been finalized.

CSS/D Limitations



Please remember:

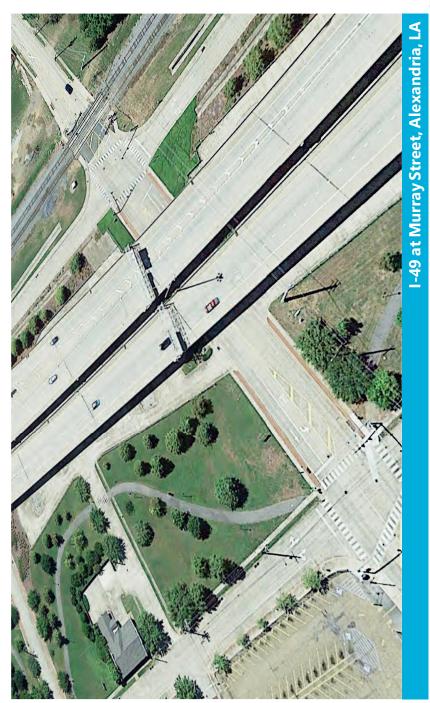
- This CSS/D Presentation considers ideas that may not have been fully studied and that may fall outside of the Louisiana DOTD responsibility and mission.
- The drawings in this presentation are preliminary and are subject to change based on engineering requirements and further study.



Example of a CSS/D Solution



- Park/Open Space
- Well defined crosswalks
- Sidewalk network including under bridge

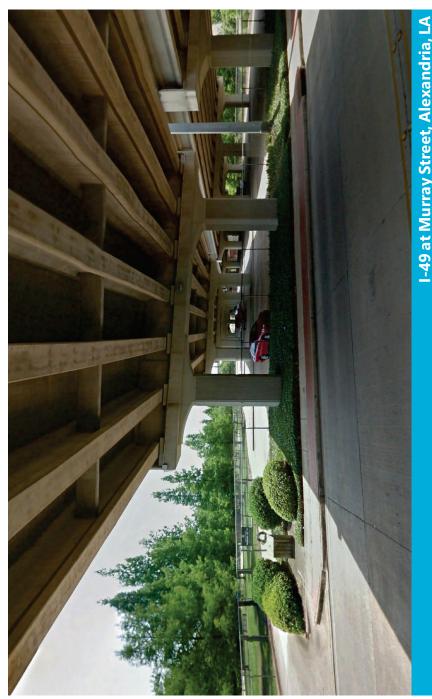


Source: Google

Example of a CSS/D Solution



- Landscaping
- Enhanced Sidewalks with brick buffer
- Parking
- Rectangular bridge columns
- Bridge mask walls at bents to cover girder
- Inverted Tee Bent Caps to increase headroom



Source: Google

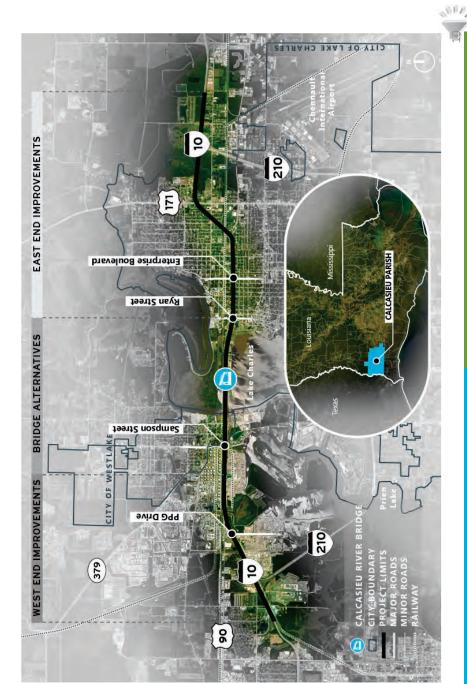
1-10 Calcasieu Bridge Improvements Project



Project length is approximately 9 miles and includes:

- Interstate roadways
- Calcasieu River Bridge and approaches
- I-10 frontage roads
- Several interchanges that connect the interstate to state and local roads.

The project may be built in phases.



1-10 Calcasieu Bridge Improvements Project



TOLL FACILITIES

- I-10 River Bridge would be constructed and maintained as a toll facility using electronic toll collection.
 - collection.
 Specific details of the toll gantry design will be determined during the design/build process.







Examples of Toll Gantry Design







Existing I-10 Calcasieu River Bridge



Project replaces the existing I-10 Bridge

- Opened to traffic 1952
- Integrated into I-10 in the 1960's
- Connects the cities of Lake Charles and Westlake
- 135 ft vertical clearance
- Steel truss architecture



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Existing Calcasieu River Bridge - Railing



The existing bridge railing includes approximately 5,286 pairs of cast-iron pistols. The crossed pistol design was inspired by the rich pirate history of Lake Charles.

Consideration should be given to:

- Salvage railing for other projects
- Incorporate the crossed pistol motif in new elements
- Develop interpretative panels and displays

Source: https://i.pinimg.com/736x/04/19/8f/04 198f243c30907824d261c6a3d7bd03.jpg



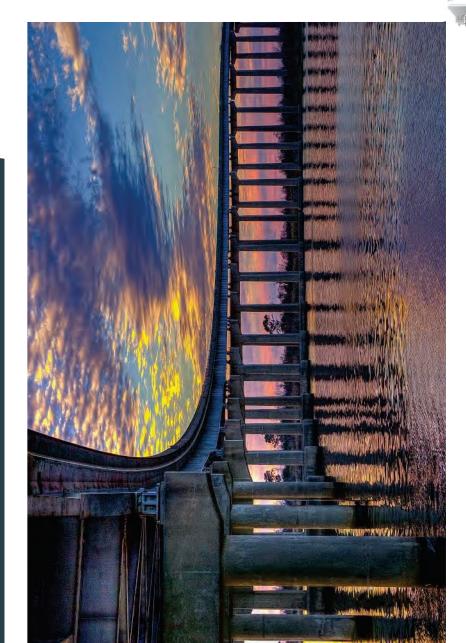
Existing I-210 River Bridge - Example



The new I-10-bridge will have a simple, low profile appearance, similar to the existing I-210 bridge (shown).

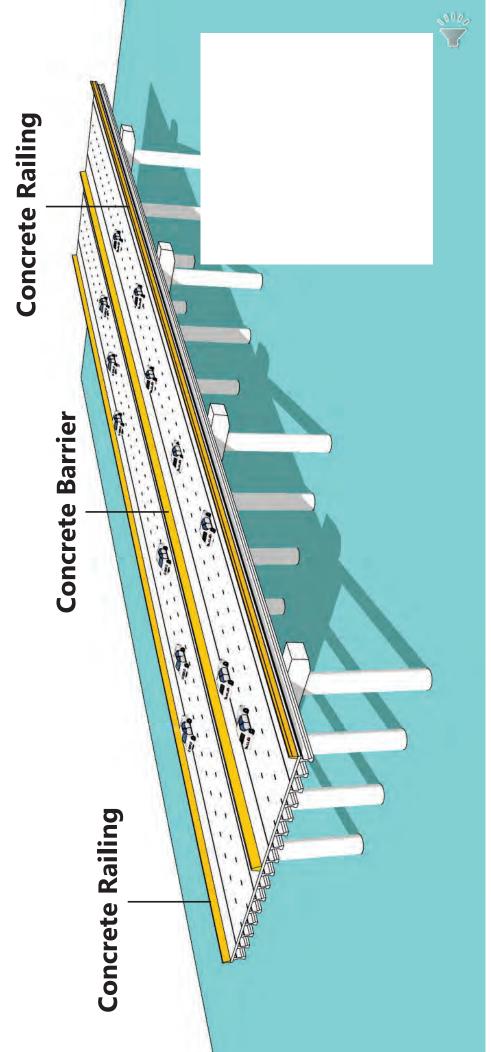
The I-10 bridge will not be as tall as the existing I-210 bridge.

- Proposed I-10 Bridge:
- 73' vertical clearance
- Existing I-210 Bridge (shown):
- 127' vertical clearance



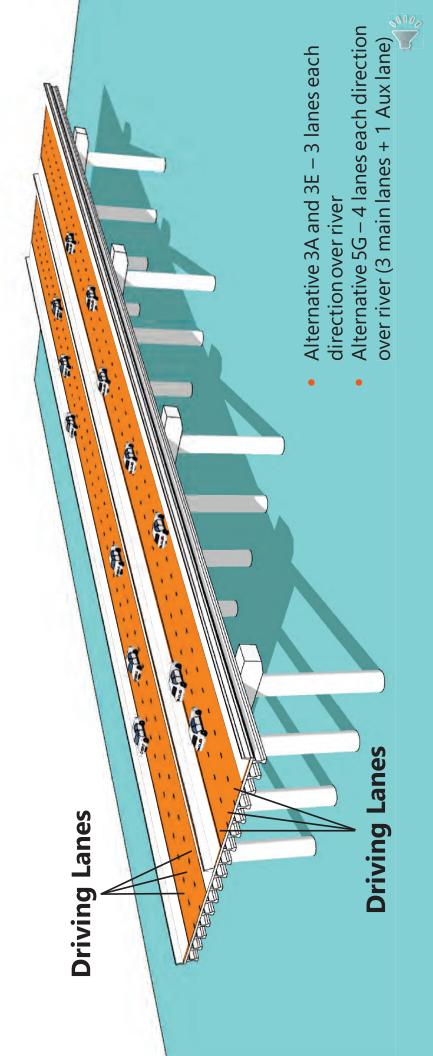
1-10 Calcasieu River Bridge – Base Design





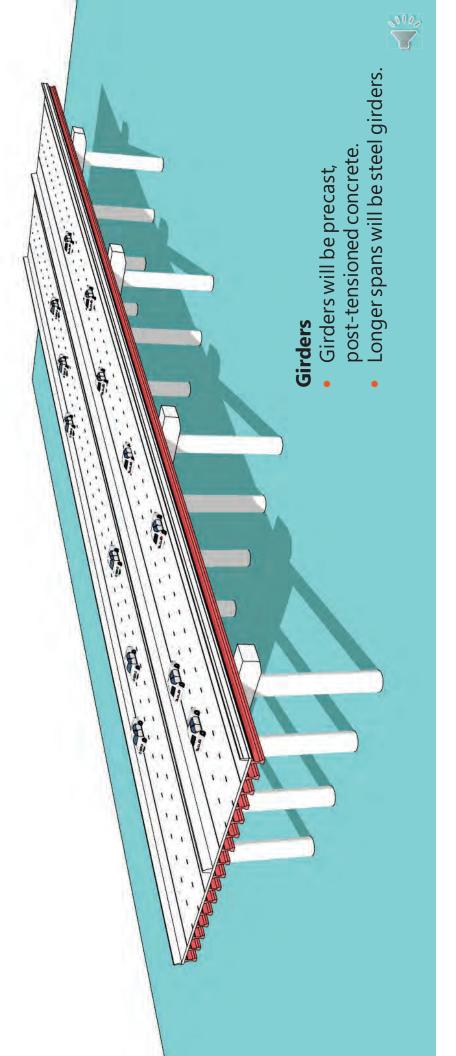
Proposed Calcasieu River Bridge – **Base Design**





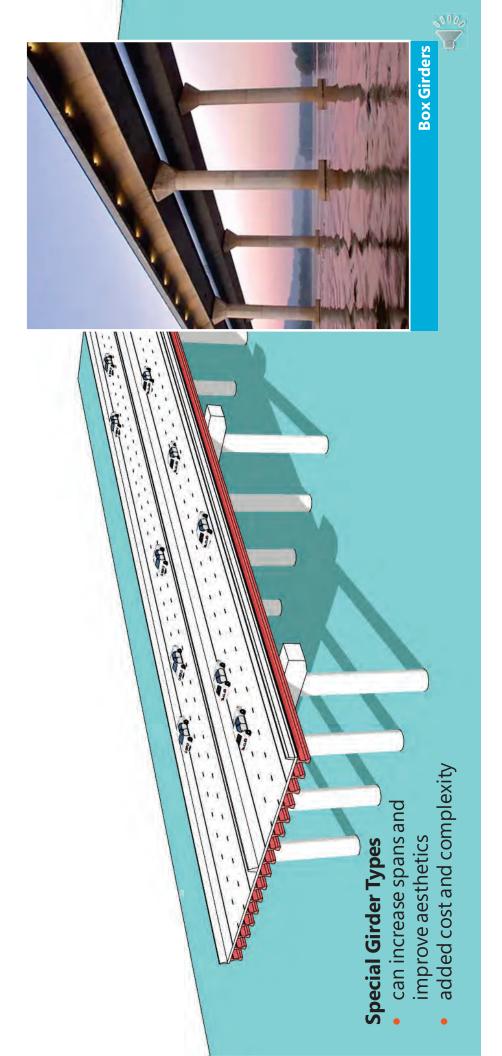
Proposed Calcasieu River Bridge – **Base Design**





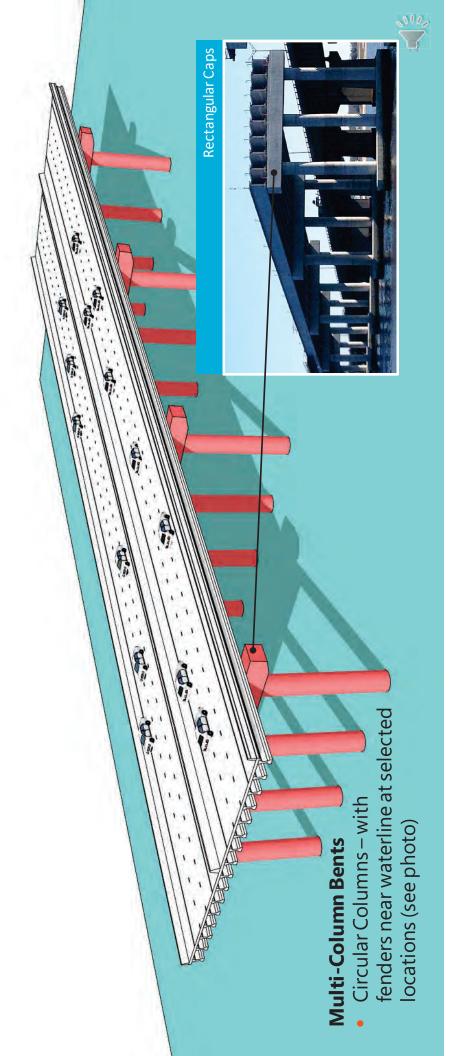
1-10 Calcasieu River Bridge – **Enhancement Example**





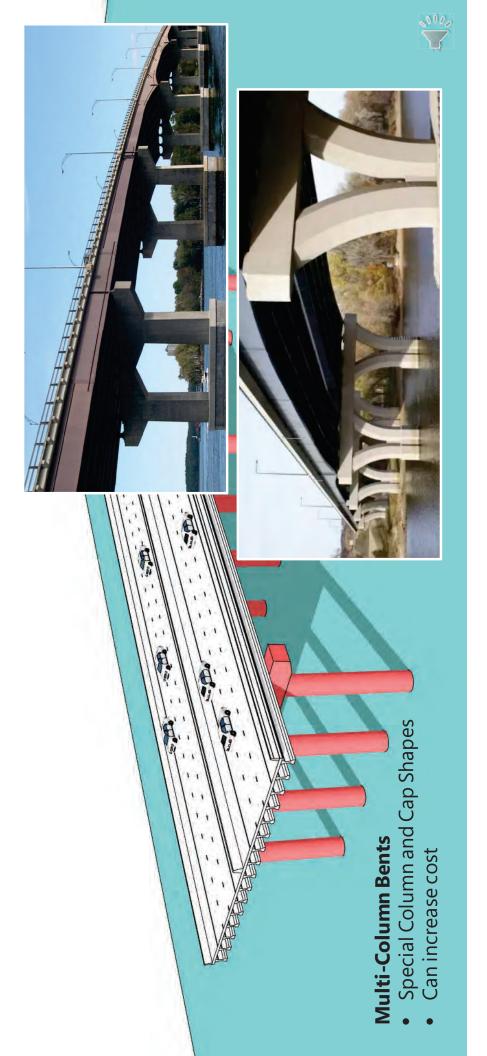
Proposed Calcasieu River Bridge – Base Design





1-10 Calcasieu River Bridge – **Enhancement Example**





Example Gateway Sign - at grade Sampson Street



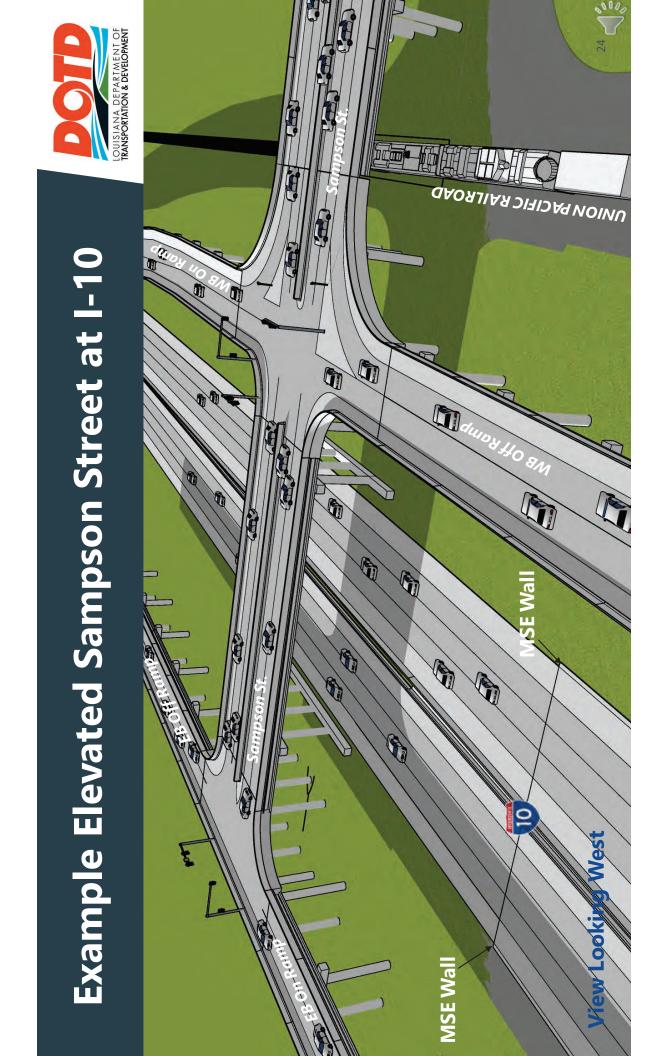
Existing Sampson Street just north of I-10 (street view)

- Aerial pipe racks
- Railroad crossings
- Gateway to Westlake



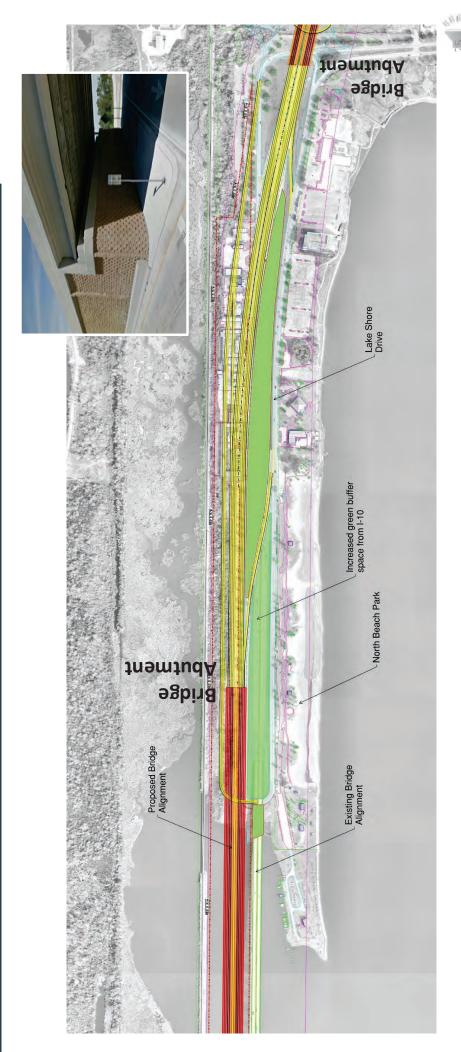
Source: Google







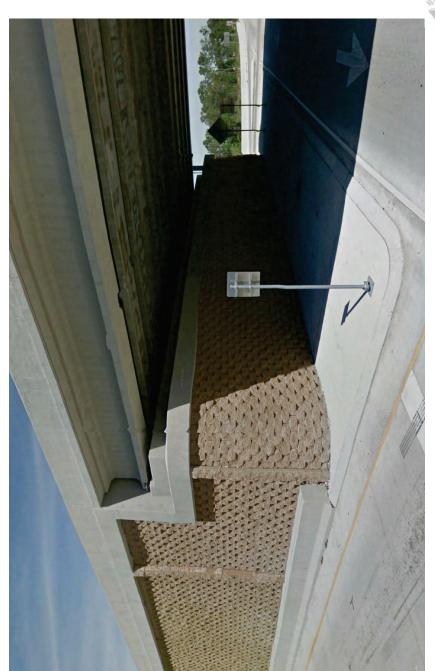
Example Bridge Abutment



Abutment and Retaining Wall - Base Design



Mechanically Stabilized Earth (MSE) Wall – Modular Block (Integral Color)



Source: Google

MSE Wall – Enhancement Examples



MSE Wall Retaining Wall Panels

- Mechanically Stabilized Earth (MSE)
- Ability to cast in variety of textures
- Paint/stain used for colors



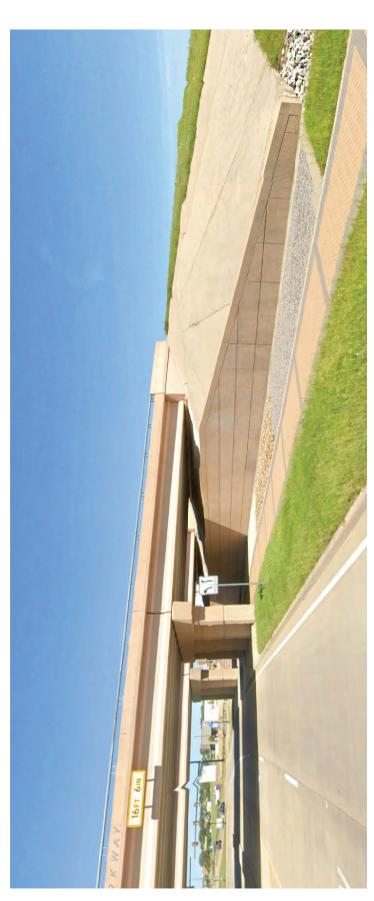






Example Bridge Abutment with Sloped Embankment

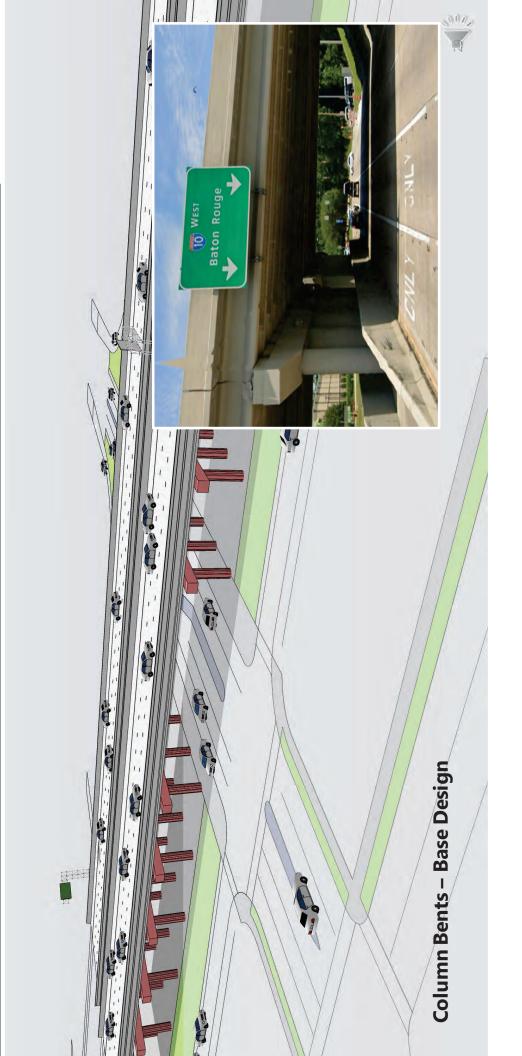
LOUISIANA DEPARTMENT OF TRANSPORTATION & DEVELOPMENT



Enhanced Bridge and Retaining Wall at Sloped Embankment



I-10 Corridor Elements



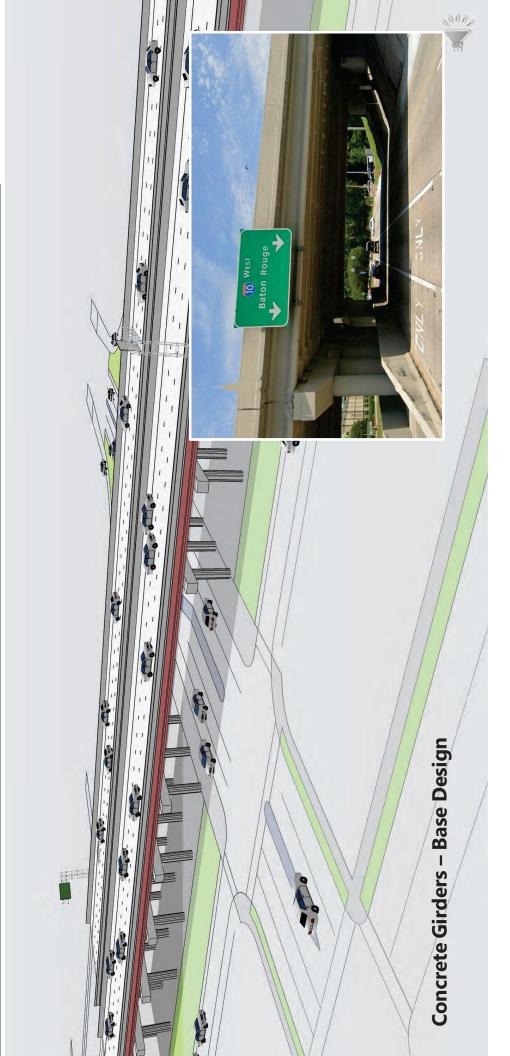


I-10 Corridor Elements

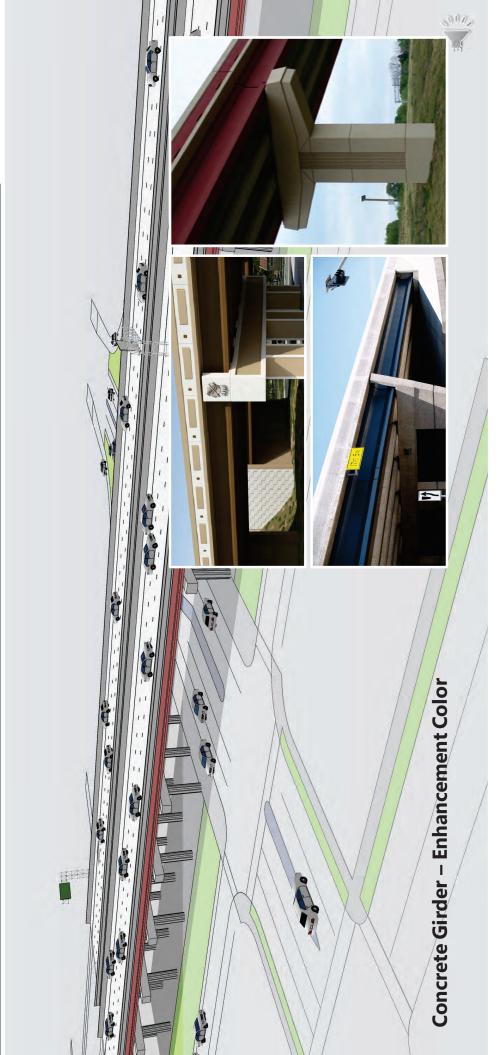




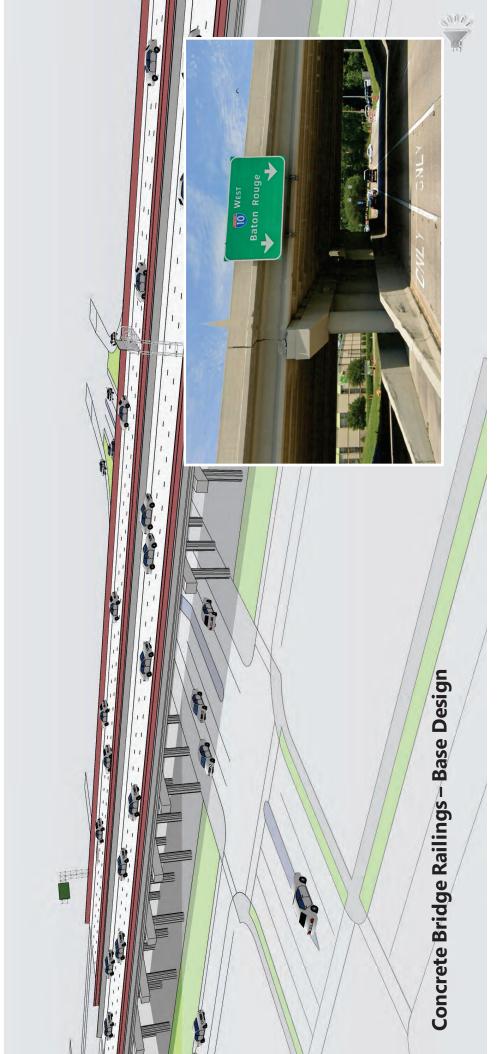
I-10 Corridor Elements



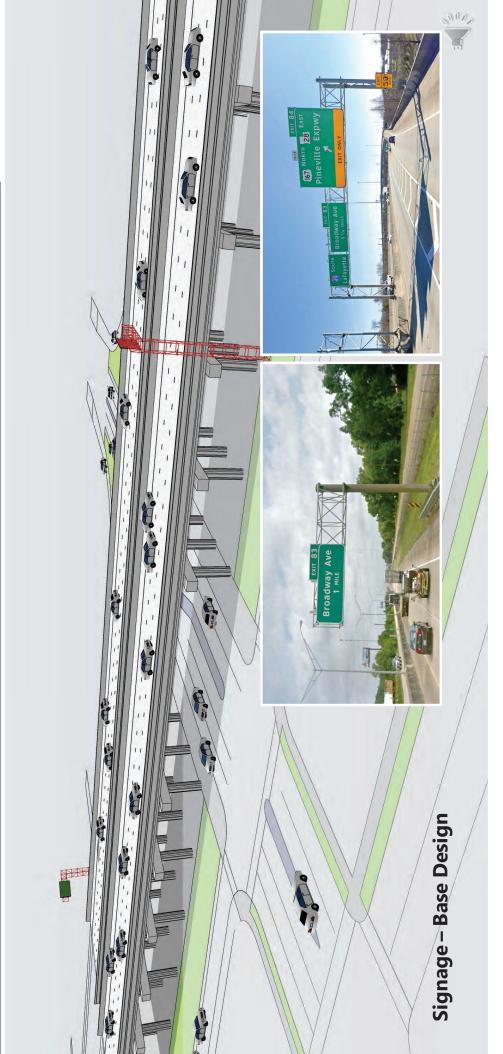








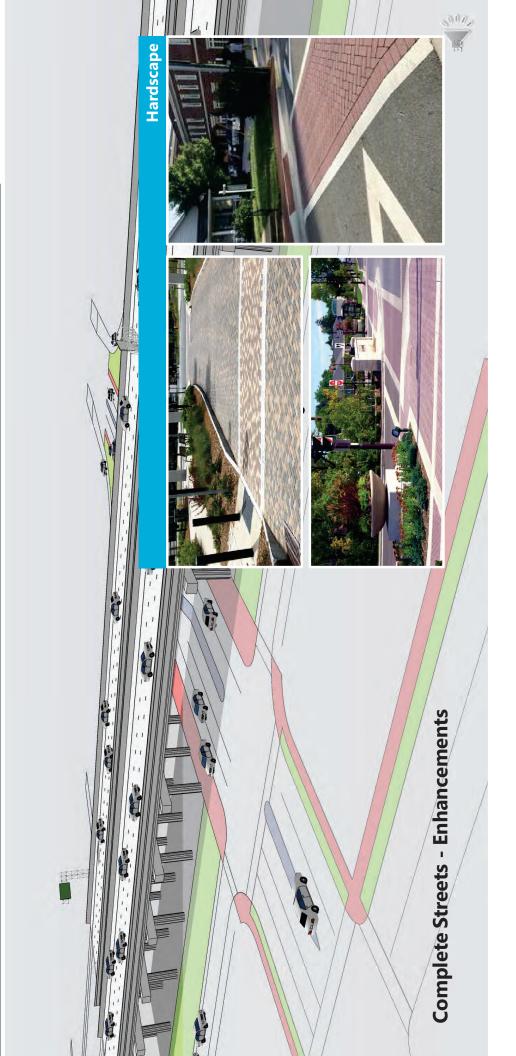




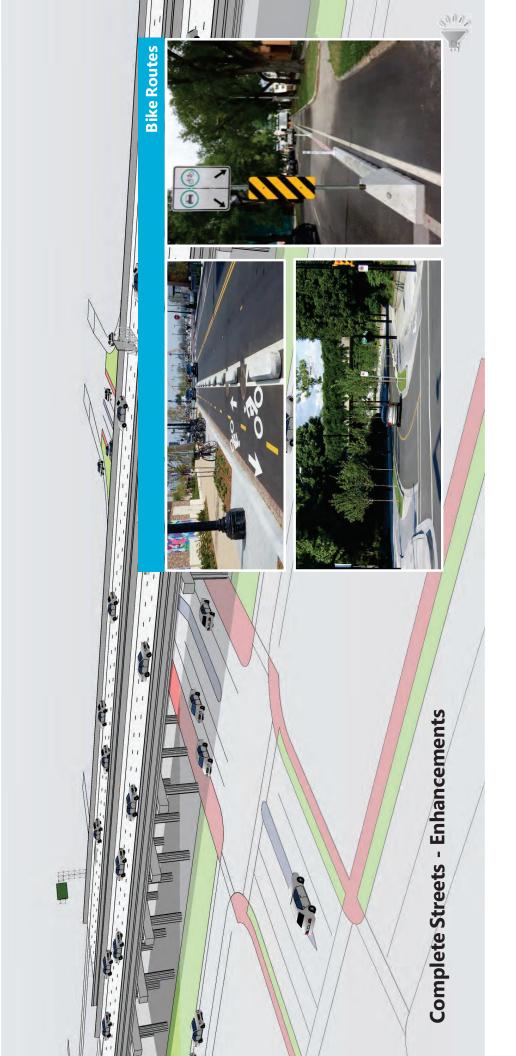








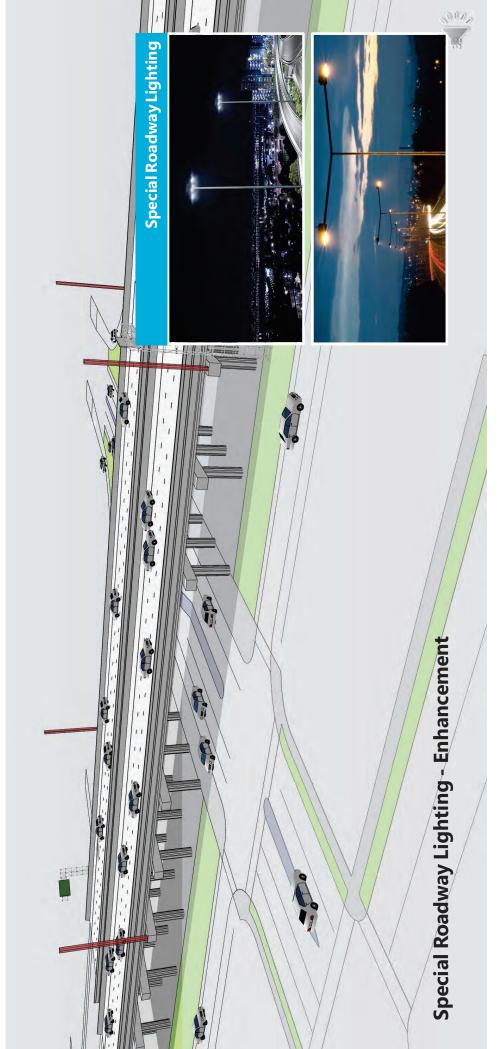




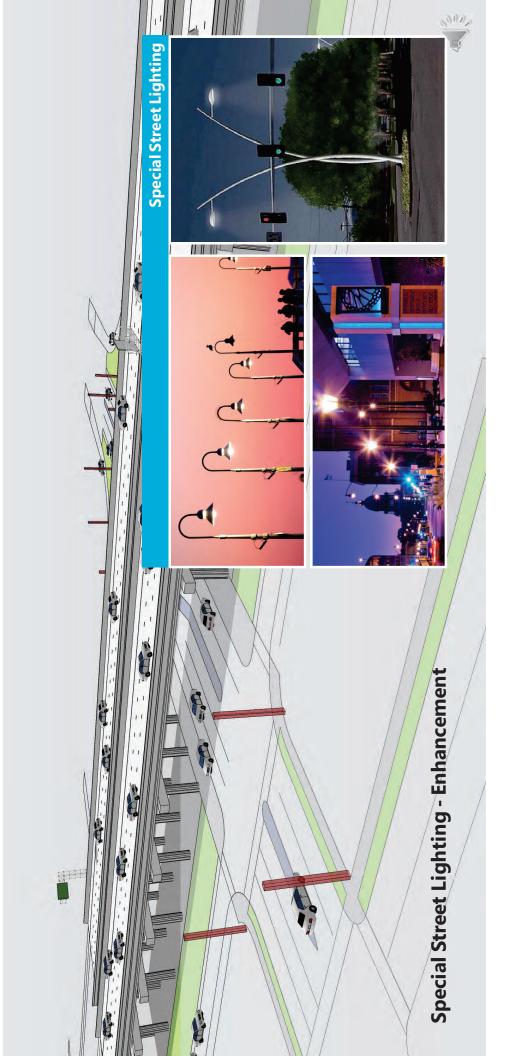








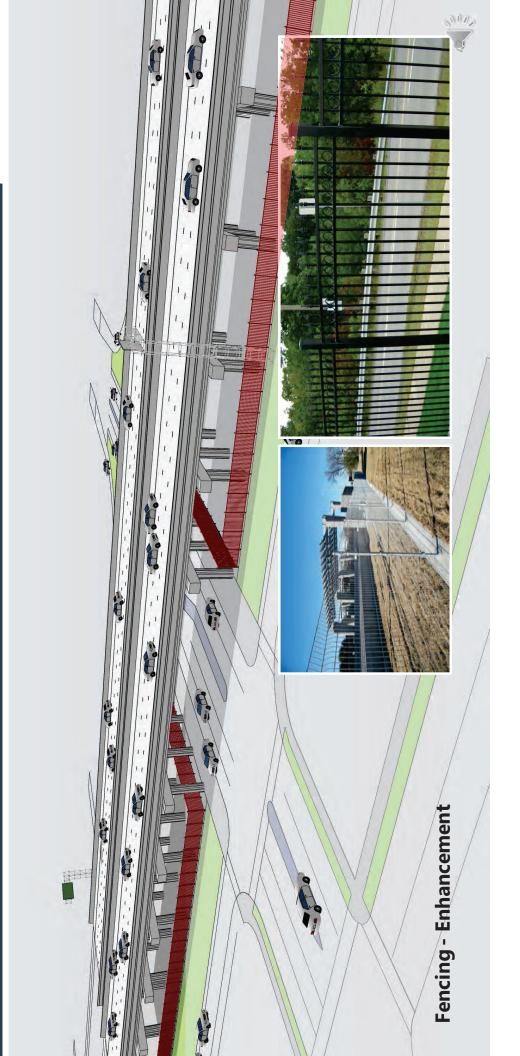








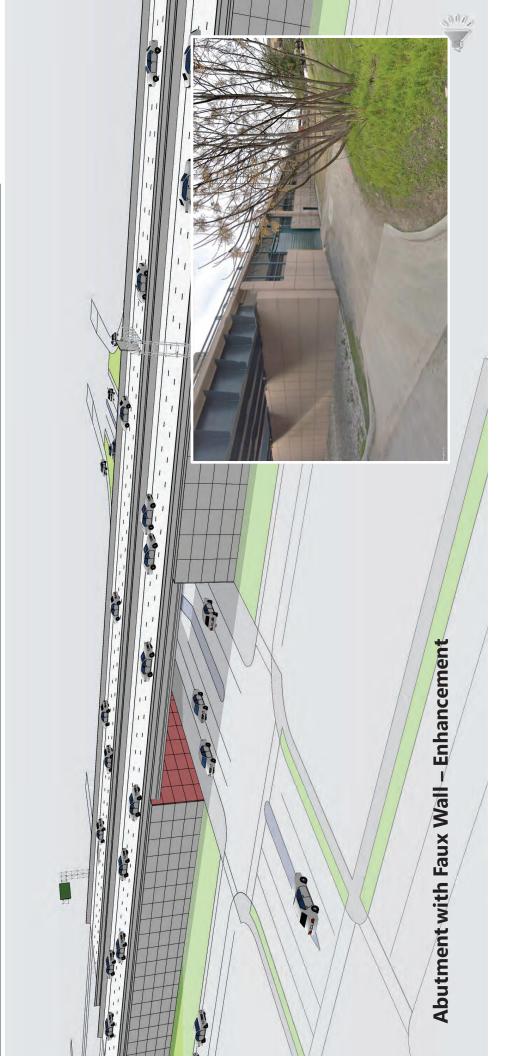






























I-10 Noise Abatement

- A noise study is underway to identify noise impacts.
- The study will also evaluate if noise walls are reasonable and feasible at impacted locations based on cost and effectiveness
- For abatement measures like noise walls the viewpoints of the community will be solicited



Example of Noise Wall mounted to bridge (I-10 New Orleans)

I-10 Noise Walls

IOUISIANA DEPARTMENT OF TRANSPORTATION & DEVELOPMENT

Final determinations of noise abatement measures will be made later, during the design stage



Ground mounted noise wall

Source: Google



Noise wall with decorative pattern



Limitations of CSS/D



Incorporation of CSS elements into the proposed project is constrained by:

- Funding availability
- Regulatory requirements
- Legal issues
- Schedule

responsibilities among LADOTD, local governments, non-governmental enhance community values may be specified in joint use agreements, Uses within the right-of-way, maintenance, and other measures to cooperative endeavors, or other contracts in order to assign organizations, and private enterprises.



Input/Question & Answer Period



THANK YOU FOR ATTENDING TODAY'S MEETING

We'd like now to give you the opportunity to ask questions and provide comments.

If you refer to a particular slide by number, it can be shown for reference. Please type your questions or comments in the "Chat" box.

You may also use the "Raise Hand" feature under "Reactions" ("Alt y" on your keyboard).

22



Additional Comments

For additional comments or questions, contact us at:

Project Phone: 225-366-9645

Email: CalcasieuBridge@hntb.com

Website: www.i10lakecharles.com

Mail: Louisiana Department of Transportation and Development, 10000 Perkins Rowe, Baton Rouge, LA 70810

CSS/D Workshop 1 comment period ends January 18, 2021

Comments received before the January deadline will be included in the Final CSS/D Workshop 1 record available online at www.i10lakecharles.com After the 10-day comment period you may still submit further comments online at www.i10lakecharles.com or by U.S. mail. These comments will be included in the Public Meeting #4 official report later this year.

A second CSS/D workshop will be held later in 2021. To sign-up, please use the contact information above.

Thank you





I-10 LAKE CHARLES CALCASIEU RIVER BRIDGE



CSS / D Meeting #2 Summary Report June 2021



CSS/D Community Meeting 2 Summary

I-10 Calcasieu River Bridge Improvements (I-10/I-210 West End to I-10/I-210 East End) Calcasieu Parish, Louisiana

State Project Number: H.003931

June 16, 2021





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ATTACHMENT

- A Meeting Invitation
- B CSS/D Community Meeting 2 Presentation C MPO Resolution

1.0 INTRODUCTION

The Louisiana Department of Transportation and Development (LADOTD), in conjunction with the Federal Highway Administration (FHWA), are preparing an Environmental Impact Statement (EIS) for the I-10 Calcasieu River Bridge Project. The proposed project is approximately 9 miles in length and includes alternatives for improvements to I-10 in the Lake Charles region between the I-210 interchanges, including the Calcasieu River Bridge (see **Figure 1**).



Figure 1: Project Location Map

The purpose and need of the proposed project is to (a) address the lack of system connectivity on I-10; (b) reduce congestion; (c) address roadway and bridge deficiencies; and (d) address roadway and

bridge safety concerns. The alternatives developed to address the above needs will be evaluated in the EIS. An EIS studies a range of reasonable alternatives, demonstrates compliance with environmental laws, and provides a means for public and agency input into the decision-making process.

The following document summarizes input obtained as part of the second Context Sensitive Solutions and Design (CSS/D) community meeting held virtually on June 16, 2021. The purpose of this community meeting was to present bridge design concepts that responded to comments received at the first CSS/D meeting. Meeting invitations

What is CSS/D?

Context Sensitive Solutions and Design (CSS/D) is a collaborative, interdisciplinary decision making process and design approach that involves all stakeholders to develop a transportation facility that fits its physical setting.

Source: FHWA

were sent to a list of stakeholders that included local officials and interested parties. A copy of the invitation is provided as Attachment A.

The meeting format was as follows:

- Instructions for the virtual meeting;
- Introduction of meeting participants (see Table 1);
- Pre-recorded meeting presentation (see Attachment B); and
- Input/questions and answer period (see Section 2.0).

Table 1: CSS/D Community Meeting #2 Participants

Participant Name	Participant Organization	Participant Title
Noel Ardoin	LADOTD	Environmental Section
Mary Bass	Calcasieu Parish School Board	Director of Transportation
Dusty Bastion	HNTB	Project Team
Tammy Broussard	Meyer and Associates / Westlake	Office Manager
Walter Council	Imperial Calcasieu Regional Planning and Development Commission (IMCAL)	Transportation Planner
Wes Crain	Director	Calcasieu Parish Police Jury
Fitzgerald Darbone	African American Chamber of Commerce	President
Don Duberville	LADOTD	District Administrator
Devin Foil	HNTB	Project Team
Jamie Gaines	IMCAL	Planner
Rick Hathaway	HNTB	Project Team
Donald Hyatt	Isle of Capri Casino	Facilities Manager
Jacquole Johnson	Federal Highway Administration (FHWA)	Area Engineer
Eric Kalivoda	DOTD	Deputy Secretary
Tom Kramer	HNTB	Project Team
Robert Mahoney	FHWA	Environmental Administrator
Lynn Maloney-Mujica	HNTB	Project Team
Cory Morgan	LADOTD	Attorney
Denise Rau	I-10 Calcasieu River Bridge Task Force	Member
Byron Racca	Meyer & Associates/Westlake	PE
Jeanne Rogers	Isle of Capri Casino	Manager
George Swift	SWLA Chamber-Economic Alliance	President/CEO
Meredith Taylor	HNTB	Project Team
Joachim Umeozulu	LADOTD	Senior Project Manager
Seth Woods	LADOTD	Assistant District 7 Administrator
Bart Yakupzack	I-10 Calcasieu River Bridge Task Force	Member

2.0 COMMENTS

Meeting attendees were given the opportunity to ask questions/provide comments subsequent to the pre-recorded presentation and could also provide comments via the project phone number, email, website, or mail through June 28, 2021, when the comment period ended. All comments/questions were received as part of the June 16, 2021 community meeting. **Table 2** summarizes the comments received by name, organization, title, and provides a response from LADOTD.

Table 2: Comments Summary and Responses

Name 2. Comments Summary and Responses							
Name (Last, First)	Organization	Title	Comment/Question	Response			
Gaines, Jamie	IMCAL	Planner	Will the resolution presented by IMCAL be entered into this CSS/D report?	Yes, the resolution is provided as Attachment C.			
Wes Crain	IMCAL	Chairman	Per the resolution decided upon, IMCAL wants to incorporate bike and pedestrian infrastructure into the bridge structure. The resolution also included an observation tower that we hope is taken into consideration. We hope that the City of Westlake's master plan for the riverfront is also taken into consideration.	The practicality of including bike and pedestrian facilities on the bridge is difficult for reasons stated in the public meeting,, but another possible option would be a small bicycle/pedestrian exclusive ferry that would operate at select times from the Lake Charles side in the vicinity of the boat launch and the marina to the new park in Westlake.			
Wes Crain	IMCAL	Chairman	I like the cable-stayed features shown in the presentation. I think incorporating the cross pistols motif is a good thing to do.	Comment noted.			
Wes Crain	IMCAL	Chairman	How long do we have from this point to get comments on the design elements back to the project team?	We normally allow 10 calendar days from the day of the meeting in order for it to be posted in the record and on our website. You are always free to put comments on the website using the comment link.			
Wes Crain	IMCAL	Chairman	Will the slideshow be available to view on the website?	Yes, posted to view on the website tomorrow.			

Table 2: Comments Summary and Responses

Table 2: Comments Summary and Responses								
Name (Last, First)	Organization	Title	Comment/Question	Response				
Bart Yakupzack	I-10 Calcasieu Bridge Task Force		I want to be clear on where the official response to comments from the public meeting regarding the bicycle and pedestrian paths on the bridge will be located on the website.	The official responses to Public Meeting #4 are located on the website, www.i10lakecharles.com . On the front page of the website to the right-hand side, there is a button labeled "Click here for access to the responses to public comments." Once that option is clicked, it will open the official responses to all comments received at Public Meeting #4. The response on this page regarding the bicycle and pedestrian facilities on the bridge remains the official response today. Building bicycle and pedestrian facilities on the bridge would require a change is state law, and the practicality of such facilities is also an issue.				
Bart Yakupzack	I-10 Calcasieu Bridge Task Force	Member	I want to point out that I think there are 22 U.S. Interstate bridges in the country that have bike/ped paths on them. I don't hear or see any research with regards to some of the problems that you anticipate. It is the intent of the MPO to give designers a chance to look at it and not tell us all the reasons it won't work but tell us how it can work.	Comment noted.				



CONTEXT SENSITIVE SOLUTIONS & DESIGN MEETING #2

The Louisiana Department of Transportation and Development is pleased to invite you to participate in a Context Sensitive Solutions & Design (CSS/D) Meeting. The first CSS/D Meeting, held on January 7, 2021, introduced the process and solicited ideas that could be developed as part of the proposed project design. This second meeting will present bridge design concepts that respond to comments received at the first meeting. Enhancement options for the section at the viaduct from Ryan Street to Opelousas Street will also be presented.

Wednesday, June 16, 2021 3:00 4:30 pm CST ZOOM Online

The meeting will be held virtually via ZOOM

A link and instructions to attend the meeting are attached to this email. You will need a computer or smart phone with video, audio, and internet capabilities to fully participate.

For technical issues, contact Devin Foil at 312-391-9475 or dfoil@hntb.com

To find general information about the project or alternatives being considered in the EIS, please visit **www.i10lakecharles.com** (best browser is Google Chrome or MS Edge).

Project Phone: 225-366-9645 Email: CalcasieuBridge@hntb.com



LOUSIANA DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT C/O HNTB 10000 PERKINS ROWE SUITE 640 BATON ROUGE, LA 70810



Context Sensitive Solutions and Design CSS/D COMMUNITY MEETING/WORKSHOP #2 | June 16, 2021



Presentation Outline



2nd CSS Workshop

- Calcasieu River Bridge
 - Iconic Feature Alternatives
- Sampson Street
 - Gateway Feature Alternative
- Enterprise Blvd
 - Multi-modal Concept
 - Under Bridge Alternatives
- Input, Questions and Answers



Source: clayhiggins.house.gov

Local Authority Participation



Enhancements require agreements with a local authority to fund all or part of the added construction cost and to assume long-term responsibility for maintenance.

P3 Participation



This project is expected to include a Public-Private Partnership (P3). The P3 Proposer would be responsible for funding and maintaining certain improvements. The details of this arrangement have not been finalized.

CSS/D Limitations

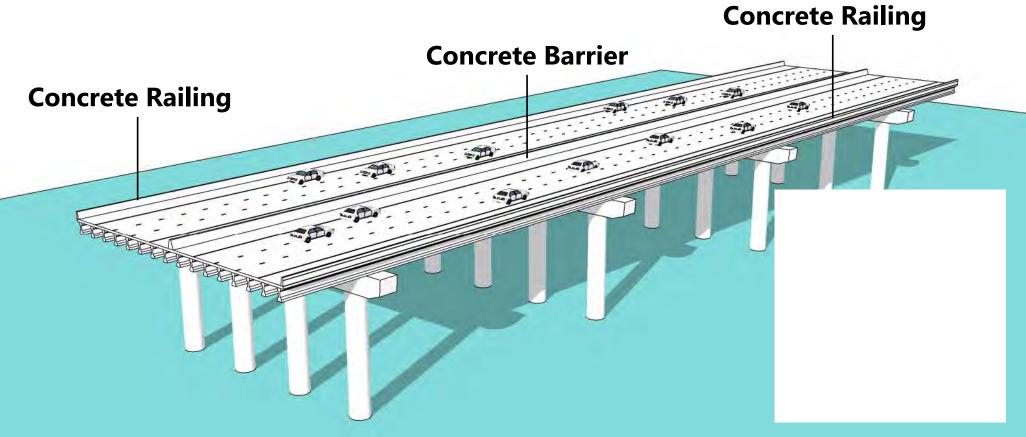


Please remember:

- This CSS/D Presentation considers ideas that may not have been fully studied and that may fall outside of the Louisiana DOTD responsibility and mission.
- The drawings in this presentation are preliminary and are subject to change based on engineering requirements and further study.

I-10 Calcasieu River Bridge – Base Design









Base Design





Base Design

Iconic Precedent Images





I-35 Frontage Road Bridges Waco, Tx

Iconic Precedent Images





I-35 Frontage Road Bridges Waco, Tx





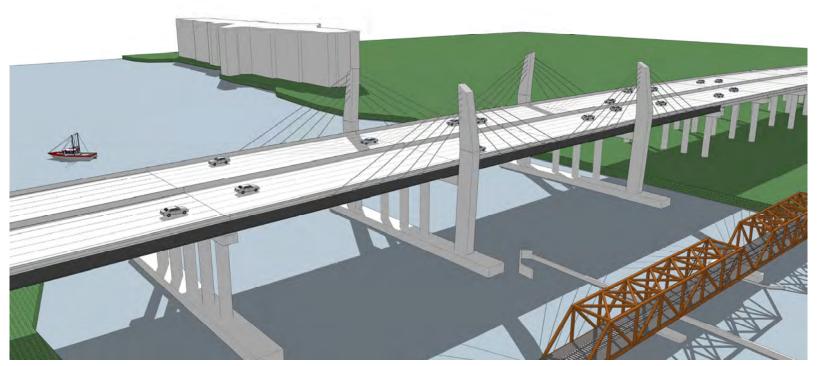
Cable Stay Features





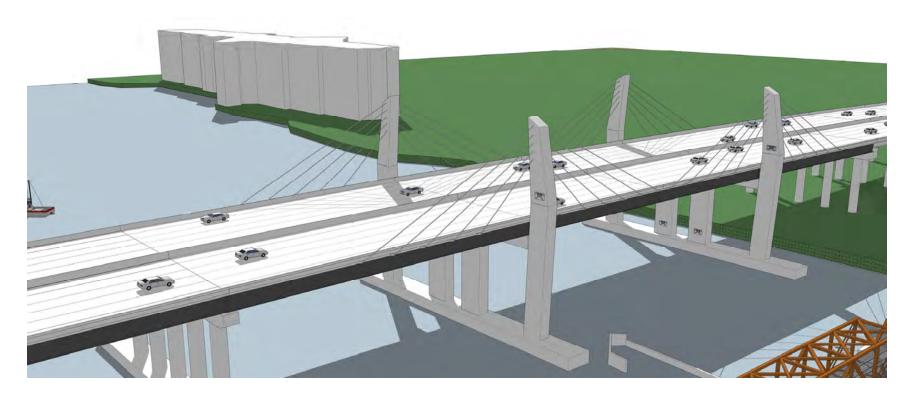
Cable Stay Features





Cable Stay Features





Cable Stay Features with Crossed Pistols – cast in concrete





Cable Stay Features with Crossed Pistols





Cable Stay Features

Calcasieu River Bridge - Context



Existing Bridge Trusswork



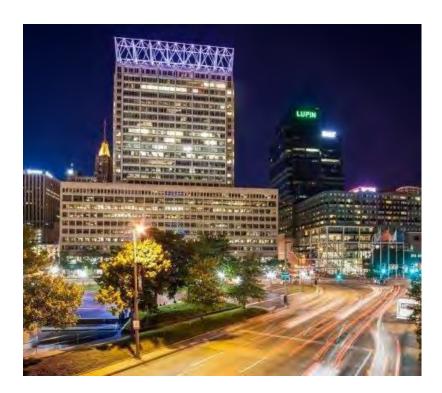


Iconic Precedent Images



Architectural Truss





Iconic Precedent Images



Bridge Truss Lighting









Pylon Features - 5 sets





Pylon Features - 5 sets





Pylon Features - 5 sets





Pylons - with Crossed Pistols





Large Crossed Pistols graphic panels or banners





Pylon Features – 5 sets





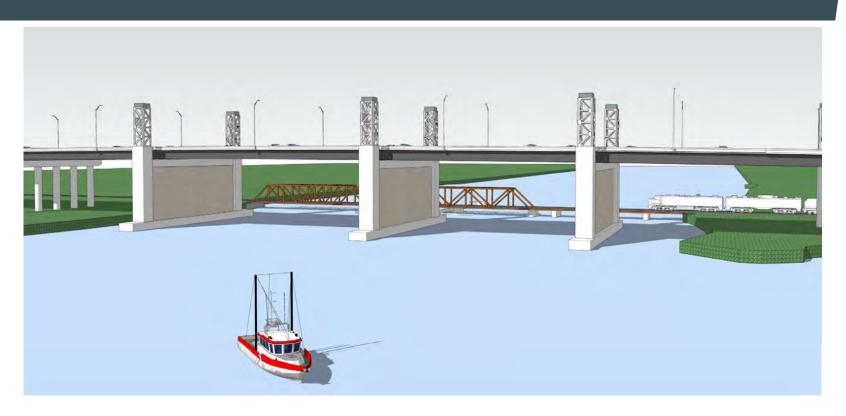
Pylon Features – 3 sets





Pylon Features – 2 sets





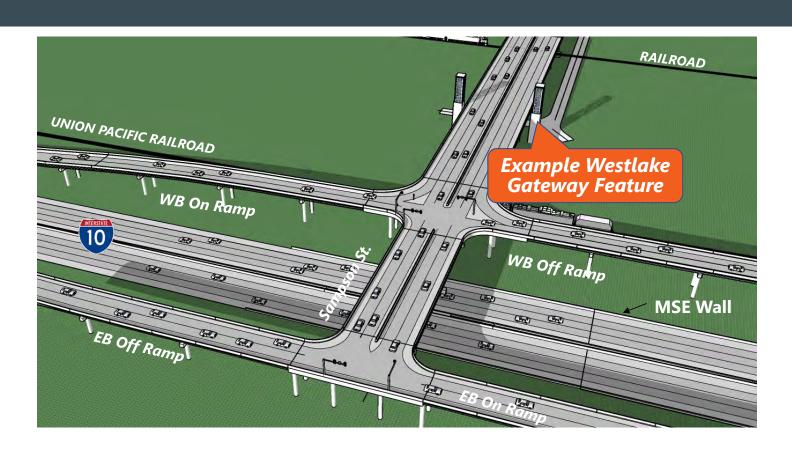
Example of Monolithic Bents with Pylons





View Looking West





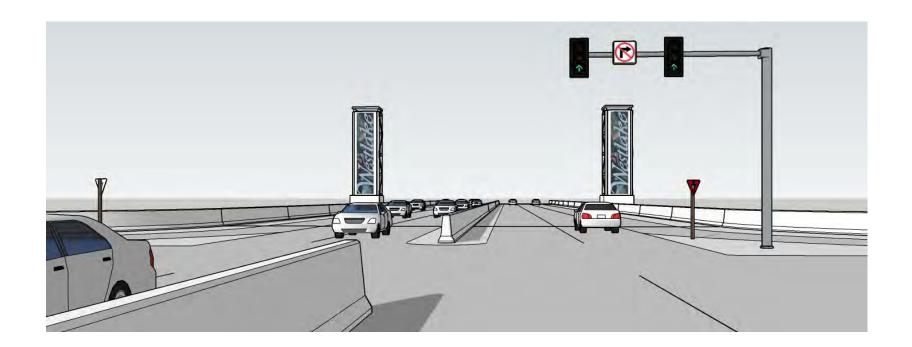
View Looking North





View Looking North

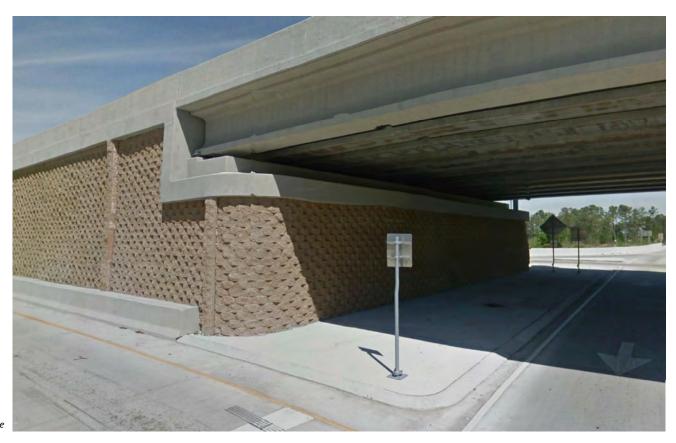




Abutment and Retaining Wall – Base Design



 Mechanically Stabilized Earth (MSE) Wall – Modular Block (Integral Color)



Source: Google



MSE Wall Retaining Wall Panels

- Mechanically Stabilized Earth (MSE)
- Ability to cast in variety of textures
- Paint/stain used for colors







MSE Wall Retaining Wall Panels

- Mechanically Stabilized Earth (MSE)
- Ability to cast in variety of textures
- Paint/stain used for colors



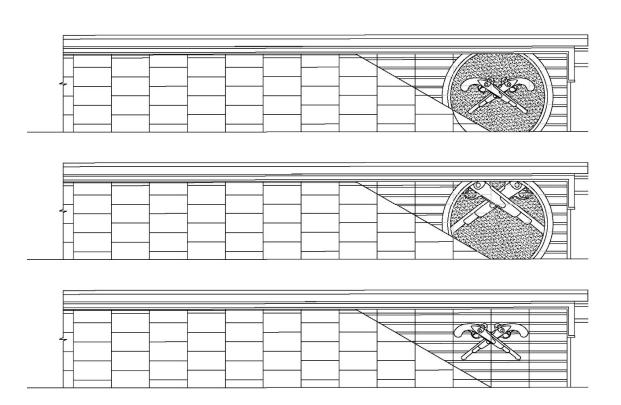






Crossed Pistols

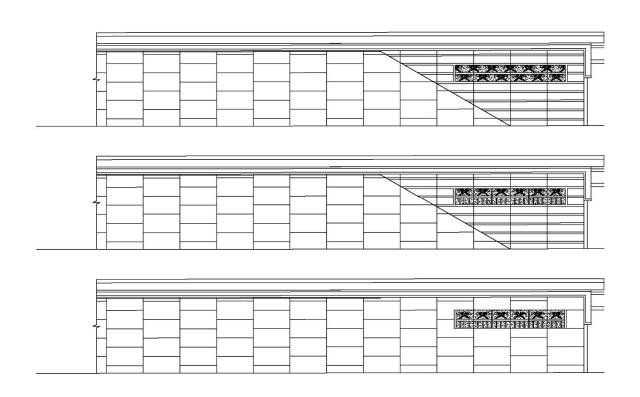
Example Graphics





Crossed Pistols

Example Graphics



Bicycle and Pedestrian Network Features

- Complete Streets Treatment for Community Enhancement
- Shared use paths for bicycle & pedestrian use
- Enterprise Blvd both sides
- Belden Street south side
- 10' min. path width with 5' street buffer
- Pedestrian signals synchronized with green lights



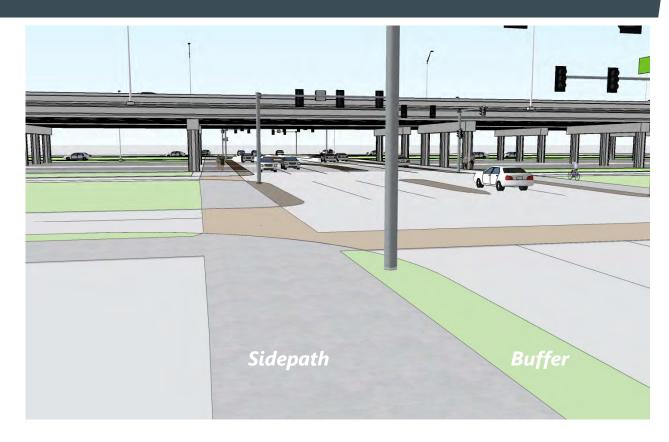
Plan View





View of Elevated I-10 – Base Condition





View of Elevated I-10 – Base Condition with Sidepath Enhancement







Under Bridge Lighting - Enhancement





View of Elevated I-10 – with Fence Enhancement





Street View of Elevated I-10 – with Fence Enhancement





Panels with Wire Grids



Ornamental Picket Fence

Enhanced Fencing Examples





View of Elevated I-10 (constructed on fill with MSE retaining walls)





View of Elevated I-10 – with Screen Wall





Transparent Top – Example



Transparent Top – Example





Street View of Elevated I-10 – with Screen Wall and Transparent Tops





Street View of Faux Wall Example





Street View of Faux Wall Example

4

I-10 Noise Abatement



- A preliminary analysis concluded that in most of the project noise walls or barriers do not meet federal limits for cost effectiveness.
- In areas with a large residential population adjacent to the proposed project, LADOTD would consider using state funds to build noise walls where they would abate traffic noise if affected neighborhoods were interested in these features.
- More information will be made available in the draft EIS. The examples shown here are for local representatives to understand the visual and aesthetic effects of noise walls.



Example of Noise Wall mounted to bridge (I-10 New Orleans)

Source: Google

I-10 Noise Walls



Final determinations of noise abatement measures will be made later, during the design stage



Ground mounted noise wall



Noise wall with decorative pattern

Limitations of CSS/D



Incorporation of CSS elements into the proposed project is constrained by:

- Funding availability
- Regulatory requirements
- Legal issues
- Schedule

Uses within the right-of-way, maintenance, and other measures to enhance community values may be specified in joint use agreements, cooperative endeavors, or other contracts in order to assign responsibilities among LADOTD, local governments, non-governmental organizations, and private enterprises.

Input/Question & Answer Period



THANK YOU FOR ATTENDING TODAY'S MEETING

We'd like now to give you the opportunity to ask questions and provide comments.

Additional Comments



For additional comments or questions, contact us at:

- Project Phone: 225-366-9645
- Email: CalcasieuBridge@hntb.com
- Website: www.i10lakecharles.com
- Mail: Louisiana Department of Transportation and Development, c/o HNTB 10000 Perkins Rowe, Baton Rouge, LA 70810

Next Steps



- CSS/D Workshop 2 comment period ends June 28, 2021.
- Comments received before the June 28 deadline will be included in the Final CSS/D Workshop 2 record that will be made available online at www.i10lakecharles.com.
- You may continue to submit comments online at www.i10lakecharles.com, by email, phone or US mail.



Thank you





LAKE CHARLES URBANIZED AREA METROPOLITAN PLANNING ORGANIZATION TRANSPORTATION POLICY COMMITTEE

June 3, 2021

Mr. Eric Kalivoda
Deputy Secretary
Louisiana Department of Transportation & Development
1201 Capitol Access Road
Baton Rouge, LA 70802

Dear Mr. Kalivoda,

Please find attached Resolution No. 2021-03 Approving and Adopting Design Options for The Proposed New I-10 Calcasieu River Bridge approved and adopted by the Lake Charles Urbanized Area Metropolitan Planning Organization Transportation Policy Committee.

Thank you

Wes Crain, Chair

MPO Transportation Policy Committee

LAKE CHARLES URBANIZED AREA METROPOLITAN PLANNING ORGANIZATION TRANSPORTATION POLICY COMMITTEE

RESOLUTION NO. 2021-03 APPROVING AND ADOPTING DESIGN OPTIONS FOR THE PROPOSED NEW I-10 CALCASIEU RIVER BRIDGE

WHEREAS, the Southwest Louisiana Regional Planning Commission (aka IMCAL) is the designated Metropolitan Planning Organization (MPO) for the Lake Charles Urbanized Area; charged with overall responsibility for managing the four (4) year Transportation Improvement Program (TIP) and the Metropolitan Transportation Plan (MTP) for the Lake Charles MPO Study Area; AND

WHEREAS, the MPO Transportation Technical Advisory Committee and MPO Transportation Policy Committee have engaged in a lengthy study and review of alternative options for design and construction of a new Interstate 10 Bridge over the Calcasieu River for many years as a critical infrastructure need; AND

WHEREAS, it is a responsibility of the Metropolitan Planning Organization to coordinate with the Louisiana Department of Transportation and Development (LADOTD) and the Federal Highway Administration (FHWA) in the most advantageous and effective transportation link both in the Lake Charles Urban Area and Southwest Louisiana Region; AND

WHEREAS, the MPO has considered several design alternatives including capacity and alignments presented by contracted consultants for the proposed new I-10 Bridge across the Calcasieu River; AND

WHEREAS, the MPO Transportation Technical Advisory Committee (TAC) has evaluated critical impacts of proposed bridge alternatives to the metropolitan transportation network and formally submitted recommendations to the MPO Transportation Policy Committee; AND

WHEREAS, the MPO Transportation Policy Committee has deliberated recommendations of the Technical Advisory Committee as well as comments of the Southwest Louisiana Economic Development Alliance I-10 Bridge Task Force and determined what are important components for the proposed new bridge.

NOW THEREFORE BE IT RESOLVED THAT THE METROPOLITAN PLANNING ORGANIZATION TRANSPORTATION POLICY COMMITTEE FOR THE LAKE CHARLES URBAN AREA APPROVES AND ADOPTS THE FOLLOWING REQUIREMENTS FOR THE PROPOSED NEW I-10 BRIDGE ACROSS THE CALCASIEU RIVER, WHICH ARE IN CONFORMITY WITH THE LONG-RANGE METROPOLITAN TRANSPORTATION PLAN:

- 1. Selection of Alignment and Design "5G".
- 2. Inclusion of bike and pedestrian infrastructure in the new bridge.
- 3. Collaboration between Kansas City Southern Railroad, Louisiana Department of Transportation and Development and the City of Westlake Administration for a rail spur alignment be satisfactorily concluded.
- 4. The "5G" Alignment Bridge Design contains iconic aesthetics, including an Observation Tower; Signature, Programmable LED Lighting; Crossed Pistol features representative of the current 1949 bridge railings, and in the event the bridge is not a cable-stayed design, a Cable Stayed feature (non-supportive) as a primary feature.

THIS RESOLUTION BEING VOTED ON AND ADOPTED THIS 12th DAY OF MAY 2021

Wes Crain, Chair

MPO Transportation Policy Committee:

ATTEST:

Executive/MPO Director



Waterway Commission Meeting Record February 2019



C. FLY MARINE SERVICES LLC

Naval Architects and Marine Engineers
P.O. Box 2 • Madisonville, LA 70447
Tel: 985-792-7761

<u>Calcasieu River Harbor Safety Navigation & Infrastructure Committee</u> <u>Meeting Record - 02/27/19</u>

Atendees:

Michael Sullivan (USACE – New Orleans)
Julio Vidal Salcedo (USACE – New Orleans)
Tracy Falk (USACE – New Orleans)
Regan Brown (Port of Lake Charles)
David Devall (Devall Towing)
Dan Cost (USCG – MSU Lake Charles)
George Mowbray (Lake Charles Pilots)
William Hickey (USCG – MSU Lake Charles)

John Nolan (USCG – MSU Lake Charles) Noel Ardoin (LADOTD) Joe Umeozulu (LADOTD) Eric Kalivoda (LADOTD) Kate Prejean (HNTB) Charles Fly (C. Fly Marine) Nathaniel Bailes (C. Fly Marine) Steve Couch (Chair HSC)

Meeting Record:

- Steve Couch started the meeting off with a brief introduction.
- Kate Prejean provided background information on the project to the committee.
- Dr. Eric Kalivoda provided further information to the committee regarding the project.
- Nathaniel Bailes presented the location that the LADOTD intends to relocate Friend Ships' vessels to.
- The discussion that followed included topics such as the effects of mooring vessels at the location on the safe navigation of the Calcasieu River and the regular operations of other organizations and companies.
 - o LA Scrap regularly has large oceangoing barges towed to their yard that is located upriver of the selected location. The towing companies will often have one tug on each side of the tow to facilitate the tow through the narrow opening of the railroad bridge. The towing operation can be much wider than the 90' clearance of the railroad bridge.
 - The Army Corps of Engineers dredges the federal navigation channel every 5-8 years. It was discussed
 that regulations may require a fairway past the end of the channel to ensure that dredging operations
 can maintain the channel up to the end.
 - The location puts vessels directly beside the turnaround location for the federal channel. This would result in heavy propeller wash being directed toward the moored vessels. Heavy propeller wash will also be directed towards the vessels when large tows pass, and the tugs are maneuvering the tow to fit through the railroad bridge. There is some concern regarding the mooring practices of Friend Ships and the consequences that would result if a vessel were to break free from her moorings.
 - The reduced space for maneuverability increases the risk of damaging the railroad bridge.
 - The Coast Guard expressed concern that Friend Ships' budget may make it difficult to maintain their operations and facilities in a proper manner, keep their vessels securely moored, and ensure that their vessels do not become an eyesore. These are issues that the Coast Guard is already working with Friend Ships to improve. The LADOTD indicated that they would have to provide mitigation to Friend Ships for any additional expenses that may incur because of the relocation.
 - Rearranging the railroad bridge would help to resolve several of the concerns.
 - o It is possible that Friend Ships and the towing companies could coordinate, and Friend Ships could temporarily relocate their vessels while the tow is brought upriver.
 - If dredging is required at the location, the Army Corps would need to know how much is being dredged, where its being dredged from, and where is the disposal area. Polluted soil from an old spill is also a concern.
- The discussion closed once all parties had voiced their concerns.



Sampson Street Stakeholders Meeting March 2019

H.003931 - I-10 Calcasieu River Bridge Sampson Street Stakeholders Meeting, 3/21/2019 SIGN-IN SHEET Please Print



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Name	Organization	Phone	E-Mail
Ed Theriot	P.A.I	337-884-8721	CTheriot alport aggraphes con
Kink Tarhan	Pont Aggnegates duc.	337-884-8712	Kwtrahaw@pontaggnog +tes. Com
Megan Hartman	Phillips 66	337-491-4443	megan.m. hartman@plote.com
Wendi Acosta	J&J Outdoor	281-992-2828	Wendi@jgioutdoor.com
Branna Van Borson	JET Outdoor	281.992.2828	brianna æjgi out dervion
CADE MULEMORE	CALLASTEU PARTISH OFFICE OL HEMELAND SELRITY	337-721-3800	Trackmone@ CPPT.WAT
Larry De Roussel	LAIA	337 436 6800	Iderovssel @ laia.com
JASON BOAZ	ISLE of CAPRI	337.763.5598	jason. boar @islecorp.com
Even Gilosos	Tole Cappi	337-438-2341	e-gre. 51ds and Itacop com
JEANNE ROGERS	ISLE OF CAPRI	337-430-2330	jeanne. Rogers a islecorp.com
PATRICK CUSEY	LADOTD	337.496.9781	PATRICK. CUSEY@LA.GOV
Noel Ardoin	DOTO		
Jared Ray	DOTD	225 379 1038	Jared. Ray @ la.gov
Bonga yourplack	channer sans	337 4970137	bart Qualvutgrave. ac
Frent Lumpkir	south and Capital	337-478-6207	Brenta Lumpkin Properties, con
JOHN POHORELY			john 20 sapal. com
Chuck Klecklan	N 11	414-5248	Churce Kleckley consulting. com
Jeff Lamper	Lonza	430-4060	Jeffrey. Lamper @ Lonza. con
Kyle Baca	Lonza	430-4088	Kyle, Baca Clonza.com









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Bridge Point Yacht Center	jsgbpyc@yahoo.com
Dunham Price Group, LLC	rsprigg@dunhamprice.com
Holcim Inc./Lafarge	andrew.martin@lafargeholcim.com
Isle of Capri Casino Hotel Lake Charles	jeff.favre@islecorp.com
JGI OUTDOOR ADVERTISING	contactigi@jgioutdoor.com
JUNIPER SPECIALTY PRODUCTS LLC	info@junipersp.com.
Kansas City Southern	Shonnur@kcsouthern.com
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SASOL CHEMICALS (USA) LLC	info@sasol.us.com
Union Pacific Railroad, Louisiana	drewtessier@up.com
Vulcan Materials Company	community@vmcmail.com
Westlake City Council	cityclerk@cityofwestlake.org
Westlake Fire Chief	jpicou@westlakefire.com
Westlake Police Department	cwilrye@westlakepd.com
Westlake United Methodist Church	secretary@westlakeumc.net



PBA 2-A, PBA 3-A, PBA 4-A





PBA 2-C. PBA 3-C





PBA 2-E, PBA 3-E





PBA 5-G

Objectives Screening Evaluation Matrix of Preliminary Alternatives

Objective	(1) Minimize	ROW Impacts	(2) Avoid/Minimize Impacts to Existing Infrastructure		(3) Minimize Roadway Disruptions during Construction			(4) Optimize Co	st	(5) Minimize Cons	(6) Improve Access at Sampson Street		
	A	В	C	D	E	F	G	Н	1	J	К	L	M
Criteria/ Measures	New ROW (acres)	Paicels Impacted (#)	Major Utilities Crossed (#)	Railroad Crossing Impacts (Sampson St.)	I-10 Full Road Glosures (#)	Arterial Full Road Closures (#)	Estimated Construction Cost in Millions (M)	Estimated ROW Cost in Millions (M)	Estimated Operations and Maintenance Gost in Millions (M)	Potential Impacts to Soil Pressure - Sampson St. Construction	Potential Impacts to Soil Pressure- Calcasieu River Bridge Construction	Potential Impacts to Project Cost and Schedule	Effect on Traffic Movements at Sampson Street
No-Build	0	ū	0	High	0	0	\$0	\$0	\$31	LOW	LOW	LOW	Low
PBA1-F	61.6	60	102	Low	48	230	\$599.8	\$28.8	\$0.63 (\$630 K)	HGH	HGH	HGH	LOW:
PBA 2-A	84.7	85	100	MEDIUM	48	226	\$770.3	\$34.6	\$1.1	LOW	MEDIUM	MEDIUM	MEDIUM
PBA 2- B	105.3	148	105	MEDIUM	40	214	\$889,7	\$39.4	\$1.1	TOM	MEDIUM	MEDIUM	MEDIUM
PBA 2-C	98.0	110	107	MEDIUM	48	226	\$778.4	\$38.2	\$1.1	LOW	MEDIUM	MEDIUM	MEDIUM
PBA 2-D	122.3	173	112	MEDIUM	40	214	\$897.8	\$41.9	\$1.1	LOW	MEDIUM	MEDIUM	MEDIUM
PBA 2-E	93.5	90	104	MEDIUM	56	223	\$803.6	\$36.7	\$1.1	LOW	MEDIUM	MEDIUM	HiGH
PBA3-A	85	85	100	MEDIUM	48	226	\$821.0	\$34.6	\$1.1	LOW	row	LOW	MEDIUM
PBA3-B	105	146	105	MEDIUM	40	214	\$940.4	\$39.4	\$1.1	LOW	Low	LOW	MEDIUM
PBA3-C	98	110	107	MEDIUM	48	226	\$829.1	\$38.2	\$1.1	LOW	LOW	LOW	MEDIUM
PBA3-D	122	173	112	MEDIUM	40	214	\$948.4	\$41.9	\$1.1	LOW	LOW	LOW	MEDIUM
PBA 3-E	93	90	104	MEDIUM	56	222	\$854.2	\$36.7	\$1.1	LOW	LÓW	Low	HġH
PBA4-A	174.9	96	131	MEDIUM	64	222	\$390.9	\$27.6	\$1.1	TOM	LOW	LÓW	MEDIUM
PBA 4-B	195.0	161	136	MEDIUM	56	214	\$1,012.2	\$31.3	\$1.1	FOM	LOW	LOW	MEDIUM
PBA 5-G	55.6	89	103 1	FOM 5	48	2317	\$604.41	\$35.5	\$0.75 (\$750 K)	LOW 4	WEDIUM 5	MEDIUM	LOW
LOW	0.75	0-60	0.99	Eliminales al-grade crossings	0.39	0-100	\$0 - \$450M	\$0 -\$30M	\$0 - \$1M	No construction in EDC area	No construction in EDG area	No additional cost and schedule impacts.	All Movements Directly at Sampson Street
MEDIUM	75-150	61-120	100-120	Reduces vehicular at-grade crossings	40-50	101-200	\$450M - \$900M	\$30M - \$40M	\$1M - \$20M	Foundation concept to equalize or minimizésoil pressure	Foundation concept to equalize or minimize soil pressure	Some potential for cost and schedule impacts.	Some Movements Indirectly to/from Sampson Street at a Single Location
HIGH	150 -	12) +	121 +	No reduction in v efricular at-grade crossings	5/	201 +	\$900M +	\$40M =	\$20M +	Increase in soil pressure	increase in soil pressure	Increased potential for cost and schedule impacts.	All Movements Indirectly folfrom Sampson Street

* PBA5-Gwould require displacement of 2 pipe racks are not included in the cost estimate above.

PBA 5-G would elevate Sampson St. over the UPRR and KCS railroad tracks; however, the existing railroad track spur serving the petrochemical facility south of existing 1-10 would require relocation due to ground level 1-10 construction. The relocation of this raif oad track spur would result in two new at-grade crossings of Miller Road – one immediately south of I-10 and one north of I-10.

Includes the full closure of Sampson St. for a minimum of 18-months and partial direction opening of Sampson St. as available. Sampson St. would not be fully opened to traffic until I-10 is relocated.

⁴ PBA 5-G would utilize traditional bridge construction with deep foundations located outside of the EDC.

^{*} PBA5-Gwould utilize a shallow foundation with a slab spread footing supported on shallow piles to support light weight fill and MSE walls. Pile depth and frequency of piles would be varied to minimize soil pressures compared to driving deep piles (PBA 1-F). In addition, the I-10 main line lanes would be constructed at an elevated ground level, thereby minimizing soil pressure compared to a multi-level main line.

Objectives Screening Evaluation Matrix of Preliminary Alternatives

Objective	(7	') Avoid/Minimi	ze Impacts	to Communit	ty	(8) Avoid/Min	oid/Minimize Impacts to Cultural Resources & 4(f)/6(f) (9) Avoid/Minimize Impacts to Natural Resources							Other Resources						
	M	N	0	P	I Q	R	S	T	u	T	٧	W	X	Y	Z	AA	BB	CC	DD	EE
Criteria / Measure	Potential Residential Displace- ments (#)	Potential Commercial Displace- ments (#)	Potential EJ/LEP Displace -ments (#)	Mapped Parks Potentially Impacted (#)	Public Facilities Potentially Impacted (#)	NRHP Listed/ Eligible Sites Potentially Impacted (#)	NRHP Listed/Eligible Historic Structures & Districts Potentially Impacted (#)	Identified Section 4(f) &6(f) Resources Potentially Impacted (#)	Mapped Surface Water Features (acres)	Mapped (acres)	Wetland Features (acres by wetland type)	Wetland Soils (acres)	Estimated Section 404 Impacts (acres)	New Crossings of a Navigable Water (#)	Quality Habitat in ROW (acres)	Fish habitat in ROW (acres)	Sensitive Noise Receivers Immediately Adjacent (#)	Potential Visual Impacts from Elevated Structures	Potential Substantial Cost Hazardous Material Sites Impacting Alternatives (#)	Potential Impacts to Private Industry Vessels
No-Build	0.	0	0	0.	0	0	0	0	0	0	Emergent=0 Shrub=0 Forested=0	0	0	0	0	0	0	Low	0	LOW
PBA 1-F	3	12	3	2	0	1	1	2	12.87	18.94	Emergent=1.05 Shrub=11.48 Forested=6.40	17.43	Water = 0.22 Wetland = 14.67 Total = 14.89	1	12.97	12.87	173	LOW	1	MEDIUM
PBA 2-A	3	10	3	2	4	1	1	2	18,99	32.12	Emergent=4.55 Shrub=22.32 Forested=5.25	29.75	Water = 0.28 Wetland = 14.70 Total = 14.98	2	26.48	18.99	177	LOW	1	MEDIUM
PBA 2- B	3	11	3	2	4	1	1	2	19.65	44.80	Emergent=1.45 Shrub=19.58 Forested=23.76	44.35	Water = 1.80 Wetland = 17.19 Total = 18.99	2	36.77	19.65	196	MEDIUM	3	MEDIUM
PBA 2-C	8	10	8	2	4	1	1	2	16.21	31.31	Emergent=0.53 Shrub=22.85 Forested=7.93	31,32	Water = 0.26 Wetland = 14.72 Total = 14.98	2	27.36	16,21	178	LOW	1	MEDIUM
PBA 2-D	7	10	3	2	4	1	1	2	17.00	44.78	Emergent=2.36 Shrub=35.99 Forested=6.43	45,68	Water = 1,80 Wetland = 17,19 Total = 18,99	2	37.30	17.00	197	MEDIUM	3	MEDIUM
PBA 2-E	3	10	3	2	6	1	-1.	2	18.14	41.75	Emergent=1.31 Shrub=29.57 Forested=10.87	41.55	Water = 0.23 Wetland = 14.76 Total = 14.99	2	36.63	18.14	177	Low	1	MEDIUM
PBA 3-A	3	10	3	2	4	1	1	2	18.99	32.12	Emergent=4.55 Shrub=22.32 Forested=5.25	29.75	Water = 0.28 Wetland = 14.70 Total = 14.98	2	26.48	18,99	177	Low	1	MEDIUM
PBA 3-B	3	11	3	2	4	1	4	2	19.65	44.80	Emergent=1.45 Shrub=19.58 Forested=23.76	44,35	Water = 1.80 Wetland = 17.19 Total = 18.99	2	36.77	19.65	196	MEDIUM	3	MEDIUM
PBA 3-C	8	10	3	2	4	1	4	2	16.21	31,31	Emergent=0.53 Shrub=22.85 Forested=7.93	31,32	Water = 0.26 Wetland = 14.72 Total = 14.98	2	27.36	16.21	178	LOW	1	MEDIUM
PBA 3-D	7	10	3	2	4	1	4.	2	17.00	44.78	Emergent=2.36 Shrub=35.99 Forested=6.43	45.68	Water = 1,80 Wetland = 17,19 Total = 18,99	2	37.30	17.00	197	MEDIUM	31	MEDIUM
PBA 3-E	3	10	3	2	6	1	1	2	18.14	41.75	Emergent=1.31 Shrub=29.57 Forested=10.87	41.55	Water = 0.23 Wetland = 14.76 Total = 14.99	2	36.63	18.14	177	Low	-1	MEDIUM
PBA 4-A	5	2	4	2	6	1	14	1	46.00	97.27	Emergent=6.04 Shrub=68.04 Forested=23.19	108.56	Water = 0.20 Wetland = 43.25 Total = 43.45	.4	87.75	46.00	179	HGH	1	HGH
PBA 4-B	5	3	1	2	6	1	14	1	47.00	105 63	Emergent=7.44 Shrub=81.13 Forested=17.06	117.56	Water = 1.72 Wetland = 45.69 Total = 47.41	4	106,19	47.00	198	HGH	3	HIGH
PBA 5-G	10	10	3	t	0	1	1	2	16.33	18.88	Emergent=1.05 Shrub=6.36 Forested=11.47	15.28	Water = 0,22 Wetland = 14.67 Total = 14.89	1	18.37	16,33	167	LOW	2	LOW
LOW	0-2	0-8	0	0	0-2	0	0	0	0-10	0)-30 (Total)	0-25	0-10 (Total)	0	0-20	0-10	0-90	No to Some Slightly Obstructed Views	0	No impact to Friend Ships' vessels; CR no additional bridge crossings
MEDIUM	3-5	9-10	1	1	3-5	1	1-10	1	10-20	3	0-60 (Total)	25-50	10-20 (Total)	1-2	20-40	10-20	91–180	More Obstructed Views	1-2	Vertical clearance impacts to Friend Ships' v essels; additional moveable crossing only
HIGH	6+	11 ±	2+	2+	6+	2+	11+	2	20+	6	0 + (Total)	50 +	20 + (Total)	3+	40 #	20 +	181 +	Most Obstructed Views	3+	Vertical clearance impacts to Friend Ships' viessels; additional moveable crossing & non-moveable crossings

$Objectives\,S\,creening\,Evaluation\,Matrix\,of\,Preliminary\,Alternatives$

Objective	(6) Supports/Consistent with Economic Plans		(6) Supports/Consistent with Economic Development and Transportation Plans				
	FF	GG	нн	II .			
Criteria <i>l</i> Measures	Supports Economic Development	Supports Transportation Plans Identified in MTP and LA STP	Comments Received at Agency Meeting #2	Comments Receiv ed at Public Meeting #2			
No-Build	GENERALLY NOT SUPPORTED	GENERALLY NOT SUPPORTED	GENERALLY NOT SUPPORTED	-6			
PBA 1-F	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	3			
PBA 2-A	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	9			
PBA 2- B	GENERALLY SUPPORTED	GENERALLY SUPPORTED	NEUTRAL	9			
PBA 2-C	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	9			
PBA 2-D	GENERALLY SUPPORTED	GENERALLY SUPPORTED	NEUTRAL	6			
PBA 2-E	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	9			
PBA 3-A	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	16			
PBA 3-B	GENERALLY SUPPORTED	GENERALLY SUPPORTED	NEUTRAL	16			
PBA 3-C	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	16			
PBA 3-D	GENERALLY SUPPORTED	GENERALLY SUPPORTED	NEUTRAL	13			
PBA 3-E	NEUTRAL	GENERALLY SUPPORTED	NEUTRAL	16			
PBA 4-A	NEUTRAL	GENERALLY SUPPORTED	GENERALLY NOT SUPPORTED	-1			
PBA 4-B	GENERALLY SUPPORTED	GENERALLY SUPPORTED	GENERALLY NOT SUPPORTED	-1			
PBA 5-G	NEUTRAL	GENERALLY SUPPORTED	n/a ⁶	n/a ⁶			
GENERALLY SUPPORTED	Improv ements generally support established economic development goals	Generally supports/consistent with MTP and STP	Generally positive comments	10 +			
NEUTRAL	Potential exists for economic dev elopment opportunities	Neutral	No comments received or number of positive and negative comments were generally equal	9 to 3			
GENERALLY NOT SUPPORTED	No improv ements to support established economic development goals	Does not support/inconsistent with MTP and STP	Generally negative comments	2 to -6			

⁶ PBA 5-G has not been presented to the public or agencies.

I-10 IMPROVEMENTS

I-10/I-210WEST END - I-10/I-210 EAST END INTERCHANGES

Sampson Street Stakeholders Meeting

March 21, 2019



Agenda



- Welcome and Introductions
 - Project History and Public Meeting #2 Review
- PBA 5-G
 - Engineering Concepts
 - Construction Traffic Phasing and Movements
- Review of Objectives Screening Matrix Updated with PBA 5-G
 - Change in PBA and Sub-Alternative Combinations
- Upcoming Public Meeting
- Discussion

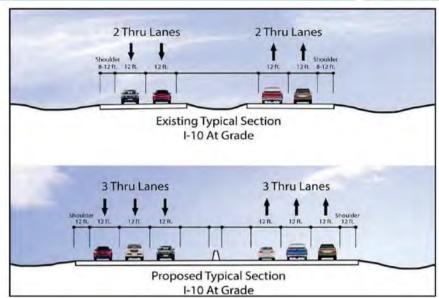


Welcome and Introductions



I-10 Proposed Improvements

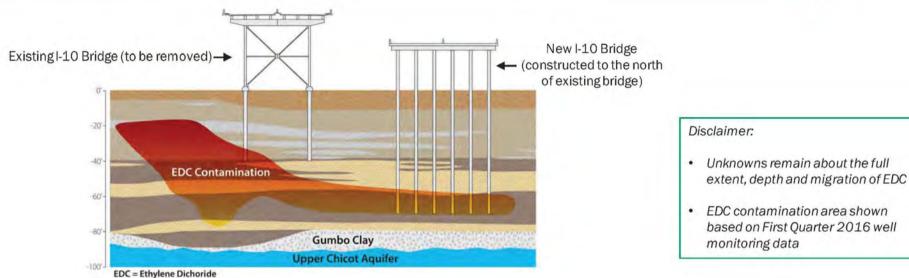
- Widening of I-10 between the I-210 interchanges
- · Six, 12-ft, lanes with 12-ft. shoulders
- New 6-lane overpasses to improve vertical clearance and allow room for I-10 widening
- Proposed access improvements at Sampson St. to/from I-10





PBA 1 | Driven Piles

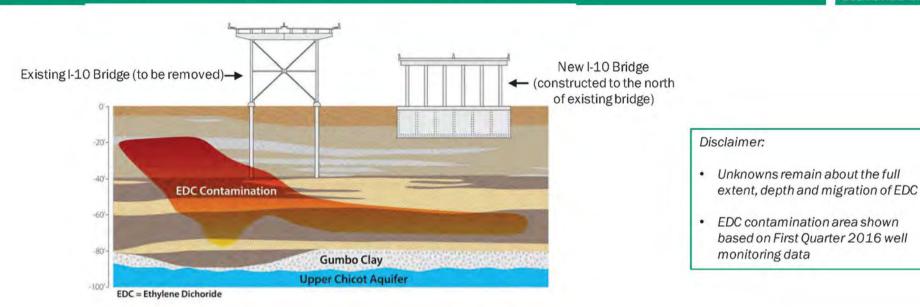














PBA 3 | Long Span Bridge



Long-Span Bridge Examples









PBA 4 | South Corridor

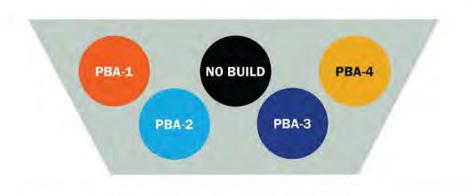
- Bridge replacement south of existing I-10
- Avoids construction in EDC area
- 2 new bridge crossings over Bayou Contraband



Recommended Screening Presented at Public Meeting #2 August 2017



Alternatives Recommended to be Screened Out





Driving piles for the bridge approach span and elevating Sampson St. above the railroad tracks (Sub-Alt. F) is a potential risk for downward migration of EDC towards aquifer



Highest cost, impacts to natural resources and visual impacts

Tier 2 Project Objectives Screening



Recommended Reasonable Alternatives



Compensated foundation and Sampson St. Sub-Alternatives (A-E) avoid/minimize risk of construction in EDC release area.



Long-span bridge and Sampson St. Sub-Alternatives (A-E) avoid/minimize risk of construction in EDC release area.





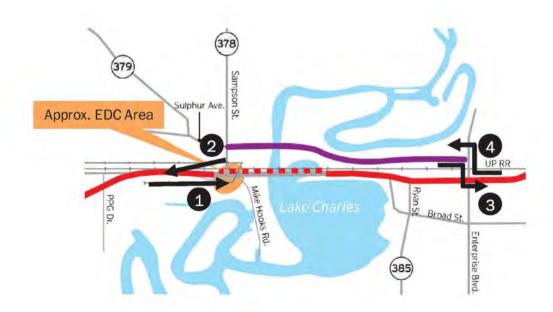
- Sulphur Ave. Extension to I-10 West of Ryan St.
 - 1. EB I-10 exit ramp to Sampson St.
 - 2. WB I-10 entrance ramp from Sampson St.
 - 3. EB I-10 entrance ramp along Sulphur Ave. extension from Sampson St.
 - 4. WB I-10 exit ramp along Sulphur Ave. extension to Sampson St.







- Sulphur Ave. extension to Enterprise Blvd.
 - 1. EB I-10 exit ramp to Sampson St.
 - 2. WB I-10 entrance ramp from Sampson St.
 - 3. EB I-10 entrance ramp at Enterprise Blvd. along Sulphur Ave. extension from Sampson St.
 - 4. WB I-10 exit ramp at Enterprise Blvd. along Sulphur Ave. extension to Sampson St.

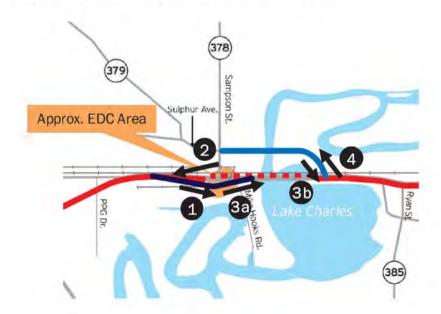






Sulphur Ave. extension to I-10 west of Ryan St.
Intersection improvements at Sampson St. south of I-10

- 1. EB I-10 exit ramp to Sampson St.
- 2. WB I-10 entrance ramp from Sampson St.
- 3. EB I-10 entrance ramp access:
 - a. from Sampson St.
 - b. along Sulphur Ave. extension from Sampson St.
- 4. WB I-10 exit ramp along Sulphur Ave. extension to Sampson St.







- Sulphur Ave. extension to Enterprise Blvd.
- Intersection improvements at Sampson St. south of I-10
 - 1. EB I-10 exit ramp to Sampson St.
 - 2. WB I-10 entrance ramp from Sampson St.
 - 3. EB I-10 entrance ramp access:
 - a. from Sampson St.
 - b. along Sulphur Ave. extension to Enterprise Blvd. from Sampson St.
 - 4. WB I-10 exit ramp from Enterprise Blvd. along Sulphur Ave. extension to Sampson St.

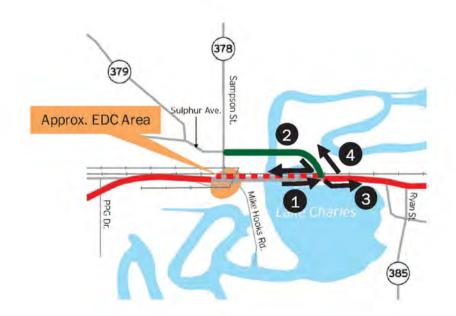






- Sulphur Ave. extension to fully directional, elevated interchange to I-10 west of Ryan St.
 - 1. EB I-10 exit ramp along Sulphur Ave. extension to Sampson St.
 - 2. WB I-10 entrance ramp along Sulphur Ave. extension from Sampson St.
 - 3. EB I-10 entrance ramp along Sulphur Ave. extension from Sampson St.
 - 4. WB I-10 exit ramp along Sulphur Ave. extension to Sampson St.

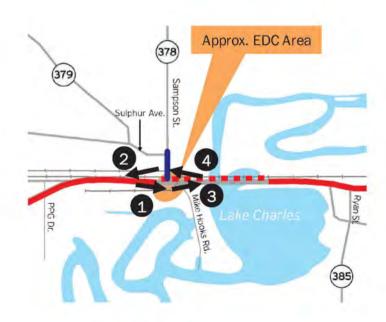
Note: No direct access to/from I-10 at Sampson St.







- Fully directional, elevated interchange over at-grade railroad tracks
- · Drives piles in EDC area
 - 1. EB I-10 exit ramp at Sampson St.
 - 2. WB I-10 entrance ramp at Sampson St.
 - 3. EB I-10 entrance ramp at Sampson St.
 - 4. WB I-10 exit ramp at Sampson St.



PBA + Sub-Alt Combinations Presented at PM #2 August 2017



PRELIMINARY BUILD ALTERNATIVES (PBAs)

- PBA 1 I-10 corridor improvements, new bridge immediately north of existing bridge, pile foundation in EDC contamination area
- PBA 2 I-10 corridor improvements, new bridge immediately north of existing bridge, compensated foundation above EDC contamination depth
- PBA 3 I-10 corridor improvements, new bridge immediately north of existing bridge, long-span bridge over EDC contamination area
- PBA 4 I-10 corridor improvements, new bridge south of existing bridge, 2 new bridge crossings of Bayou Contraband, avoids construction in EDC Area

SAMPSON SUB-ALTERNATIVES (SUB-ALTS)

- A. Sulphur Ave. extension to West of Ryan St.
- B. Sulphur Ave. extension to Enterprise Blvd.
- Sulphur Ave. extension to West of Ryan St. & intersection improvements at Sampson St. south of I-10
- D. Sulphur Ave. extension to Enterprise Blvd. & intersection improvements at Sampson St. south of I-10
- Sulphur Ave. extension to fully directional, elevated interchange to I-10 west of Ryan St.
- F. Fully directional, elevated interchange over Sampson St. at-grade railroad tracks



HOW THE PBAS MATCH UP WITH THE SUB-ALTS

PBA 1	PBA 2	PBA 3	PBA 4
PBA 1 - F	PBA 2 - A	PBA 3 - A	PBA 4 - A
	PBA 2 - B	PBA 3 - B	PBA 4 - B
	PBA 2 - C	PBA 3 - C	
	PBA 2 - D	PBA 3 - D	
	PBA 2 - E	PBA 3 - E	

Preliminary Recommendation After Public Meeting #2 in 2017



Recommended Alternatives To Be Evaluated in the EIS

- PBA 2
- PBA 3
- · Sub-AltA
- · Sub-Alt C
- · Sub-Alt E

Recommended Alternatives Screened Out

- PBA 1
- PBA 4
- Sub-Alt B
- Sub-Alt D
- Sub-Alt F



PBA 5-G

PBA 5 Sub-Alt G Background



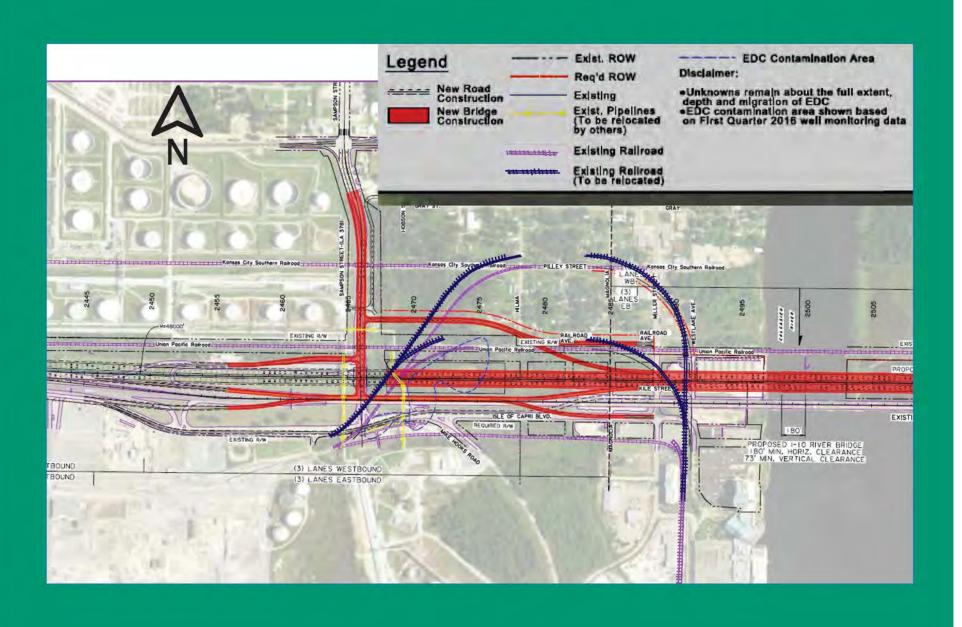
- Additional alternative/sub-alternative developed at the request of LADOTD to address a fully-directional interchange at Sampson.
- HNTB modified PBA 1-F to address Westlake and other stakeholder concerns.
- LADOTD met with Westlake officials to present PBA 5-G.
- Westlake officials agreed that this alternative should be presented to the public.
- It was decided that this alternative should be developed and presented to the public at the same level of detail as the other PBAs.

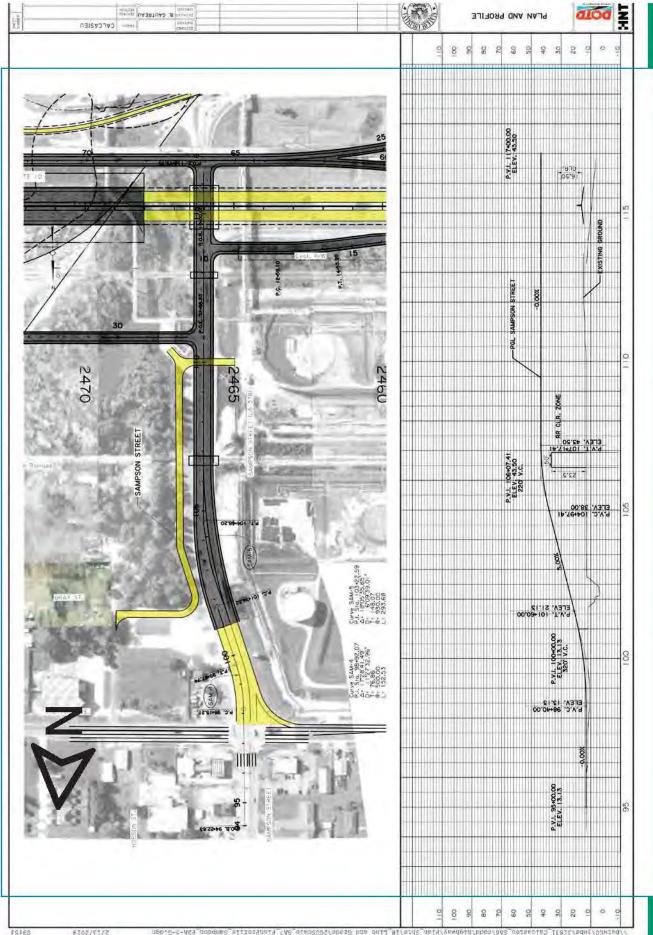
PBA 5-G





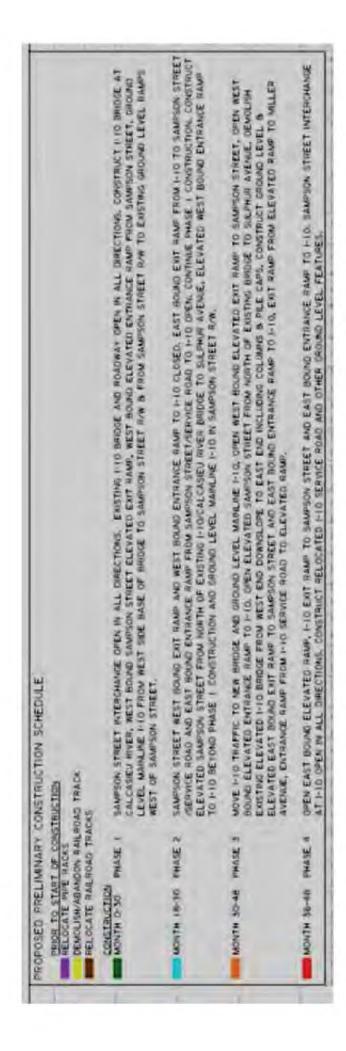
- I-10 bridge ends at the eastern boundary of the EDC area
- Mainline I-10 crosses EDC area on an MSE Wall coming to grade at the western boundary of the EDC area
- Fully directional Sampson Street interchange is elevated over I-10 mainline connecting to the bridge on the east and to the at-grade roadway on the west



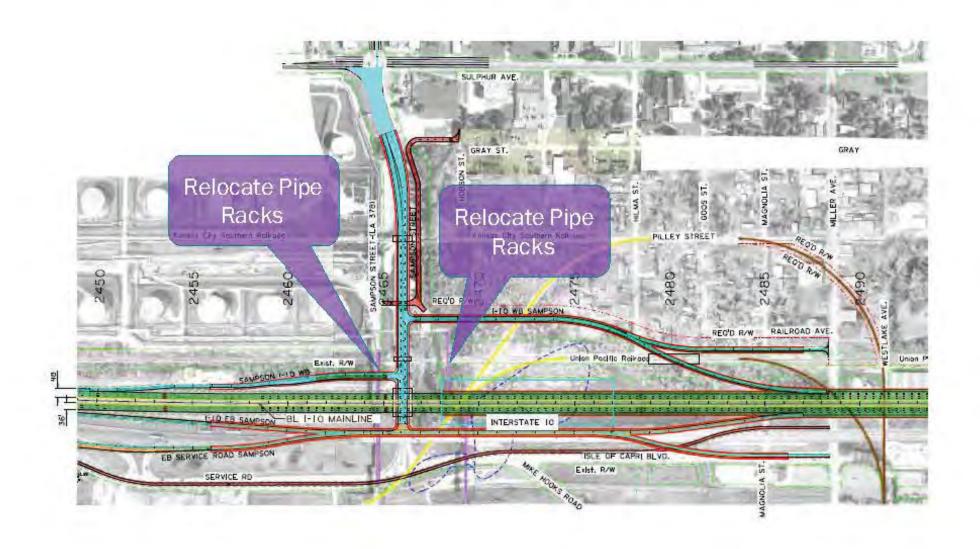


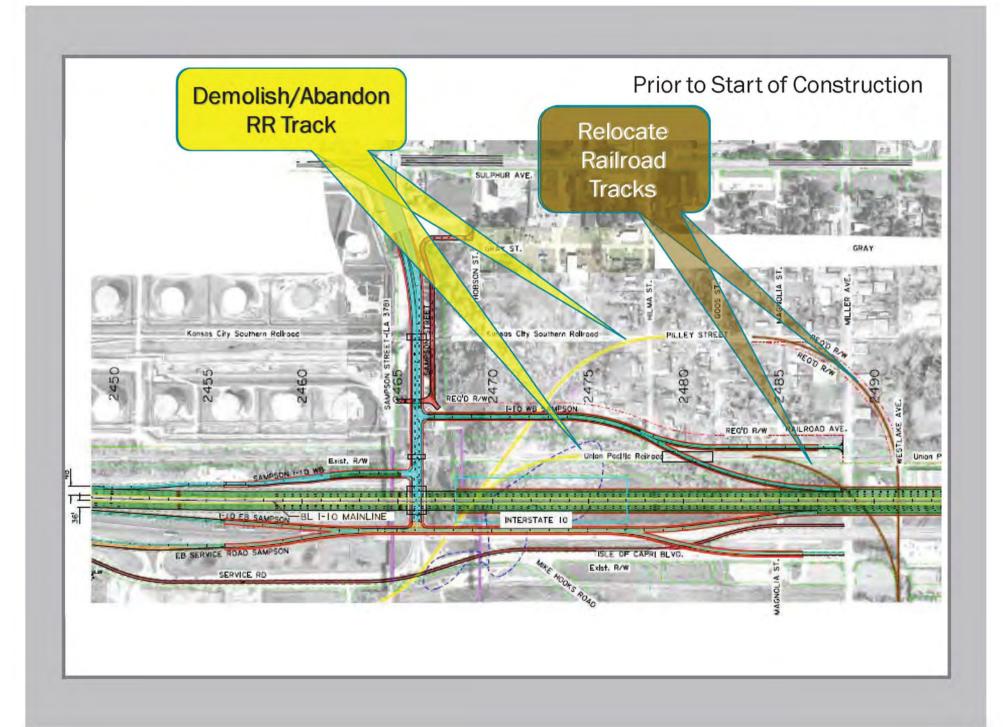


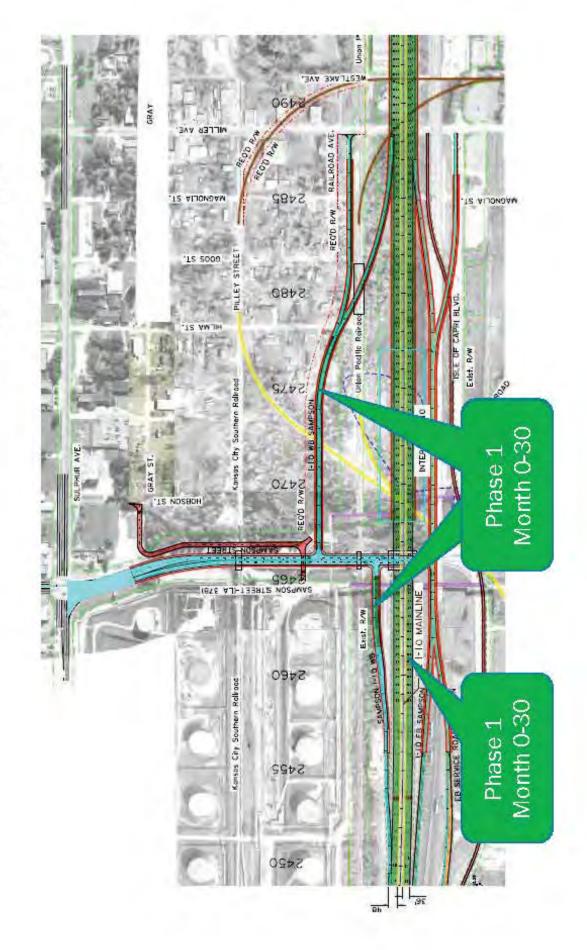
Proposed Preliminary Construction Schedule

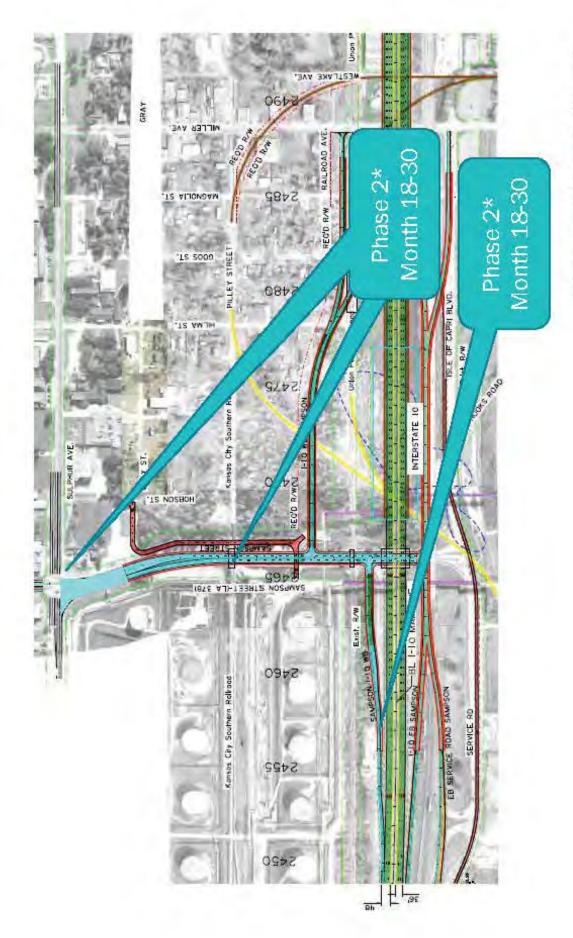


Prior to Start of Construction

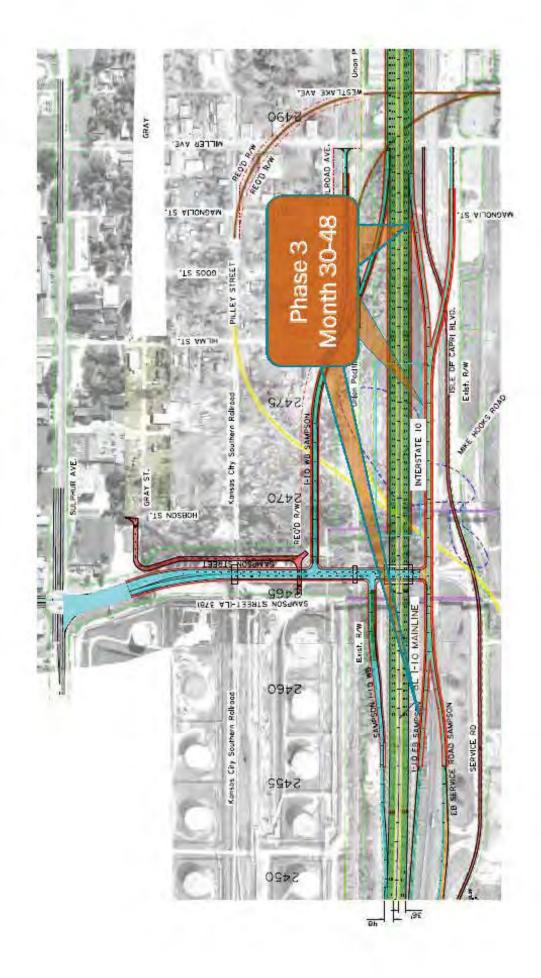


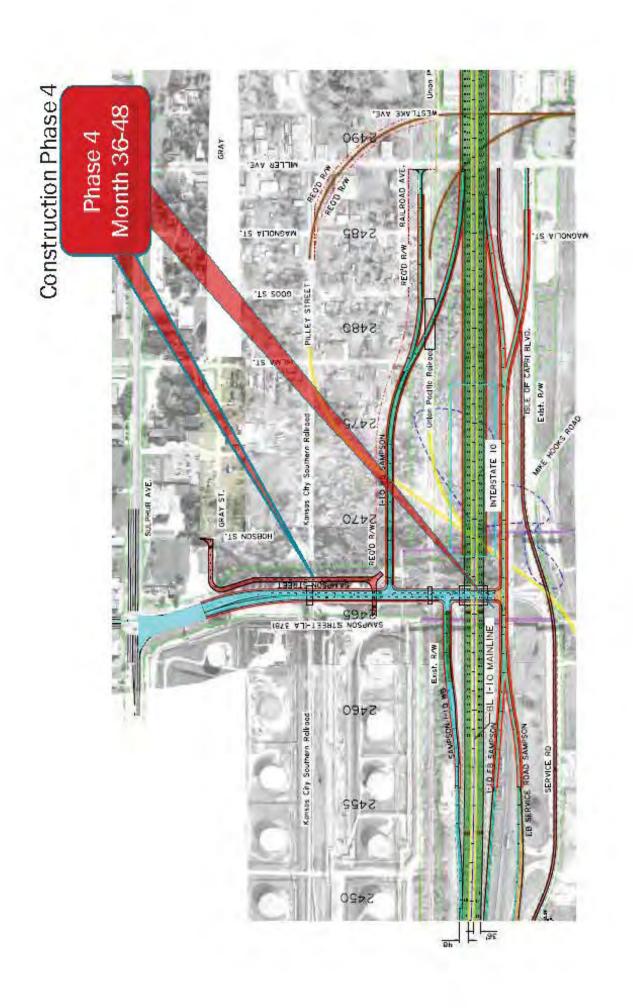






*Sampson Street Closed







WB from Lake Charles to Isle of Capri and Mike Hooks Road





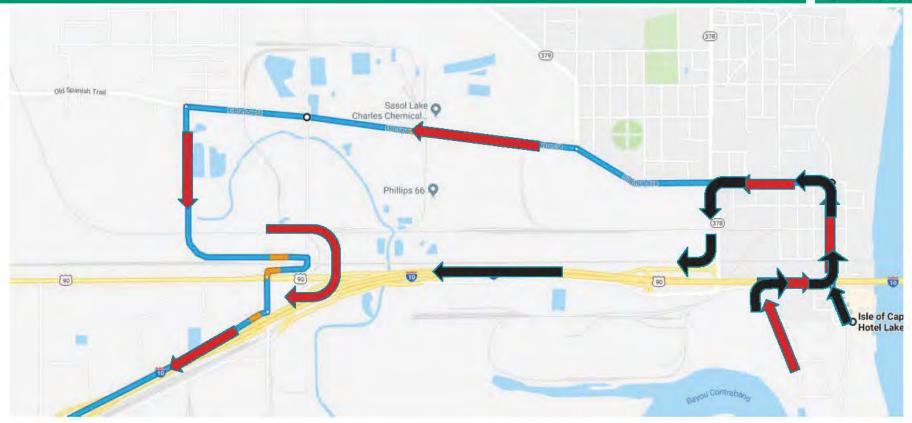


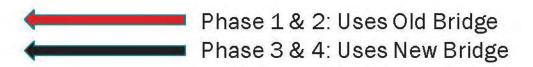
Phase 1 & 2: Uses Old Bridge

Phase 3 & 4: Uses New Bridge

WB to Texas from Isle of Capri and Mike Hooks Road

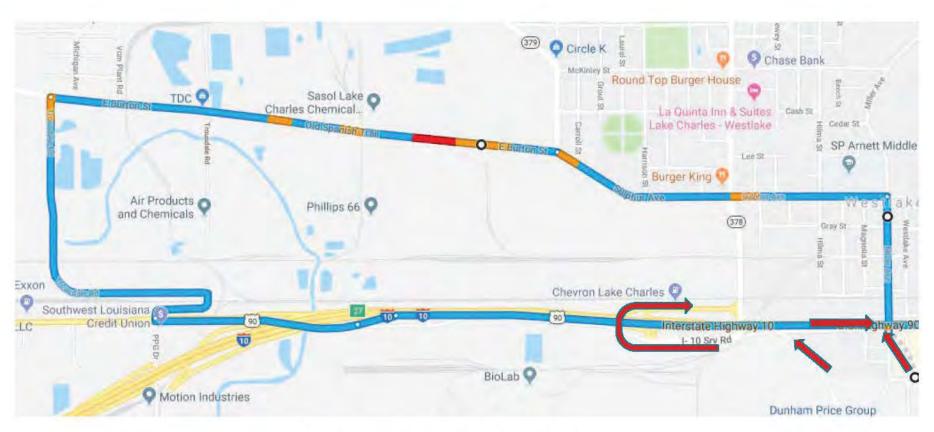








EB from Isle of Capri - Phases 1 & 2



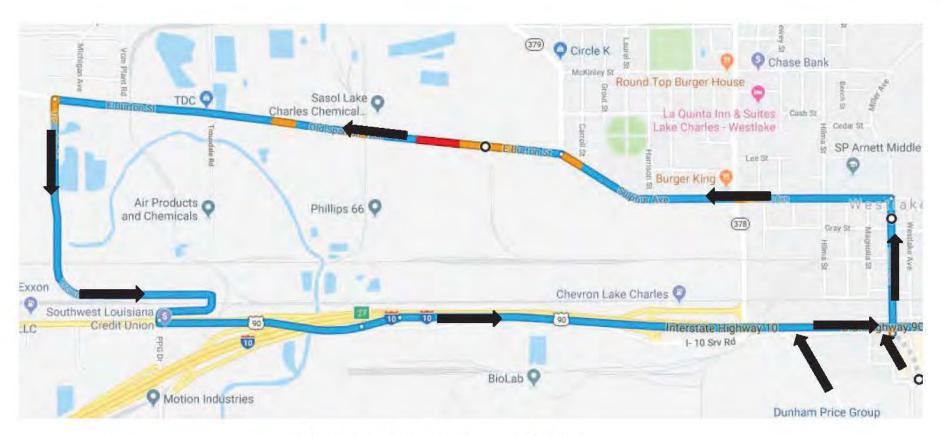


Phase 1 & 2: Uses Old Bridge

Phase 3 & 4: Uses New Bridge



EB from Isle of Capri - Phases 3 & 4



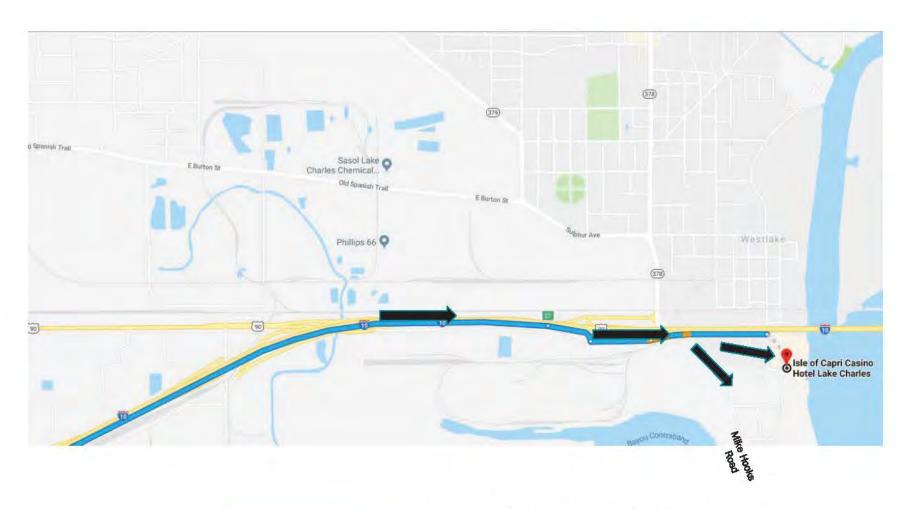


Phase 1 & 2: Uses Old Bridge

Phase 3 & 4: Uses New Bridge

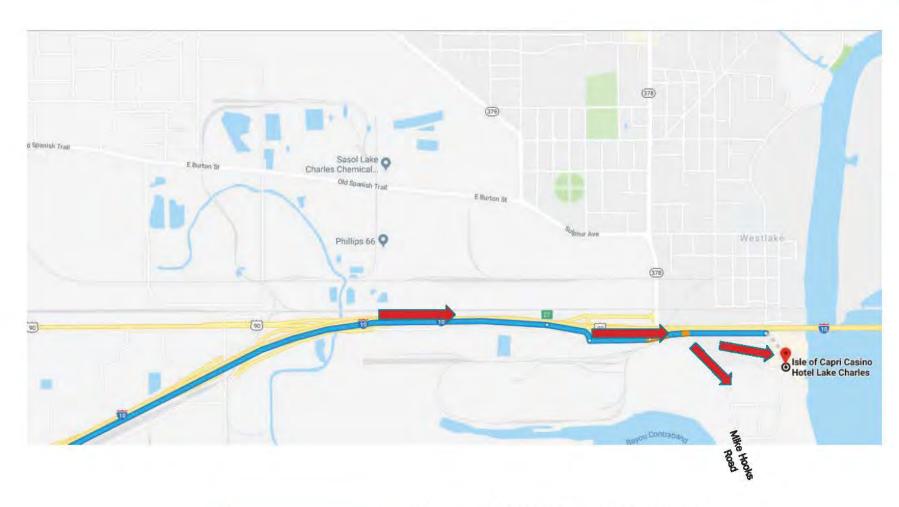
WB to Isle of Capri and Mike Hooks Road from Texas





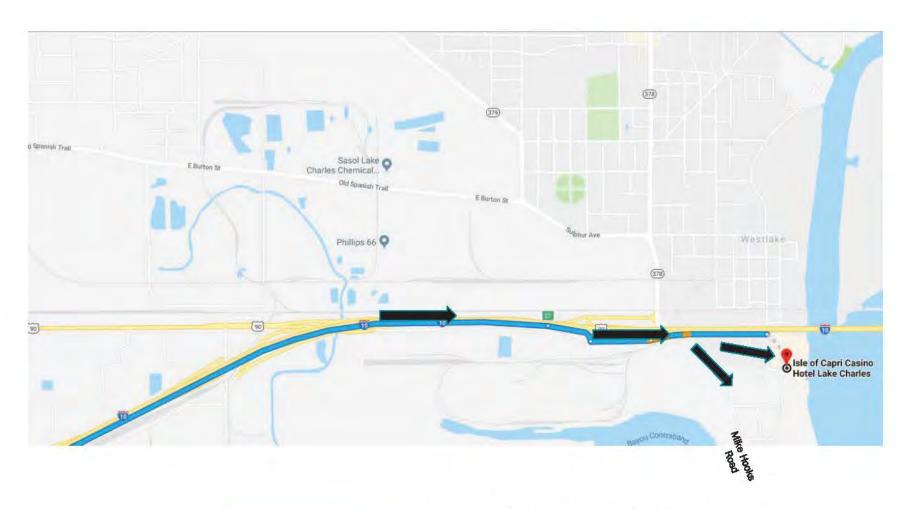
WB to Isle of Capri and Mike Hooks Road from Texas





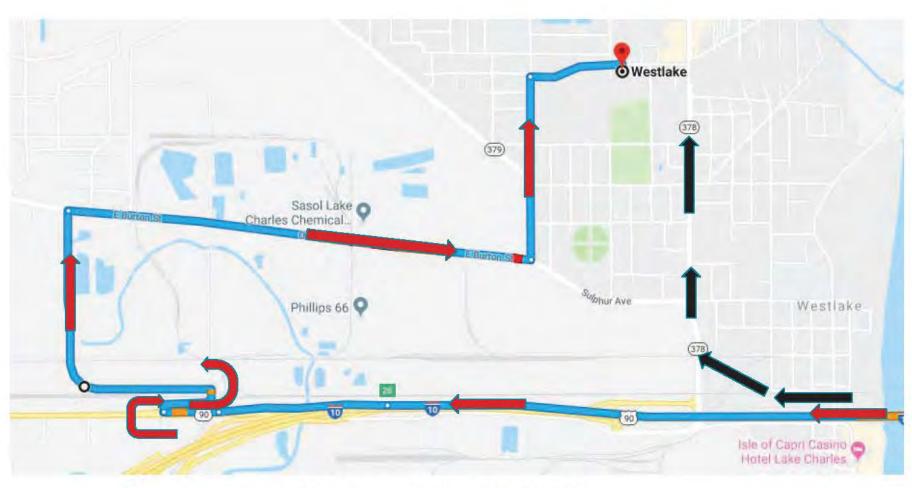
WB to Isle of Capri and Mike Hooks Road from Texas





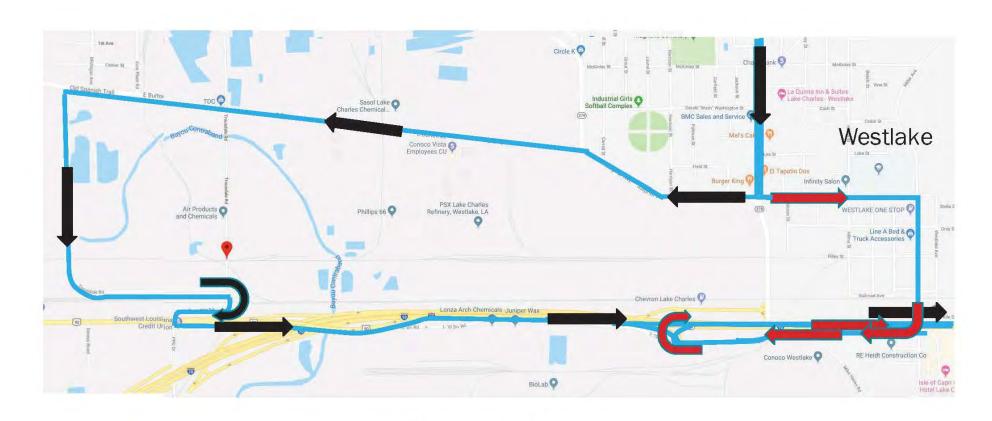
WB to Westlake from Lake Charles





EB from Westlake to Lake Charles







Phase 1 & 2: Uses Old Bridge

Phase 3 & 4: Uses New Bridge





PRELIMINARY BUILD ALTERNATIVES (PBAs)

- PBA 1 I-10 corridor improvements, new bridge immediately north of existing bridge, pile foundation in EDC contamination area
- PBA 2 I-10 corridor improvements, new bridge immediately north of existing bridge, compensated foundation above EDC contamination depth
- PBA 3 I-10 corridor improvements, new bridge immediately north of existing bridge, long-span bridge over EDC contamination area
- PBA 4 I-10 corridor improvements, new bridge south of existing bridge, 2 new bridge crossings of Bayou Contraband, avoids construction in EDC Area

SAMPSON SUB-ALTERNATIVES (SUB-ALTS)

- A. Sulphur Ave. extension to West of Ryan St.
- B. Sulphur Ave. extension to Enterprise Blvd.
- C. Sulphur Ave. extension to West of Ryan St. & intersection improvements at Sampson St. south of I-10
- D. Sulphur Ave. extension to Enterprise Blvd. & intersection improvements at Sampson St. south of I-10
- E. Sulphur Ave. extension to fully directional, elevated interchange to I-10 west of Ryan St.
- F. Fully directional, elevated interchange over Sampson St. at-grade railroad tracks

PBA 5 — I-10 corridor improvements, new bridge immediately north of existing bridge, MSE wall over EDC contamination area



G. Fully directional interchange on Sampson St. over main line I-10, railroad tracks relocated

HOW THE PBAs MATCH UP WITH THE SUB-ALTS

PBA 1	PBA 2	PBA 3	PBA 4	PBA 5
PBA 1-F	PBA 2 - A	PBA 3 - A	PBA 4 - A	PBA 5 - G
	PBA 2 - B	PBA 3 - B	PBA 4 - B	1000000
	PBA 2 - C	PBA 3 - C		
	PBA 2 - D	PBA 3 - D		
	PBA 2 - E	PBA 3 - E		

Revised Objectives Screening Evaluation Matrix of Preliminary Alternatives

Objective	(1) Minimize ROW Impacts		(2) AvoidMinimize impacts to Existing Infrastructure		(3) Hinimize Roadway Disruptions during Construction		(4) Optimize Cost		(6) Minimize Construction Risk in EDC Contamination Area			(6) Improve Access at Sampson Street	
	A	В	C.	D	E	F	G	н	- I	1	К	L	И
Criteria/ Measures	New ROW (acres)	Parcels Impacted (#)	Major-Utilities Crossed (#/)	Railroad Crossing Impacts (Sampson St.)	I-10 Full Road Closures (#)	Arterial Full Road Closures (#)	Estimated Construction Cost in Millions (M)	Estimated ROW Cost in Millions (M)	Estimated Operations and Maintenance Cost in Millions (M)	Potential impacts to Soil Pressure - Sampson St. Construction	Potential Impacts to Soil Pressure - Calicasieu River Bridge Construction	Potential Impacts to Project Cost and Schedule	Ellect on Traffic Movements at Sampson Street
No-Build	0	ō	ő	нен	ō	in .	\$0	\$0	\$31	Low	LOW	LOW.	LOW'
PBA 1-F	61.6	60	102	FOM	48	230	\$599\$	\$288	\$0.63 (\$630 K)	HIGH	HIGH	HIĞH	Low
PBA 2-A	84 <i>J</i>	85	100	MEDIUM	48	226	\$770.3	\$34.6	\$1.1	Low	MEDIUM	MEDIUM	MEDIUM
PBA 2- B	105.3	146	105	MEDIUM	40	214	\$8897	\$39.4	\$1.1	LÖW	MEDIUM	MEDIUM	MEDIUM
PBA 2-C	98.0	110	107	MEDIUM	48	226	\$778.4	\$38.2	\$1.1	Lów	MEDIUM	MEDIUM	MEDIUM
PBA 2-D	122.3	173	112	MEDIUM	40	214	\$897.8	\$41.9	\$1.1	Low	MEDIUM	MEDIUM	MEDIUM
PBA 2-E	93.5	90	104	MEDIUM	56	222	\$803.6	\$367	\$1.1	Lòw	MEDIUM	MEDIUM	нан
PBA 3-A	86	85	100	MEDIUM	48	226	\$821.0	\$34.6	\$1.1	Low	LOW	FOW	MEDIUM
PBA 3-B	105	146	105	MEDIUM	40	214	\$940.4	\$394	\$1.1	LOW	LOW	LOW	MEDIUM
PBA 3-C	9,8	110	107	MEDIUM	48	226	\$829.1	\$382	\$1.1	Low	Low	Fow	MEDIUM
PBA 3-D	122	173	112	MEDIUM	40	214	\$948.4	\$41.9	\$1.1	Low	LOW	LOW	MEDIUM
PBA 3-E	93	90	104	MEDIUM	56	222	\$854.2	\$36.7	\$1.1	LÓW	LOW	LOW	нівн
PBA 4-A	174.9	95	181	MEDIUM	64	222	\$9909	\$27.6	\$1.1	Low	Low	LOW	MEDIUM
PBA 4-B	195.0	1.61	136	MEDIUM	56	214	\$1,0122	\$313	\$1.1	LOW	LOW	LOW	MEDIUM
PBA 5-G	55.6	89	1031	LOW 2	48	231 ^{\$}	\$60441	\$355	\$0.75 (\$750 K)	Low4	MEDIUM ^{\$}	MEDIUM	Low
LOW	0-75	0-60	0-99	Biminates at-grade crossings	0-39	0-100	\$0-\$450M	\$0-\$30M	\$0 - \$1 M	No construction in EDC area	No construction in EDC area	No additional cost and schedule impacts.	All Movements Directly at Sampson Street
MEDIUM	75-150	61-120	100-120	Reducesvehicular at-grade crossings	40-50	101-200	\$450M-\$900M	\$30M-\$40M	\$1 M-\$20M	Foundation concept to equalize or minimize soil pressure	Foundation concept to equalize or minimize soil pressure	Some potential for cost and schedule impacts.	Some Movements Indirectly to/from Sampson Street at a Single Location
нен	150+	121+	121 +	No reduction in vehicular at-grade crossings	51 +	201+	\$900M+	\$40M+	\$20M+	hcrease in soil pressure	Increase in soll pressure	hareased potential for cost and schedule impacts.	All Movements Indirectly to from Sampson Street

¹ PBA 5-G would require displacement of 2 pipe racks crossing perpendicular to 1-10, one west of Sampson St. and the other east of Sampson St. Note: cost of relocating the pipe racks are not included in the cost estimate above.

² PBA 5-G would elevate Sampson St. over the UPRR and KCS railroad tracks; however, the existing railroad track spur serving the petrochemical facility south of existing I-10 would require relocation due to ground level I-10 construction. The relocation of this railroad track spur would result in two new at-grade crossings of Miller Road – one immediately south of I-10 and one north of I-10.

³ Includes the full closure of Sampson St. for a minimum of 18-months and partial direction opening of Sampson St. as available. Sampson St. would not be fully opened to traffic until I-10 is relocated.

⁴ PBA 5-G would utilize traditional bridge construction with deep foundations located outside of the EDC.

⁵ PBA 5-G would utilize a shallow foundation with a slab spread footing supported on shallow piles to support light weight fill and MSE walls. Pile depth and frequency of piles would be varied to minimize soil pressures compared to driving deep piles (PBA 1-F). In addition, the I-10 main line lanes would be constructed at an elevated ground level, thereby minimizing soil pressure compared to a multi-level main line.





- Present full range of preliminary alternatives including 5-G to public
- Present screening including addition of Sampson Street traffic impacts
- Present revised recommended reasonable alternatives
- Solicit comments and concerns



Proposed Meeting Schedule and Venue

Thursday, April 25, 2019

Lake Charles Convention Center

Elected Officials and Agency Briefing: 2:30-3:30 pm

Public Open House Meeting: 4:00-7:00 pm







Railroad Coordination

RAILROAD COORDINATION MEETING DOCUMENTATION



Project: I-10 Calcasieu River Bridge (I-10/I-210 West End to I-10/I-210 East End)

Job Number: 31831

Meeting Location: WebEx Conference Call

Subject: Railroad Coordination for Relocation of Spur Tracks (Alternative 5G)

Meeting Date & Time: 04/09/2020, 1:00 pm

Participants: Shawn Wilson, Secretary, LADOTD

Joachim Umeozulu, Project Management, LADOTD

Jared Ray, Railroad Section, LADOTD

Noel Ardoin, Environmental Section, LADOTD

Tyson Moeller, Network Development, UP

Brandon Kasper, Consultant Representing UP, Benesch

Sri Honnur, Network Development, KCS

Lynn Maloney-Mújica, HNTB

Attachments: 1. Relocation Option 1

2. Relocation Option 2

3. Relocation Options 3 and 4

4. Google Earth Image of Railroad Network

5. Relocation Options 4 (Revised) and 5 Developed After Call

The purpose of the meeting was to initiate coordination among representatives of the Kansas City Southern (KCS) and Union Pacific Railroad (UP) that might be affected by the closure of two spur tracks that cross Sampson Street and the I-10 Service Road from the northeast to the southwest in Westlake, LA. These tracks provide access to the industrial area in the southwest quadrant of the intersection. The closure would only apply to one of the three build alternatives being evaluated in the Environmental Impact Statement (EIS), namely Alternative 5G. Mr. Ray pointed out that the this alternative had the benefit of providing a grade separation for the two mainlines crossing Sampson Street and eliminating the skewed crossing of the I-10 Service Road by the spur tracks.

Four potential relocation scenarios were presented as shown on Attachments 1, 2, and 3. Google Earth with kmz lines of the railroad network was also viewed during the conference call (Attachment 4).

Meeting Documentation (cont'd.)



Mr. Umeozulu welcomed the participants and encouraged the railroad representatives to provide their thoughts on the proposed scenarios.

Mr. Moeller stated that the area to the west where the options would extend the existing rail lines within the industrial area is problematic because several railroads are currently competing for access to the industrial areas west of PPG Drive. Secretary Wilson stated that LADOTD would facilitate coordination at the appropriate levels as needed to ensure clear decision making if necessary.

It was agreed that the scenarios proposed on the east side provide better options. Ms. Maloney noted that Option 2 (shown in red on Attachment 4) would require rehabilitation of the existing spur that parallels Isle of Capri Boulevard. Ms. Maloney explained that the section of bridge on Attachment 3 colored in orange represents the transition of I-10 between at-grade, shown in yellow, and the elevated sections, shown in red. This transition would be built on a retaining wall in order to avoid deep foundations as it crosses the area of EDC contamination. The proposed load platform for the wall is identified on the drawing. Mr. Honnur asked about the possibility of putting the spur track through the retaining wall.

Mr. Moeller and Mr. Honnur discussed different configurations to simplify the alternative access. Mr. Honnur and Mr. Moeller discussed vertical clearance needs and other limitations. Mr. Honnur stated that because the spurs are mostly industry track that the vertical clearance might be relaxed. It was agreed that it would be prudent to design the relocations to ensure that any future needs are not precluded.

It was suggested that the northern access of the red line (Attachment 4) be reconfigured to allow westbound trains traveling across the existing railroad bridge to move forward to the rehabilitated line beginning at Westlake Avenue. This would avoid the need for UP to use the KCS line (shown in yellow on Attachment 4) and then backing up all the way into the industrial area. Mr. Moeller and Mr. Honnur agreed to continue this conversation off-line and provide any other ideas as they are developed.

Action Items:

- 1. Mr. Ray to share Mr. Moeller's, Mr. Kasper's, and Mr. Honnur's contact information.
- 2. Ms. Maloney to provide vertical height data for the proposed I-10 Bridge including the area of the retaining wall.
- 3. Ms. Maloney to have HNTB engineering to develop the UPRR alternative as suggested by Mr. Honnur. (See Attachment 5.)

From: Erik Lewis < <u>eslewis@up.com</u>>
Sent: Sunday, October 3, 2021 2:05 PM

To: Eric Kalivoda < Eric Kalivoda@LA.GOV>; Paul Rathgeber < PAULRATHGEBER@UP.COM>; Tyson

Moeller < TOMOELLER@up.com >

Cc: Drew Tessier < drewtessier@up.com>; Shawn Wilson < Shawn.Wilson@LA.GOV>; Algy Semien

<<u>Algy.Semien@la.gov</u>>

Subject: RE: Calcasieu Rail Discussion

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Mr. Kalivoda,

From an engineering perspective UPRR is agreeable to the most recently submitted concept (Option 2). I have attached an edited version of the last draft of Option 2 with some items we would like your team to look into regarding this project's ability to support some our efforts in the future. As you will note in the solid blue line on the plan set, we're asking that our connection be moved further east. This facilitates our need to constructs a siding in this area in the future, shown in the dashed blue line. While we do not currently have immediate plans to build this siding we know it is coming and the relocation of the spur will help better facilitate that project.

We'd also like the state to take into consideration the closure of the at-grade crossing at Miller Street. With the relocation of the connection route and eventual siding this location will eventually have an increase of blocked crossing events due to how we will have to serve the route as well as perform meets when the siding project comes. Also, with the relocation of the switch and railroad

diamond closer to Miller, the crossing devices will need to be upgraded to support that move and with the presence of this type of track infrastructure in the foot print of this crossing, that upgrade could easily bump up against a seven figure number. The closure will not only mitigate a potential increase in blocked crossing events, but it will inhibit the ability of trucks from the industry just south of I-10 from using the crossing and cutting through the adjacent neighborhoods. And a final aspect of the closure will provide some benefit to the locals: it gets our train horn out of the neighborhood (with the exception of certain horn sequences that are required as part of switching operations).

If you have any question about our additions to the proposed option please feel free to contact me to discuss further.

Regards,



Erik Lewis
Manager Public Projects
TX LA TN TXHSR & CBP

Phone: 281-350-7609 Mobile: 346-337-3107

Email:

eslewis@up.com

24125 Aldine-Westfield Rd. Spring, TX 77373



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It's who stands beside you."



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Public Safety Information: http://www.upcares.com

UPRR Response Management Communication Center 1-888-877-7267

From: Eric Kalivoda < Eric.Kalivoda@LA.GOV Sent: Tuesday, August 10, 2021 4:44 PM

To: Paul Rathgeber < PAULRATHGEBER@UP.COM>

Cc: Drew Tessier <<u>drewtessier@up.com</u>>; Erik Lewis <<u>eslewis@up.com</u>>; Shawn Wilson

<<u>Shawn.Wilson@LA.GOV</u>>; Algy Semien <<u>Algy.Semien@la.gov</u>>

Subject: FW: Calcasieu Rail Discussion

* PROCEED WITH CAUTION - This email was sent from outside the Company *

Gentlemen,

Since our Zoom meeting in early June, we have been in discussions with KCS regarding the track realignment in Westlake, LA. The only option acceptable to KCS is Option 2 wherein they cross the UP track as opposed to sharing a track (see attached RR_Presentation 1 in PowerPoint or PDF). We have revised Option 2 slightly at the request of the Mayor, City of Westlake; KCS is currently reviewing this revision (see attached KCS_Miller_Opt2-August 2021).

Please advise of the acceptability of Option 2 to Union Pacific.

Best regards,

Eric Kalivoda
Deputy Secretary
Louisiana Department of Transportation and Development
Post Office Box 94245
Baton Rouge, LA 70804-9245

Tel: (225) 379-1200 Fax: (225) 379-1851

From: Paul D. Rathgeber < PAULRATHGEBER@UP.COM>

Sent: Friday, June 4, 2021 11:48 AM **To:** Eric Kalivoda < <u>Fric.Kalivoda@LA.GOV</u>>

Cc: Algy Semien < <u>Algy.Semien@la.gov</u>>; <u>drewtessier@up.com</u>; <u>eslewis@up.com</u>; Shawn Wilson

<<u>Shawn.Wilson@LA.GOV</u>>

Subject: Calcasieu Rail Discussion

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Thanks for sending these documents. I have already obtained comments from three groups, and we will start our formal process today.

Paul Rathgeber
Director - Industry & Public Projects
Union Pacific Railroad
24125 Old Aldine Westfield Road
Spring, Texas 77373

Email: PAULRATHGEBER@up.com

Office: (281)350-7197



Public Projects Contact Portal: https://benesch.quickbase.com/db/bpqhu6hqy? a=dbpage&pageid=13

"Eric Kalivoda" ---06/04/2021 09:43:09 AM---* PROCEED WITH CAUTION - This email was sent from outside the Company *

From: "Eric Kalivoda" < Eric.Kalivoda@LA.GOV>
To: "Algy Semien" < Algy.Semien@la.gov>, "Shawn Wilson" < Shawn.Wilson@LA.GOV>, "paulrathgeber@up.com" < special-gener

* PROCEED WITH CAUTION - This email was sent from outside the Company *

In both PowerPoint and PDF – 3% and 4% grade options on Interstate 10

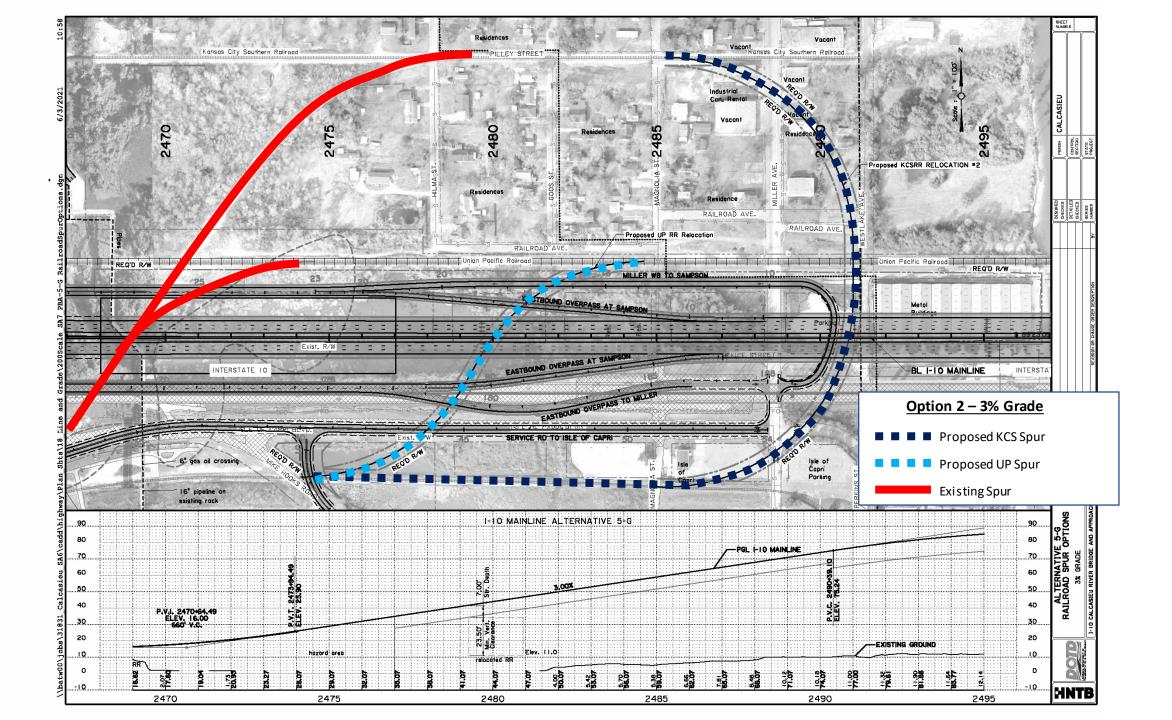
Eric Kalivoda
Deputy Secretary
Louisiana Department of Transportation and Development
Post Office Box 94245
Baton Rouge, LA 70804-9245

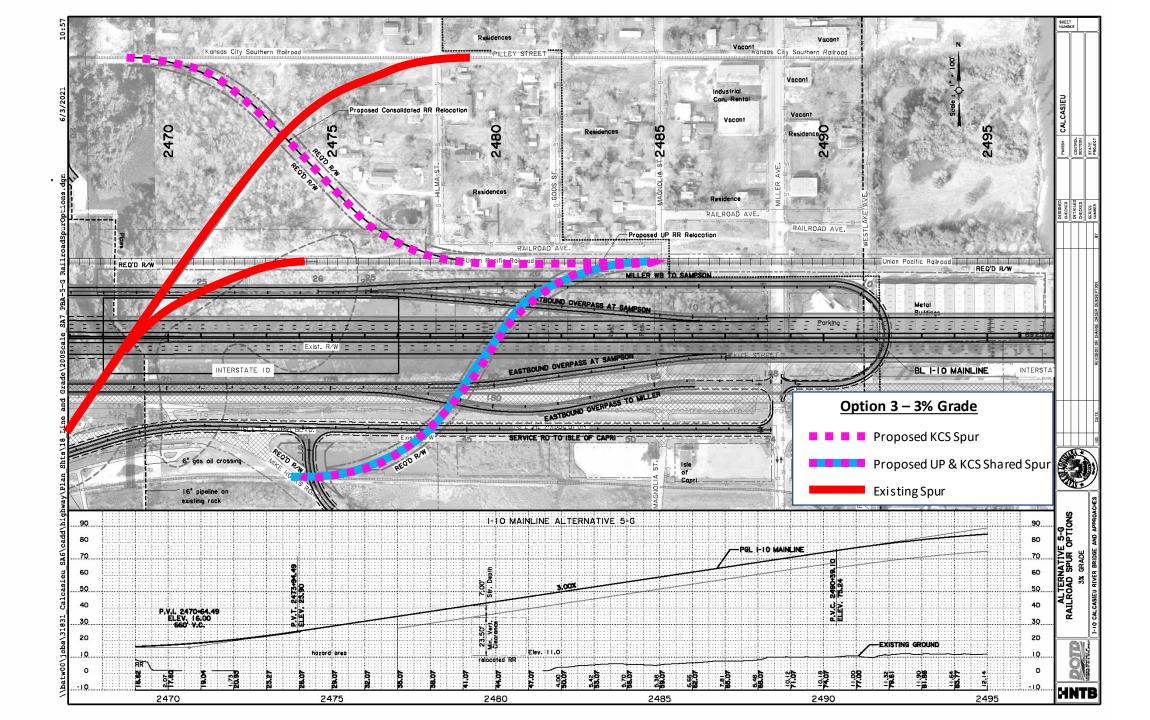
Tel: (225) 379-1200 Fax: (225) 379-1851

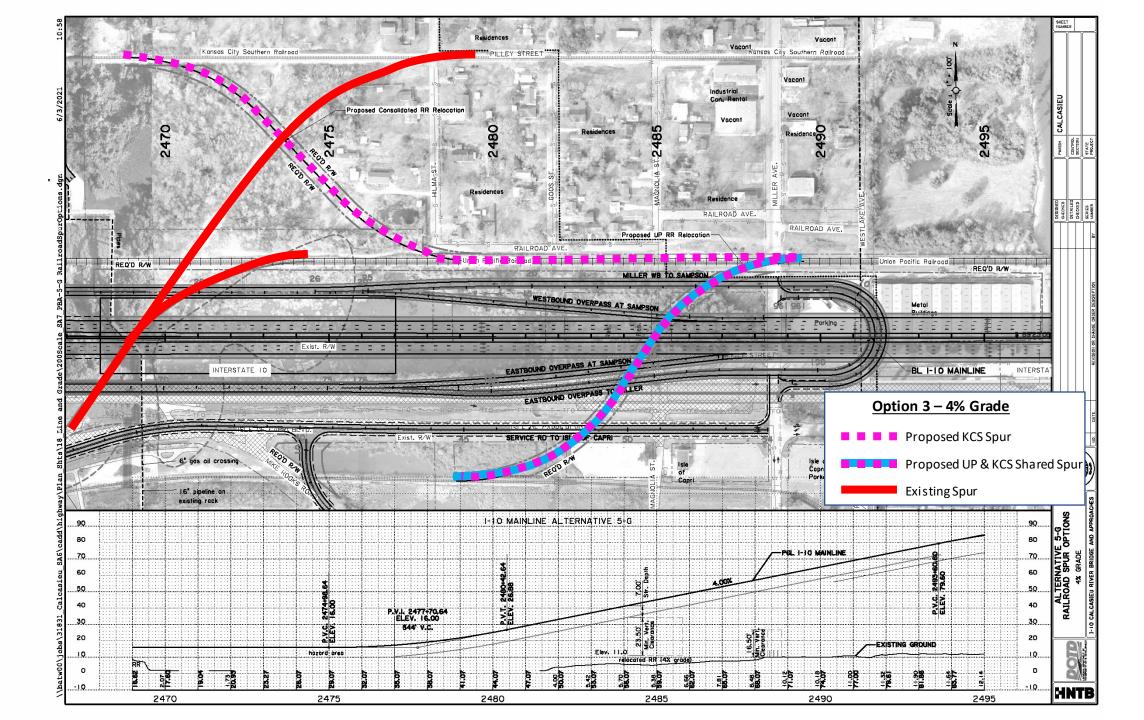
(See attached file: RR Presentation1.pptx)(See attached file: RR Presentation1.pdf)

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**







 From:
 Paul Vaught III (DOTD)

 To:
 Lynn Maloney-Mujica

 Co:
 Need Andries

Cc: <u>Noel Ardoin</u>

Subject: FW: KCS Response to LADOTD Revised Option 2 (Westlake I-10 Bridge Project)

Date: Monday, October 24, 2022 3:18:22 PM

FYI

Paul Vaught, P.E. LA DOTD Critical Projects Division (225) 379-1816 paul.vaughtiii@la.gov

From: Eric Kalivoda <Eric.Kalivoda@LA.GOV> **Sent:** Monday, October 24, 2022 3:17 PM

To: Paul Vaught III (DOTD) <Paul.VaughtIII@LA.GOV>

Subject: FW: KCS Response to LADOTD Revised Option 2 (Westlake I-10 Bridge Project)

See below

From: Barry Morton < <u>BMorton@KCSouthern.com</u>>

Sent: Monday, October 11, 2021 4:38 PM **To:** Eric Kalivoda < Eric. Kalivoda@LA.GOV>

Cc: David O'Neal <<u>doneal@kcsouthern.com</u>>; David Reeves <<u>DReeves@KCSouthern.com</u>>; Manny Loureiro <<u>MLoureiro@KCSouthern.com</u>>; Steven Raiche <<u>SRaiche@KCSouthern.com</u>>; Danny Lites <<u>DLites@KCSouthern.com</u>>

Subject: RE: KCS Response to LADOTD Revised Option 2 (Westlake I-10 Bridge Project)

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Eric:

The original Option 2, with the road crossing projects as noted in the July 30 email and below, is acceptable to KCS.

Barry Morton

From: Eric Kalivoda < Eric.Kalivoda@LA.GOV Sent: Monday, October 4, 2021 11:21 AM

To: Barry Morton < BMorton@KCSouthern.com>

Cc: David O'Neal <<u>doneal@kcsouthern.com</u>>; David Reeves <<u>DReeves@KCSouthern.com</u>>; Manny Loureiro <<u>MLoureiro@KCSouthern.com</u>>; Steven Raiche <<u>SRaiche@KCSouthern.com</u>>; Danny Lites <<u>DLites@KCSouthern.com</u>>

Subject: RE: KCS Response to LADOTD Revised Option 2 (Westlake I-10 Bridge Project)

This email originated from outside the company. Please use caution when opening attachments

or clicking on links.

Gentlemen:

We would prefer to return to the original Option 2 for a number of reasons. The following is the list of closure, upgrades, etc. pertaining to the original Option 2 transmitted via e-mail to me on July 30, 2021. Do you have any concerns about returning to original Option 2?

Eric,

This is the information for Option 2

Connecting road to be built adjacent to the south side of KCS ROW between Hilma St. and Magnolia St.

Close Hilma St Grade Crossing.

Install Flashers and Gates on Goos St.

Close Magnolia St North of I-10.

Install Flashers and Gates on Miller Ave North of I-10.

No crossing on Railroad Ave.

No Crossing on Kile St.

Install Flashers and Gates on Isle of Capri Blvd / Perkins St

Install Flashers and Gates on Miller Ave South of I-10.

Close Magnolia St South of I-10.

Lets us know if you have any other questions Thanks

Daniel L. Lites

Public Safety Director

The Kansas City Southern Railway Company 4601 Hilry Huckaby III Avenue | Shreveport LA. 71107

Office: 318.6766296 | Fax: 318.6766273

Email: <u>dlites@kcsouthern..com</u>

Best Regards,

Eric Kalivoda
Deputy Secretary
Louisiana Department of Transportation and Development
Post Office Box 94245
Baton Rouge, LA 70804-9245

Tel: (225) 379-1200 Fax: (225) 379-1851 **From:** Barry Morton < <u>BMorton@KCSouthern.com</u>>

Sent: Friday, September 10, 2021 2:09 PM **To:** Eric Kalivoda < Eric.Kalivoda@LA.GOV>

Cc: David O'Neal <<u>doneal@kcsouthern.com</u>>; David Reeves <<u>DReeves@KCSouthern.com</u>>; Manny Loureiro <<u>MLoureiro@KCSouthern.com</u>>; Steven Raiche <<u>SRaiche@KCSouthern.com</u>>; Danny Lites <<u>DLites@KCSouthern.com</u>>

Subject: KCS Response to LADOTD Revised Option 2 (Westlake I-10 Bridge Project)

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Fric:

Our internal team has reviewed the latest proposal regarding the Westlake I-10 Bridge Project, which resulted from conversations between LADOTD and the Mayor of Westlake. Regarding the revision itself, it obviously shifts the proposed track alignment further west, creating increased curvature. It does appear that the curvature will fit within engineering standards. However, the revision will require a rail bridge to be built in the area south of I-10 and just to the west of Magnolia Street (South). There is an existing rail bridge at the location, however the revised track layout will place the new rail switch right on top of the existing bridge.

From a public safety perspective, this new proposal includes two new crossings; one at Isle of Capri Boulevard and one at the I-10 exit ramp to Isle of Capri. Based upon the new design, we have the following requests of the department pertaining to public safety:

Crossing Closures:

- Hilma Street
- Magnolia Street (North of I-10)
- Magnolia Street (South of I-10)

Flashers and Gates:

- Goos Street (upgrade)
- I-10 Exit Ramp (new construction)
- Isle of Capri Boulevard (new construction)

Connecting Road Construction:

 Adjacent to, and following the south side of KCS ROW between Hilma Street and Magnolia Street

Attached please find our thoughts overlaid on the existing drawing. As always, please reach out if you have any questions or concerns.

Assistant Vice President Health & Safety The Kansas City Southern Railway Company

427 West 12th Street | Kansas City, MO 64105 Office: 816.983.1518 | Cell: 904.614.5923 Email: <u>bmorton@kcsouthern.com</u>